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CHIEF EXECUTIVE'S OFFICE
CHIEF EXECUTIVE
Fiona Marshall

22 February 2017

Dear Councillor

You are summoned to attend the meeting of the;

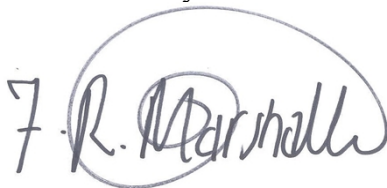
PLANNING AND LICENSING COMMITTEE

on **THURSDAY 2 MARCH 2017 at 7.30 pm.**

in the Council Chamber. Maldon District Council Offices, Princes Road, Maldon.

A copy of the agenda is attached.

Yours faithfully

A handwritten signature in blue ink, appearing to read 'Fiona Marshall', enclosed within a large, loopy oval shape.

Chief Executive

COMMITTEE MEMBERSHIP

CHAIRMAN

Councillor Mrs P A Channer, CC

VICE-CHAIRMAN

Councillor A K M St. Joseph

COUNCILLORS

E L Bamford
H M Bass
B S Beale MBE
R G Boyce MBE, CC
P G L Elliott
M S Heard
S J Savage
Mrs M E Thompson

Ex-officio non-voting Members:

*Councillor M F L Durham
Councillor Miss M R Lewis*

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AGENDA
PLANNING AND LICENSING COMMITTEE
THURSDAY 2 MARCH 2017

1. Chairman's notices (please see overleaf)
2. Apologies for Absence
3. **Minutes of the last meeting** (Pages 7 - 20)

To confirm the Minutes of the meeting of the Committee held on 19 January 2017 (copy enclosed).

4. **Disclosure of Interest**

To disclose the existence and nature of any Disclosable Pecuniary Interests, other Pecuniary Interests or Non-Pecuniary Interests relating to items of business on the agenda having regard to paragraphs 6-8 inclusive of the Code of Conduct for Members.

(Members are reminded that they are also required to disclose any such interests as soon as they become aware should the need arise throughout the meeting).

5. **Public Participation**

To receive the views of members of the public on items of business to be considered by the Committee (please see below):

1. A period of ten minutes will be set aside.
2. An individual may speak for no more than two minutes and will not be allowed to distribute or display papers, plans, photographs or other materials.
3. Anyone wishing to speak must notify the Committee Clerk between 7.00pm and 7.20pm prior to the start of the meeting.

6. **FUL/MAL/16/01419 and LBC/MAL/1420 Vaulty Manor, Goldhanger Road, Heybridge** (Pages 21 - 28)

To consider the planning application and recommendations of the Interim Head of Planning Services (copy enclosed, Members' Update to be circulated)*.

7. **Dengie Gateway Project Update**

To receive a presentation from the Group Manager (Leisure, Countryside and Tourism).

8. **Economic Development Update** (Pages 29 - 40)
To receive and note the report of the Chief Executive, (copy enclosed).
9. **Community Led Housing Programme** (Pages 41 - 50)
To consider the report of the Director of Customers and Community, (copy enclosed).
10. **Strategic Design Codes - North Heybridge Garden Suburb** (Pages 51 - 126)
To consider the report of the Interim Head of Planning Services, (copy enclosed).
11. **Draft Maldon District Design Guide and Proposed Public Consultation** (Pages 127 - 282)
To consider the report of the Interim Head of Planning Services, (copy enclosed).

(Members are reminded that there will be a Member briefing on the Draft Maldon District Design Guide on Wednesday 1 March at 7:30pm in the Training Room.)
12. **Response to Natural England - England Coastal Path, Maldon to Salcott Section** (Pages 283 - 288)
To consider the report of the Director of Customers and Community, (copy enclosed).
13. **Community Infrastructure Levy** (Pages 289 - 292)
To consider the report of the Interim Head of Planning Services, (copy enclosed).
14. **Approval of Woodham Walter Conservation Area Designation Article 4(1) Direction and Character Statement** (Pages 293 - 346)
To consider the report of the Interim Head of Planning Services, (copy enclosed).
15. **Woodham Walter Village Design Statement** (Pages 347 - 398)
To consider the report of the Interim Head of Planning Services, (copy enclosed).
16. **A12 Chelmsford to A120 Widening: Highways England Consultation** (Pages 399 - 408)
To consider the report of the Director of Customers and Community, (copy enclosed).
17. **Planning Policy Update** (Pages 409 - 412)
To consider the report of the Interim Head of Planning Services, (copy enclosed).
18. Any other items of business that the Chairman of the Committee decides are urgent

Note:

1. The Council operates a facility for public speaking. This will operate only in relation to the consideration and determination of planning applications under Agenda Item No. 6.
2. The Committee may hear from one objector, one supporter, a Parish / Town Council representative, and the applicant / agent. Please note that the opportunity to speak is afforded only to those having previously made previous written representation.
3. Anyone wishing to speak must notify the Committee Clerk or a Planning Officer between 7pm and 7.20pm prior to the start of the meeting.
4. For further information please ring 01621 875791 or 876232 or see the Council's website – www.maldon.gov.uk/committees

* Please note the list of related Background Papers attached to this agenda.

NOTICES**Sound Recording of Meeting**

Please note that the Council will be recording any part of this meeting held in open session for subsequent publication on the Council's website. At the start of the meeting an announcement will be made about the sound recording. Members of the public attending the meeting with a view to speaking are deemed to be giving permission to be included in the recording.

Fire

In event of a fire, a siren will sound. Please use the fire exits marked with the green running man. The fire assembly point is outside the main entrance to the Council Offices. Please gather there and await further instruction.

Health and Safety

Please be advised of the different levels of flooring within the Council Chamber. There are steps behind the main horseshoe as well as to the side of the room.

Closed-Circuit Television (CCTV)

Meetings held in the Council Chamber are monitored and recorded by CCTV.

BACKGROUND PAPERS

The Background Papers listed below have been relied upon in the preparation of this report:

1. The current planning applications under consideration and related correspondence.
2. All third party representations and consultation replies received.
3. The following Statutory Plans and Supplementary Planning Guidance, together with relevant Government legislation, Circulars, Advice, Orders, Directions and Guidance:

Development Plans

- Maldon District Replacement Local Plan 2008 – Saved Policies
- Maldon District Local Development Plan 2014-2029 - submitted to the Secretary of State for Examination in Public on 25 April 2014 (as amended).

Legislation

- The Town and Country Planning Act 1990 (as amended)
- Planning (Listed Buildings and Conservation Areas) Act 1990
- Planning (Hazardous Substances) Act 1990
- The Planning and Compensation Act 1991 and The Planning and Compulsory Purchase Act 2004 (as amended)
- The Planning Act 2008
- The Town and Country Planning (General Permitted Development) Order 1995 (as amended)
- The Town and Country Planning (Development Management Procedure) (England) Order 2010
- The Town and Country Planning (Use Classes) Order 1987 (as amended)
- The Town and Country Planning (Control of Advertisements) (England) Regs 2007
- The Town and Country Planning (Environmental Impact Assessment) Regs 2011
- Localism Act 2011
- The Neighbourhood Planning (General) Regulations 2012
- The Town and Country Planning (Local Planning) (England) Regulations 2012
- Growth and Infrastructure Act 2013

Supplementary Planning Guidance and Other Advice

- i) Government policy and guidance
 - National Planning Policy Framework (NPPF) and Technical Guidance
 - Planning Practice Guidance (PPG)
 - Planning policy for traveler sites
 - Planning practice guidance for renewable and low carbon energy
 - Relevant government circulars
- ii) Essex County Council
 - Essex Design Guide 1997
- iii) Maldon District Council
 - Submission Local Development Plan (April 2014) (as amended)
 - Five Year Housing Land Supply Statement 2014/15
 - Planning Policy Advice Note v.4 (October 2015)
 - Infrastructure Delivery Plan (All versions, including update in Council's Hearing Statement)
 - Infrastructure Phasing Plan (January 2015)

- North Heybridge Garden Suburb Strategic Masterplan Framework - 2014
- South Maldon Garden Suburb Strategic Masterplan Framework - 2014
- Vehicle Parking Standards SPD - July 2006
- Accessibility to Buildings SPD – December 2006
- Children’s Play Spaces SPD – March 2006
- Sadd’s Wharf SPD – September 2007
- Heybridge Basin Timber Yard SPD – February 2007
- Developer Contributions Guide - 2010
- Affordable Housing Guide – June 2006
- Heybridge Basin Village Design Statement – November 2006
- Wickham Bishops Village Design Statement - 2010

Copies of all Background Papers are available for inspection at the Maldon District Council Offices, Princes Road, Maldon, Essex CM9 5DL during normal office hours

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**MINUTES of
PLANNING AND LICENSING COMMITTEE
19 JANUARY 2017**

PRESENT

Chairman	Councillor Mrs P A Channer, CC
Councillors	E L Bamford, H M Bass, B S Beale MBE, R G Boyce MBE, CC, M S Heard, S J Savage, and Mrs M E Thompson
Substitute Members	Councillors M F L Durham and Miss M R Lewis

883. CHAIRMAN'S NOTICES

The Chairman welcomed Councillor M S Heard and Mr Simon Rowberry, Interim Head of Planning Services to the meeting and then drew attention to the list of notices published on the back of the agenda.

884. APOLOGIES FOR ABSENCE

Apologies for absence were received from Councillors P G L Elliott and A K M St. Joseph. In accordance with notice duly given Councillor M F L Durham was attending as a substitute for Councillor Elliott and Councillor Miss M R Lewis as a substitute for Councillor St. Joseph.

885. MINUTES OF THE LAST MEETING

RESOLVED

- (i) that the Minutes of the meeting of the Committee held on 17 November 2016 be received.

Minute No. 698 – Disclosure of Interest

Councillor S J Savage advised that in respect of his declaration recorded since the meeting it had been confirmed to him that the Town Council was not misquoted.

Minute 705 – 2017 / 18 Revenue Budget and Fees and Charges

Councillor Savage pointed out that the Chief Executive had agreed to provide the Committee with information regarding the Dawn Sailing Barge and this had not been forthcoming. In response, the Chief Executive apologised and advised that information would be circulated at the earliest opportunity.

RESOLVED

- (ii) that the Minutes of the meeting of the Committee held on 17 November 2016 be confirmed.

886. DISCLOSURE OF INTEREST

Councillor S J Savage disclosed a non-pecuniary interest in Agenda Item 7 – Draft Maldon and Heybridge Central Area Masterplan and Proposed Public Consultation as he was the liaison Councillor on behalf of the Town Council to liaise with the members of the public in Downs Road, Chelmer Terrace, North Street and the Hythe regarding the works to Chelmer Terrace.

Councillor R G Boyce disclosed a non-pecuniary interest as a Member of Essex County Council and advised that in relation to Agenda Item 10 – Proposal to Establish a Strategic Housing Board he was a member of an Almshouse Trust.

Councillor Mrs P A Channer disclosed a non-pecuniary interest as a Member of Essex County Council in relation to the following Agenda items:

- 7 – Proposed Flood Alleviation Scheme at Brickhouse Farm, Maldon
- 8 – Community Flood Grant Applications and Project Management of the Scheme
- 9 – Meeting the Need of Affordable Housing.
- 10 – 2017 / 18 Revenue Budget Fees and Charges

Councillor Mrs Channer also advised that in relation to Agenda Item 10 – Proposal to Establish a Strategic Housing Board she was a member of an Almshouse Trust.

In respect of Agenda Item 10 – Proposal to Establish a Strategic Housing Board Councillor Savage informed the Committee that he was a member of two Almshouse Trusts.

887. PUBLIC PARTICIPATION

In accordance with the Council's public participation scheme Mr Harrison of Stock Chase, Heybridge addressed the Committee in relation to Agenda Item 7 – Draft Maldon and Heybridge Central Area Masterplan and Proposed Public Consultation.

At this point, the Chairman advised that Agenda Item 12 – Planning Enforcement had been withdrawn.

At the request of the Chairman, it was agreed that Agenda Item 18 – Update on the Neighbourhood Plan would be considered after Agenda Item 8 – Planning Policy Update.

888. LOCAL DEVELOPMENT PLAN SITE ALLOCATIONS DEVELOPMENT PLAN DOCUMENTS

The Committee considered the report of the Interim Head of Planning Services which set out the key recommendations from the Gypsy & Travellers and Rural Housing & Employment Allocations Member Task and Finish Working Groups.

The report provided a detailed update on the following matters:

- Local Development Plan (LDP);
- Planning Policy Framework;
- Task and Finish Groups Background;
- The Site allocations Development Plan Document (DPD);
- Implications for the LDP.

Appendix 1 to the report set out the Council's hearing statement issues which incorporated the draft revised policy H6 Provision for Travellers. This document had been presented to the Planning Inspector for consideration as a modification to the LDP at the Examination in Public (EiP).

The report provided an update on the status of the DPD and progression since its publication in January 2014.

It was noted that the Task and Finish Working Group considered three options for the DPD and option 2 - to Create headroom for the preparation of the Neighbourhood Plans was the recommendation of the Working Groups. The report advised how the Council had exceeded the housing target set within the Pre-submission LDP and the rural target and therefore Members were informed there was no longer a need for a Rural Housing Allocations DPD and it was recommended that the shortfall of 100 units as required in Policy S2 be delegated to the Neighbourhood Plans.

The Planning Policy Manager provided Members with an update following the EiP which had recently concluded.

In response to a question regarding informing Parish Councils, the Strategic Planning Policy Manager advised that following the EiP there would be a public consultation on matters arising. He advised that Parish Councils could be notified of the decision and the approach being taken in respect of the LDP.

RESOLVED

- (i) that the recommendation of the Gypsy and Travellers, and Rural Housing and Employment Allocations Task and Finish Group to not proceed with the preparation of the Site Allocations Development Plan Document is endorsed;
- (ii) that a report be made to the Council.

889. DRAFT MALDON AND HEYBRIDGE CENTRAL MASTERPLAN AND PROPOSED PUBLIC CONSULTATION

The Committee considered the report of the Interim Head of Planning Services seeking Members' agreement to amend the draft Maldon and Heybridge Central Area Masterplan (the Masterplan) (attached as Appendix 1 to the report) put forward by the Member Task and Finish Working Group. In addition, if the amendment was agreed Members' were also asked to agree for the Masterplan to go out for public consultation.

A number of proposed amendments to the Masterplan were set out in the report and Members were reminded of the project brief attached at Appendix 2

The Committee discussed the document and a number of comments were raised. The Chairman reminded the Committee that it was being asked to approve the document for consultation and she was sure as part of the consultation process a number of the comments made by Members would be raised. Members were also informed that any Member could write in regarding the document and provide comment which would be taken into consideration as part of the process.

In response to a questions regarding the change from 'the Maldon District Story' to the Sense of Place, Members were referred to Appendix 3 to the report which provided details in respect of the Maldon District Sense of Place.

Councillor S J Savage reaffirmed his declaration of interest at this point.

In response to the Masterplan comments were raised by Members of the Committee and these related to:

- the proposed alteration to highways junctions and it was requested that intervention 4 be removed;
- the "new iconic bridge" at Sadd's Wharf and access by the existing boatyard;
- the location of the proposed bike track;
- there being no reference to the Maldon Museum, the possible relocation of the Maldon Tourist Information Centre and the lake at Promenade Park;
- the proposed market on the Hythe Quay.

In response to questions, the Senior Conservation and Urban Design Officer confirmed that:

- the Battle of Maldon site was designated as such but Historic England had accepted that there was still uncertainty about the location.
- there was no intention to change any highway junctions.

Councillor Miss M R Lewis commented on the document and proposed that the Committee accept the recommendations as set out in the report and move forward to public consultation as soon as possible. This proposal was duly seconded and upon a vote being taken was agreed.

The Chairman requested that Members thanks be Minuted and in particular, the hard work of Officers in the preparation of the Masterplan and specific reference was made to the Senior Conservation and Urban Design Officer.

RESOLVED

- (i) that the proposed amendments put forward by the Member Task and Finish Group as set out below, are agreed;

Page No.	Comment	Amendment
9	Remove graphic and text titled <i>The Maldon District Story</i> . Replace with <i>Sense of Place</i> text and an image of graphic language toolkit.	As Appendix 3 to the report.
Page 26, Paragraph 3.6 Masterplan Projects	Remove first sentence regarding ‘ <i>long, medium and short term actions</i> ’ and move to the Action Plan section on page 77.	The Action Plan section on page 77 does include reference to short, medium and long term actions. Ensure clarity of text.
Page 29, Chapter 4 Key Projects	Remove third sentence regarding <i>funding</i> and move to the Action Plan section on page 77.	The Action Plan section on page 77 does include reference to low, medium and high priority projects. Ensure clarity of text.
Pages 30 to 35 Project 1	Remove reference to ‘ <i>Core</i> ’ High Street in project title and graphics. Remove reference to ‘ <i>Central</i> ’ High Street in graphic and replace with ‘ <i>Middle</i> ’ High Street.	Replace with ‘ <i>Upper</i> ’ High Street (and in Action Plan) and ‘ <i>Middle</i> ’ High Street.
Page 41 Project 4 Para 2	Remove reference to ‘ <i>food</i> ’ and ‘ <i>seafood and oysters</i> ’ in bullet point 1.	To read: <i>Creation of a new seasonal market on Hythe Quay.</i>
Page 61 Project 12	Project 12 to include all types of flooding in the Causeway Regeneration Area and opportunity for mitigation through development proposals and funding via s106 also.	Add reference to fluvial, tidal and surface water flooding within text on page 61. Add specific reference to s106 against Project 12 in external funding section of Action Plan on page 77.
Pages 71 to 75 Projects 16, 17, 18	Acknowledging the partnerships identified, the Group requested engagement with Haven Gateway and in particular George Keiffer.	Add Haven Gateway to Action Plan ‘Partners’ for these projects and add Mr G Keiffer to Stakeholder list.

- (ii) that any additional comments raised by the Committee be included within the Draft Maldon and Heybridge Central Area Masterplan;
- (iii) that the amendments in (i) and comments in (ii) above are embedded within the Draft Maldon and Heybridge Central Area Masterplan and the document is approved for public consultation.

890. PLANNING POLICY UPDATE

The Committee considered the report of the Interim Head of Planning Services updating Members on the A12 Consultation, Maldon Design Guide and Planning Legislation

update. The report highlighted three further subjects which were the reported separately on the agenda for this meeting.

The Planning Policy Manager provided Members with an update regarding the Local Development Plan and the next steps following the conclusion of the Examination in Public, which included work requested by the Planning Inspector.

Councillor M F L Durham congratulated the Council's team of Officers involved in the Examination in Public. He proposed that the Council contact both Members of Parliament to ask them to put some pressure on the Secretary of State or appeal for him not to take too long in making a final decision. This was duly agreed. Further congratulations were offered to Mr Berkley, the Planning Inspector.

Councillor Miss M R Lewis referred to the current consultations on the A12 and A120 and urged Members to attend at least one of the consultation events planned.

RESOLVED

- (i) that the Planning Policy update outlined in the report be noted and that at the appropriate time matters for decision will be brought to this Committee for be noted;
- (ii) that the Council writes to both Members of Parliament to ask them to put some pressure on the Secretary of State or appeal for him not to take too long in making a final decision.

891. UPDATE ON THE NEIGHBOURHOOD PLAN

The Committee considered the report of the Interim Head of Planning Services providing an update on progress of Neighbourhood Plans (NP) in the District.

A table within the report provided a synopsis of the stages each of the NPs had reached. The Planning Policy Officer provided Members with a verbal update on the status of NPs including:

- Burnham-on-Crouch – Tenders for an examiner for the NP had drawn no interest. The Town Council would therefore be retendering and details of additional information being provided was given.
- Langford and Ulting – Officers advised that the Council needed to source external advice.
- Althorne – It was confirmed that their Village Design Statement had been incorporated into the NP.

The Chairman requested an update on Village Design Statements (VDS) for Parishes making specific reference to Woodham Walter. In response this request and a further question regarding VDS, the Planning Policy Officer advised that a VDS could be created by a Community / Parish Council as a way of influencing the design and new development locally through identifying and describing the local character of an area. A consultation process would take place and once completed the Community / Parish Council would submit the VDS to the Council for endorsement as a material planning

consideration and then form part of the Local Plan and used to help assess planning applications. It was confirmed that a VDS could also be incorporated into a NP.

Concern was raised regarding the status of the Langford and Ulting Neighbourhood Plan and in response the Planning Policy Officer provided Members with an update including the decision by the Parish Council to withdraw the plan (although formal withdrawal notice had not been issued), and legal advice which was being sought. It was requested that NP be given the highest of priority.

In response to a question, the Chairman agreed that clarification would be sought on whether VDSs were adopted or endorsed and the status of VDSs received by the Council.

RESOLVED that the progress of Neighbourhood Plans in the District be noted.

892. ECONOMIC DEVELOPMENT UPDATE

Councillor Mrs P A Channer advised that her earlier non-pecuniary interest related to this item of business.

The Committee considered the report of the Chief Executive providing an update on key issues relating to Economic Development. An update on the activities of various Economic Development workstreams were set out in Appendix 1 to the report.

Members' attention was to section 5 of Appendix , Magnox / Nuclear Decommissioning Authority NDA), which included a recommendation requiring endorsement from Members to support a bid to the Magnox Socio-Economic Fund for secretariat costs for the Bradwell Legacy Partnership.

The Chairman proposed a change to the recommendation set out in Appendix 1 proposing that the Chief Executive be authorised, in consultation with the Chairman and Vice-Chairman of this Committee, to submit the bid. A debate ensued.

The Economic Development Officer clarified that the £33,000 bid connected to secretariat support did not require match funding. Magnox and the Nuclear Decommissioning Agency wanted to bring to the Council's attention that other projects would require match funding.

The Chairman then withdrew her earlier proposition and put the recommendation as set out in the report which upon a vote being taken was agreed.

RESOLVED

- (i) that the contents of the report be noted;
- (ii) that the submission of a bid to the Magnox Socio-Economic Fund for £33,000 to help support Secretariat costs for the Bradwell Legacy Partnership between April 2017 and March 2019, be endorsed.

893. PROPOSAL TO ESTABLISH A STRATEGIC HOUSING BOARD

The Committee considered the report of the Director of Customers and Community seeking Members' agreement to establish a Strategic Housing Board. It was noted that the report was also being considered by the Community Services and Finance & Corporate Services Committees.

It was noted that the range of Housing issues for the Council had evolved and was not now principally concerned with Affordable Housing but were about meeting and managing all housing needs, and helping to turn Planning approvals into actual delivery. The proposal to set up of a Strategic Housing Board would assist in providing strategic direction to meet these objectives.

Councillor H M Bass proposed that Councillors E L Bamford and R G Boyce be the appointed Members from this Committee on the Strategic Housing Board. This proposal was duly seconded and upon a vote being taken agreed.

RECOMMENDED

- (i) that a Strategic Housing Board be created with Councillors E L Bamford and R G Boyce from this Committee along with two Members appointed from each of the other programme Committees with the intention that the Board would meet quarterly;
- (ii) that the Terms of Reference for the Board be agreed once established by its Members which will also include senior officers of the Council;
- (iii) that the fundamental purpose of the Strategic Housing Board would be to consider and advise relevant committees on the Council's strategic priorities and actions for meeting the need for affordable housing and the housing need of other groups in the District.

894. COASTAL COMMUNITY TEAMS

The Committee considered the report of the Director of Customers and Community providing Members with an update on the work of the Maldon & Blackwater Estuary and River Crouch Coast Community Teams (CCTs). Comments were also sought on the draft Memorandum of Understanding (MOU) between the Council and the CCTs, attached as Appendices 2a and 2b to the report.

The existing Terms of Reference for each CCT were attached at Appendix 1 to the report.

The report provided background information regarding the launch of CCTs by the Department for Communities and Local Government (DCLG) in 2015 to identify and drive projects and activities which focussed on coastal regions and have a positive and sustainable impact on the economic well-being of coastal towns and communities. The Director of Customers and Community drew Members' attention to the projects identified and it was noted that they were being processed by small working groups from each CCT.

In response to comments from the Committee, the Chairman clarified that the Monitoring Officer would be involved in the finalising of the Draft MOU and the intention of the MOUs was to make clear the governance aspect of these bodies.

The Director of Customers and Community highlighted that the MOUs were still being worked on and the Monitoring Officer was happy to receive any further comments Members may have. Councillor E L Bamford highlighted her concerns regarding the MOU, which she had sent to the Monitoring Officer.

RESOLVED

- (i) that the efforts being made by the Coastal Community Teams in supporting the ambitions of the District's Economic Prosperity Strategy, be noted;
- (ii) that Members comments on draft Memoranda of Understanding between Maldon District Council and the Maldon & Blackwater Estuary Coastal Community Team and the River Crouch Coastal Community Team, be noted;
- (iii) that the Director of Customers and Community be authorised, in consultation with the Chairman of the Planning and Licensing Committee, to finalise the Draft Memoranda of Understanding.

Councillor R G Boyce left and then re-joined the meeting during this item of business.

895. PLANNING ENFORCEMENT

This report had been withdrawn.

896. SECTION 106 PROCESS AND THE MONITORING OF AGREEMENTS AND THE COLLECTION OF PLANNING CONTRIBUTIONS

The Committee considered the report of the Interim Director of Resources presenting information about the Section 106 (S106) process in connection with planning applications, including details of monies currently held by the Council. It was noted that this report had come about following a request for information from a meeting of the Overview and Scrutiny Committee.

Appendices to the report provided the following information:

- Appendix A – payments received since 2012 with details of the date each had to be used by and the purpose for which the contribution is to be used.
- Appendix B – the position in respect of the strategic sites.
- Appendix C – sites under construction where financial contributions and / or affordable housing have been provided and sites with extant permissions and subject to a legal agreement but where work is yet to commence.

The Interim Director of Resources took Members through the report, outlining the work undertaken by the Council to monitor S106 monies including the coordination of partners to ensure monies were spent. Members were advised that a report on this matter would also be taken to the next meeting of the Overview and Scrutiny

Committee. It was noted that since August 2015 the Council had a dedicated S106 Officer.

Councillor S J Savage reiterated his earlier declaration of interest at this point.

Concern was raised regarding the process for pre-application meetings and how the process for creating such meetings appeared to have changed without any consultation with Members. The benefit of pre-application meetings was stressed along with the importance of Ward Members in such discussions. The Interim Head of Planning Services advised that at the request of the Chief Executive he was actively looking into the application of S106 along with pre-application meetings.

Councillors R G Boyce and Mrs P A Channer reiterated their earlier declaration of interests at this point.

A number of comments were raised by Members and in response Officers provided the Committee with detailed information regarding S106 monies including their use in respect of affordable housing, play and sports, highways and primary care. Members were also advised of the process if a shortfall in S106 monies occurred. The Interim Head of Planning Services advised that there was continuous ongoing monitoring of S106 monies.

It was agreed that an update would be provided to Councillor B S Beale outside of the meeting regarding on the pedestrian linkages to the Southminster West Business Park, Scotts Hill.

Councillor E L Bamford proposed that this report be brought back to the Committee on a six monthly basis. This was duly seconded and agreed.

RESOLVED

- (i) that the views and comments of Members on the information in this report, be noted;
- (ii) that an update to this report be brought back to the Committee on a six monthly basis.

897. UPDATE ON THE OPERATION OF THE BURNHAM-ON-CROUCH TOURIST INFORMATION CENTRE

The Committee received the report of the Director of Customers and Community providing a review of the first three months of operation of the Burnham-on-Crouch Tourist Information Centre (TIC).

The Burnham-on-Crouch TIC opened officially on 23 August 2016 and the report set out the current arrangements in respect of staff and opening hours. Appendix 1 to the report provided a breakdown of usage during the three month period. It was noted that key statistical information would be used to identify Key Performance Indicators. A sample of merchandise offered by the TIC was set out in Appendix 2.

RESOLVED that the contents of the report be noted.

898. UPDATE OF THE BLACKWATER ESTUARY TASK AND FINISH WORKING GROUP

The Committee received the report of the Blackwater Estuary Task and Finish Working Group (the Working Group) providing an overview of the discussion that had taken place at the most recent meeting of the Working Group.

The Group Manager (Leisure, Countryside and Tourism) provided a verbal update on establishment of the England National Coastal Footpath and confirmed that mud flats and saltmarsh would be excluded. A report would be brought to the next meeting of this Committee on this matter. It was also reported that Natural England had confirmed that position of the path.

RESOLVED that the update from the Blackwater Estuary Task and Finish Working Group be noted.

899. UPDATE OF THE ECONOMIC PROSPERITY AND TOURISM WORKING GROUP

The Committee received the report of the Economic Prosperity and Tourism Working Group (the Working Group) providing an overview of the discussions that had taken place at the most recent meeting of the Working Group.

The report provided an update on each of the following areas of work:

- Bradwell Legacy Partnership (BLP);
- Dengie Gateway Project / Coastal Communities Fund Project;
- Coastal Revival Fund (Historic Waterfront Revival Fund).

RESOLVED that the updated from the Economic Prosperity and Tourism Working Group be noted.

900. GUIDELINES FOR COMMITTEE SITE VISITS

The Committee considered the report of the Interim Head of Planning Services seeking Members' agreement to a revision to Committee Site Visit Guidelines to make provision for organised site visits prior to a Committee meeting.

Appendix 1 to the report set out the current guidelines and how, at a recent meeting of the Planning Committee Chairmen an amendment had been suggested to make provision for site visits to be organised following publication of Committee report prior to the report. The proposed amendment was set out in the report.

Some concern was raised regarding the proposed change and whether there would be sufficient time between publication of Committee reports and meetings take place to arrange site visits. The Interim Head of Planning Services provided further information in response and made reference to the need for a proactive approach and to ensure that Members were aware of all material considerations relating to a planning application.

It was clarified that site visits would not be considered until applications were validated.

Following further discussions the Chairman put to the Committee that the site visit guidelines either be brought back to the Committee for approval following amendment or the Interim Head of Planning Services be authorised to amend them in consultation with the Chairman and Vice-Chairman of the Planning and Licensing Committee and the Committee Chairmen involved at the Planning Committee Chairmen's meeting. This was duly agreed.

RESOLVED that taking in account the comments of Members the site visit guidelines either be brought back to the Committee for approval following amendment or the Interim Head of Planning Services be authorised to amend them in consultation with the Chairman and Vice-Chairman of the Planning and Licensing Committee and the Committee Chairmen involved at the Planning Committee Chairmen's meeting.

901. EXCLUSION OF THE PUBLIC AND PRESS

RESOLVED that under Section 100A (4) of the Local Government Act 1972 the public be excluded from the meeting for the following items of business on the grounds that they involve the likely disclosure of exempt information as defined in Paragraph 2, 3 and 6 of Part 1 of Schedule 12A to the Act, and that this satisfies the public interest test.

902. SUBMISSION OF A JUDICIAL REVIEW

The Committee received the report of the Interim Head of Planning Services providing information regarding an appeal decision (as received from the Planning Inspectorate) and how the decision was considered unlawful and the Council had, as a result, served a Judicial Review against the decision. The report provided detailed information regarding this appeal, including legal advice received.

The Chief Executive made reference to a letter received in respect of the judicial review and in response to questions Officers provided the Committee with additional information.

RESOLVED

- (i) that the implications of this appeal decision for strategic housing growth in be noted;
- (ii) that the timescale, costs and outcome of this Judicial Review process be noted.

903. REVIEW OF SLIPWAY AND LAUNCHING FACILITY

The Committee considered the report of the Director of Customers and Community, updating Members on river enforcement activity and highlighting associated issues.

The report advised Members of a particular issue, the history surrounding it and possible solutions. Members discussed these and where questions were raised the Group Manager (Leisure, Countryside and Tourism) provided additional information.

Councillor M F L Durham proposed that the Council set a fixed time limit by which compliance with the licence for the relevant site was required and if not met then the licence would be revoked. He suggested that the time limit be set at three months from Easter and by 1 August. This was duly agreed.

RESOLVED

- (i) that Members note the contents of the report and that attempts have been made to reduce on the water antisocial behaviour associated with some individuals launching from the site;
- (ii) that Officers continue to take steps to improve control of the facility by working with the site owners at a local and national level;
- (iii) that compliance with the licence for the site be required by 1 August after which the licence would be revoked;
- (iii) that a further report be brought back to this Committee detailing the outcome of further negotiations.

There being no further items of business the Chairman closed the meeting at 10.24 pm.

MRS P A CHANNER, CC
CHAIRMAN

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REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

*(TO BE CONSIDERED BY THE NORTH WESTERN AREA PLANNING COMMITTEE
ON 6 FEBRUARY 2017 AND THE CENTRAL AREA PLANNING COMMITTEE
ON 22 FEBRUARY 2017)*

Application Number	FUL/MAL/16/01419
Location	Vaulty Manor Goldhanger Road Heybridge Essex
Proposal	Wedding gazebo within the enclosed garden of Vaulty Manor
Applicant	Mr Colin Downie - Osea Leisure Park
Agent	Geoffrey Vale - Geoffrey Vale Chartered Architect
Date Valid	6 December 2016
Target Decision Date	7 March 2017
Case Officer	Yee Cheung, TEL: 01621 876220
Parish	GOLDHANGER
Reason for Referral to the Committee / Council	The planning application and Listed Building Consent both fall under the parishes of Goldhanger and Heybridge and therefore would need to be considered by Members at the North Western Area Planning Committee and Central Area Planning Committee.

Application Number	LBC/MAL/16/01420
Location	Vaulty Manor Goldhanger Road Heybridge Essex
Proposal	Wedding gazebo within the enclosed garden of Vaulty Manor
Applicant	Mr Colin Downie - Osea Leisure Park
Agent	Geoffrey Vale - Geoffrey Vale Chartered Architect
Date Valid	6 December 2016
Target Decision Date	31 January 2017
Case Officer	Yee Cheung, TEL: 01621 876220
Parish	GOLDHANGER
Reason for Referral to the Committee / Council	As above.

1. RECOMMENDATION

FUL/MAL/16/001419:

APPROVE subject to the conditions (as detailed in Section 9 of this report).

LBC/MAL/16/001420:

GRANT LISTED BUILDING CONSENT subject to the conditions as detailed in Section 9 of this report.

Agenda Item no. **6**

Our Vision: Working in Partnership to make the Maldon District a better place to live, work and enjoy

2. SITE MAP

Vaulty Manor, Goldhanger Road, Heybridge
FUL/MAL/16/01419



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Maldon District Council 100018588 2014



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Scale: 1:5,000

Organisation: Maldon District Council

Department: Department

Comments: C Committee 16/01419/FUL

Date: 09/02/2017

MSA Number: 100018588

Agenda Item no. **6**

3. AREA COMMITTEE CONSIDERATION

- 3.1 The North Western Area Planning Committee considered this report at its meeting on 6 February 2017 and resolved to recommend to this Committee that the recommendations as set out in this report be agreed.
- 3.2 The views of the Central Area Planning Committee on 22 February 2017 will be reported by way of an Addendum to this Committee.

4. SUMMARY

4.1 Proposal / brief overview, including any relevant background information

- 4.1.1 The application site is located in the rural area beyond the settlement boundaries for both Goldhanger and Heybridge. The site is located on the north side of Maldon Road/Goldhanger Road with the buildings set back approximately 140 metres from the highway. To the front / southern boundary of the application site there is an established hedge and some trees have been planted along the private driveway into the site. The site itself is occupied by a Grade II listed building and planning permission was granted to change the use of the existing six bedroom 'Vaulty Manor' to be used as a hotel in association with the existing wedding venue (reference: FUL/MAL/16/00026).
- 4.1.2 Planning permission is sought for the erection of a gazebo to be used in conjunction with the existing wedding venue. The gazebo would be octagon in shape and measure approximately 4 metres wide and 4 metres deep with a height of 3.9 metres to ridge level. It would be positioned approximately 12 metres further forward and 32 metres away to west from the main house.

4.2 Conclusion

- 4.2.1 The gazebo, to be used in conjunction with the existing wedding venue at Vaulty Manor is considered acceptable in principle and would not cause demonstrable harm to the character and appearance of the rural area, the residential amenity of neighbouring properties and the setting of the Grade II listed building. In this respect, the proposal would accord with policies BE1, BE16 and CC6 of the adopted local plan, policies S1, S8, D1, D3 and H4 of the Local Development Plan, and Government advice contained within the National Planning Policy Framework.

5. MAIN RELEVANT POLICIES

Members' attention is drawn to the list of background papers attached to the agenda.

5.1 National Planning Policy Framework 2012 including paragraphs:

- 7, 14, 17, 56, 57

5.2 Maldon District Replacement Local Plan 2005 – Saved Policies:

- BE1, BE16, CC6

5.3 Maldon District Local Development Plan submitted to the Secretary of State for Examination-in-Public on 25 April 2014:

- S1, S8, D1, D3, H4

5.4 Relevant Planning Guidance / Documents:

- National Planning Policy Guidance (NPPG)
- National Planning Policy Framework (NPPF)

6. MAIN CONSIDERATIONS

6.1 Principle of Development

- 6.1.1 The principle of erecting a gazebo to be used in conjunction with the established wedding venue within the house and ground is considered acceptable. There is no objection to the development proposal in principle subject to other planning considerations which will be discussed below:-

6.2 Design and Impact on the Character of the Area and the setting of the Listed Building

- 6.2.1 Policy BE1 of the adopted local plan states that development proposals will only be permitted if they are compatible with their surroundings in terms of layout, scale, bulk, height and visual impact. The gazebo would be set within the grounds of a listed building and as such policy BE16 would also apply to this case. Further, policy D3 of the Local Development Plan and the National Planning Policy Framework both provide guidance on heritage assets. Policy BE16 states that ‘extensions, alterations to, and additional buildings in the curtilage of listed buildings’ will only be permitted if the proposal would not harm the building, its setting and any features that contribute to its special architectural or historic interest.
- 6.2.2 The development proposes to construct a gazebo to be used for wedding purposes in conjunction with the main house. The gazebo would be octagon in shape and of a simple design. It is modest in scale, featuring external materials such as wood composite for the decking and painted timber posts supporting the cedar shingles to form the roof. The gazebo would be positioned approximately 35 metres away from west of the main house and therefore would not have material impact on the principle building which is Grade II listed. Whilst it is noted that the gazebo would be set 12 metres further forward than the house, it would still be screened by existing line of trees and therefore cannot be seen from the public domain. The structure is considered to be appropriate and sympathetic to the rural setting in accordance with Policies BE1, BE16 and CC6 of the adopted local plan and Policies S1, S8, D1 and D3 of the Local Development Plan. The Conservation Officer has raised no objection to the proposal.

6.3 Impact on Residential Amenity

- 6.3.1 Policy BE1 of the adopted local plan seeks to protect neighbouring occupiers from development which would be detrimental to their amenities by reason of overlooking, overpowering and loss of light. The nearest dwellings are No. 1 and 2 Vaulty Manor

Cottages located approximately 190 metres away to the south east of the site with the Barrow Marsh Caravan Park located some 170 metres to the south west. Having considered the scale and nature of work proposed, it is not considered the gazebo would have an adverse impact on the amenity of the occupiers at the named cottages or the occupiers at the caravan park to warrant refusal.

- 6.3.2 Whilst it is acknowledged that the gazebo, to be used for wedding purposes, may result in some noise and disturbance i.e. with music being played from the structure, it is important to note that there is currently no restriction within the grounds of the house to erect a temporary structure such as a tent for a day with music being played within. As such, it is considered the gazebo would not significantly harm the residential amenity of the above residential properties to justify refusal of this application.

- 5.3.4 Having checked the planning history for the site, it appears that no restriction has been placed on the house and grounds to facilitate functions, parties and weddings, it is therefore considered to impose a condition restricting music being played during wedding ceremonies within the gazebo would be unreasonable.

6.4 Access, Parking and Highway Safety

- 6.4.1 The construction of a gazebo would not reduce the level of car parking spaces that are currently available within the grounds of Vaulty Manor. The proposed gazebo would not result in additional visitors on top of the existing facility to such a degree as to result in any demonstrable increase in on-site parking. In this respect, the proposal would accord with policy T8 of the adopted local plan and policy T2 of the Local Development Plan.

6.5 Landscaping

- 6.5.1 The proposed gazebo positioned on a lawn area and would be screened by existing soft landscaping. In preparation for wedding ceremonies, temporary decorations such as flower displays, mats and seats would be laid out for the people attending the wedding. As these features would be temporary, it is considered that it would not harm the character and appearance of the area and the setting of the listed building.

5.6 Other Material Considerations

- 5.6.1 The application site is located on the boarder of Flood Zones 2 and 3. The site area is approximately 2.6 hectares and as such the Environment Agency, Essex County Council Sustainable Urban Drainage (ECC SUDs) Team and the Emergency Planner were consulted regarding the development proposal. No objection has been raised by the Environment Agency and the ECC SUDs Team. The Emergency Planner has raised no objection to the proposal subject to a condition to be imposed for a flood water evacuation plan to be presented on site. This can be dealt with by a planning condition.

7. ANY RELEVANT SITE HISTORY

- **FUL/MAL/01/00105** - Proposed additional banqueting hall and covered terrace alongside the existing banqueting hall. Approved 28.03.2001

- **LBC/MAL/01/00106** - Proposed additional banqueting hall and covered terrace alongside the existing banqueting hall.
- **FUL/MAL/02/00201** - Convert covered terrace approved under refs: FUL/MAL/01/00105 and LBC/MAL/01/00106 to extensions of banquet hall. Approved 15.04.2002
- **LBC/MAL/02/00202** - Application for listed building consent to Convert covered terrace approved under refs: FUL/MAL/01/00105 and LBC/MAL/01/00106 to extensions of banquet hall. Approved 15.04.2002.
- **FUL/MAL/02/00849** - Erection of guest accommodation rooms. Approved 12.11.2002
- **FUL/MAL/03/01220** - Extend existing barn currently used for wedding ceremonies to provide additional area for seating, disabled WC, and improved WC facilities. Approved 14.01.2004
- **LBC/MAL/03/01221** - Extend existing barn currently used for wedding ceremonies to provide additional area for seating, disabled WC, and improved WC facilities. Approved 14.01.2004
- **FUL/MAL/05/00666** - Extension to Banqueting Hall. Approved 07.02.2006
- **FUL/MAL/16/00026** - Change of use (of the house on the estate site) from (C3) private dwelling house to (C1) hotel/guest house. Approved: 10.05.2016
- **LBCMAL/16/00027** - Minor alterations to first floor interior bedroom and en-suite arrangements. Granted Listed Building Consent 08.03.2016.

8. **CONSULTATIONS AND REPRESENTATIONS RECEIVED**

8.1 **Representations received from Parish / Town Councils**

Name of Parish / Town Council	Comment	Officer Response
Goldhanger Parish Council	Supports the FUL and LBC applications	Acknowledged
Heybridge Parish Council	To grant planning permission and Listed Building Consent	Acknowledged

8.2 **Statutory Consultees and Other Organisations (*summarised*)**

Name of Statutory Consultee / Other Organisation	Comment	Officer Response
Environment Agency (EA)	The site is in an area of flood risk. EA has advised that in managing flood risk, is for the Council to consult the Emergency Planner when making decisions.	Noted

Name of Statutory Consultee / Other Organisation	Comment	Officer Response
Emergency Planner	No objection subject to a condition for a flood water evacuation plan to be presented on site	Noted
Sustainably Urban Drainage (SUDs) Team	The scheme is unlikely to have much of an impact on surface water drainage.	Noted
Essex County Council (ECC) Highways	No objection	Noted

8.3 Internal Consultees (*summarised*)

Name of Internal Consultee	Comment	Officer Response
Conservation Officer	No objection to the proposed gazebo. It will cause no harm to the setting or significance of the Grade II listed Vaulty Manor.	This has been addressed in Paragraph 5.2.2.

8.4 Representations received from Interested Parties (*summarised*)

8.4.1 No letters of representation at the time of writing this report.

9. **PROPOSED CONDITIONS**

FUL/MAL/16/01419:

- 1 The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
REASON To comply with Section 91(1) of The Town & Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
- 2 The development hereby permitted shall be carried out in complete accordance with the approved drawings specifically referenced on this decision notice.
REASON To ensure that the development is carried out in accordance with the details as approved.
- 3 No development shall take place until written details or samples of all materials to be used in the construction of the external surfaces of the development hereby permitted have been submitted to and approved in writing by the local planning authority. The development shall be carried out using the materials and details as approved.
REASON To ensure that external materials for the construction of the development is appropriate to the rural area in accordance with policies BE1 and CC6 of the Maldon District Replacement Local Plan, policies S1, S8 and

- D1 of the Maldon District Local Development Plan, and Government guidance contained within the National Planning Policy Framework.
- 4 Prior to first use of the gazebo hereby permitted, weatherproof signs shall be displayed within the gazebo, which comprehensively describe the site evacuation procedures in the event of a flood. The signage shall thereafter be retained.
- REASON: To ensure that all users of the development are aware that the site is at risk of flooding in accordance with policy D5 of the Maldon District Local Development Plan, and government advice contained within the National Planning Policy Framework and Planning Practice Guidance.

LBC/MAL/16/01420:

- 1 The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
- REASON To comply with Section 18(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 (as amended).
- 2 The development hereby permitted shall be carried out in complete accordance with the approved drawings specifically referenced on this decision notice.
- REASON To ensure that the development is carried out in accordance with the details as approved.



REPORT of CHIEF EXECUTIVE

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

ECONOMIC DEVELOPMENT UPDATE

1. PURPOSE OF THE REPORT

- 1.1 Economic Development works towards strengthening and encouraging development of the local economy. It was agreed that a regular report would be submitted to this Committee to update Members on key issues and provide an opportunity for feedback and input.

2. RECOMMENDATIONS

- (i) that Members note the contents of the report;

To the Council:

- (ii) that the Council provide in principle support to the Heritage Marine Foundation's maritime apprenticeship project, subject to all necessary planning and environmental requirements being met.

3. SUMMARY OF KEY ISSUES

- 3.1 **APPENDIX 1** to the report provides an update on activity on a various economic development workstreams as follows:

- Coastal Community Teams (CCT) – Members' attention is drawn to the recommendation detailed within this section of update.
- Business Support
- Business Engagement
- Bradwell Legacy Partnership (BLP)
- Magnox/Nuclear Decommissioning Authority (NDA)
- Enterprise Centre for the Maldon District
- Sense of Place (SOP)
- Skills
- Economic Regeneration
- Industrial Strategy

- Essex Economic Commission.

3.2 Members' attention is drawn to Section 1 of **APPENDIX 1** which seeks the Council's support to the Heritage Marine Foundation's maritime apprenticeship project, subject to all necessary planning and environmental requirements being met.

4. IMPACT ON CORPORATE GOALS

4.1 The work of the Economic Development Team in partnership with key stakeholders will help to create opportunities for economic growth and prosperity.

5. IMPLICATIONS

- (i) **Impact on Customers** – Engagement with businesses and organisations across the district as well as supporting the District at sub-region level.
- (ii) **Impact on Equalities** – None identified.
- (iii) **Impact on Risk** – Work of the Economic Development Team helps to mitigate against Corporate Risk 11 – Failure of the Council to influence partners and support / encourage economic prosperity.
- (iv) **Impact on Resources (financial)** – The Team are being proactive in seeking funding to create opportunities for economic prosperity in our District.
- (v) **Impact on Resources (human)** – The proposed bid to source funding to help support Secretariat costs for the Bradwell Legacy Partnership will deliver a continuation of the resource to focus on delivery of the projects set out in the Bradwell Legacy Partnership and Coastal Communities Plan.
- (vi) **Impact on the Environment** – None identified.

Background Papers: None.

Enquiries to: Heidi Turnbull, Economic Development Officer, (Tel: 01621 876215).

Economic Development Team Update

1. COASTAL COMMUNITY TEAMS (CCT)

- 1.1 Maldon and Blackwater Estuary CCT. The most recent meeting was held on 19th January 2017.

Officers updated the meeting on progress with the Historic Water Revival Project, Hythe Quay, Maldon and arrangements for public consultation on a number of proposals to improve the District's tourism offer.

The meeting also discussed the CCT project list and it was agreed to include Heybridge Basin Public Conveniences on the project list of ideas.

- 1.2 A representative from Heritage Marine Foundation, which had been developed from the Cutty Sark project, gave a presentation on a proposed maritime apprenticeship programme to be based in Maldon. The Foundation plan to establish a recognised training scheme, in association with Colchester Institute, that encompasses all aspects of traditional vessel conservation, maintenance and operation, with the emphasis on "Preservation by Operation" taking the Thames Sailing Barges and East Coast Smacks as a prime example.

They intend to develop a three year apprenticeship where each apprentice will spend 18 months at sea, in various vessels and 18 months working in a traditional ship/boatyard environment, as well as attending the appropriate "Standards of Training, Certification and Watchkeeping for Seafarers" (STCW) and other required courses. It is intended that completion of the training period will give the apprentices a good chance at either a job at sea on a traditional vessel or ashore in operation/conservation.. It is intended that training will be provided from a classroom to be situated out of Down's Road Boat Yard. The Foundation is seeking the support of Colchester Institute with a view to them becoming the course provider. They have submitted an initial funding enquiry to Heritage Lottery Fund and are exploring other revenue streams.

This project has the potential to link with the proposed LIBOR project that is mentioned in more detail below.

The Foundation has requested that the Council endorse the project, as an opportunity to meet a strategic need to support maritime skills provision.

This proposal supports **Action 3.3 – Promote Apprenticeships and Pre-Apprenticeship Programmes set out in our Economic Prosperity Strategy** - *The Council will work with businesses and a range of partners to expand the breadth of apprenticeship opportunities on offer in the District and increase uptake of these opportunities through promotional activity.*

It is **RECOMMENDED** that the Council provide in principle support to the Heritage Marine Foundation's maritime apprenticeship project, subject to all necessary planning and environmental requirements being met.

- 1.3 The next meeting of the Maldon and Blackwater Estuary CCT will be the annual General meeting on Friday 5 May 2017.

- 1.4 The River Crouch Management Team met on 17th January 2017 and agreed to progress a reprint of the popular River Crouch Guide. The RSPB, Maldon, Rochford and Chelmsford City Councils agreed to contribute £200 each to the cost of the reprint, and the Team will be seeking sponsorship for the balance of c.£1,200 from attractions and parishes featured in the document. A copy of the current guide can be found at <http://mediafiles.thedms.co.uk/Publication/EE-MDCW/cms/pdf/CrouchLeafletForWebLoRes.pdf>

Following discussion and feedback from the CCT, the Management team agreed to include a number of new project ideas in their work plan as follows:

- Crouch CCT website development
- Newsletter & marketing
- Burnham week
- Coast Path landowner engagement

The next meeting of the River Crouch CCT will be the AGM which is being held on Monday 27th March 2017.

- 1.5 A copy of the updated Project List for both CCTs is attached to this report for members' information.

2. BUSINESS SUPPORT

2.1 Dengie Enterprise Support (DES)

With the current contract for this service coming to an end at the end of March 2017 the Economic Development Team are currently seeking funding from the Magnox Socio Economic Grant Fund to continue the service for at least a further 2 years. Since its inception DES has assisted in the start-up of 21 new businesses which is estimated to have created 43 jobs. The bid to extend the services will include an additional day in Maldon with a view to increasing business advice services in the Maldon District. The service also provides a regular monthly forum for clients where they meet for a workshop on a themed subject, currently relating to sales and marketing, and compare experiences and problems on an informal networking basis. The forum has an active membership of 23.

In addition to the funded activities DES has also been assisting in the re-launch of the Burnham Chamber of Commerce, exhibited at the BEST Growth Hub Business Showcase event, and sit on the River Crouch Coastal Communities Team management group and the Bradwell Legacy Partnership.

At the request of the BLP and Maldon District Council, Dengie Enterprise Support has also been providing assistance to a local group of wine growers who are considering forming the Crouch Valley Wine Growers Association. This will promote and create awareness of the area as a wine producing region. Longer term goals include running tours of the vineyards, developing co-operation in joint training schemes, wine festivals and other activities with brewers and food producers of the District.

2.2 BEST Growth Hub

Training Workshops - The Economic Development Team are currently promoting a series of fully funded training workshops available to Maldon District businesses. The workshops are commissioned by BEST Growth Hub and are being delivered online and face-to-face across Essex including in the Maldon District from January-March 2017.

BEST Growth Hub Drop in Clinic - The Economic Development Team have arranged for drop-in clinics to now take place on a regular basis in the Maldon District every 6 weeks, with the next clinic taking place on Monday 20th March 2017 at Maldon Library. This will be from 10-4pm and will be a mixture of booked appointments and drop-ins.

Local Engagement Plan – Maldon District Council are currently putting together a local engagement plan with BEST Growth Hub to see how best this government initiative can complement those services currently delivered by Dengie Enterprise Support to provide Maldon District businesses with the best possible business support and create additional business support activities.

3. BUSINESS ENGAGEMENT

- 3.1 The Economic Department Team continues to attend and support local business networking and support groups including Burnham-on-Crouch (as Executive member, attending next meeting which takes place on 6th February, to discuss further its re-launch on April 1, 2017), Maldon Small Business Networking Lunch (next meeting 8th February) and the Local Clinic (next meeting Tuesday 7 February).

3.2 Events

a. Maldon District Council Chairman's Business Awards

Judging of the 20 entries received has now been completed and Members and all entrants have been invited to an Afternoon Tea Event which is to take place on Friday 24 February 2017 between 3 and 5 pm at the Royal Corinthian Yacht Club, Burnham-on-Crouch to announce the winners.

b. BEST Essex Business Show

The Economic Development Team were represented on a stand at this event which took place on 16 February 2017 at Hylands House, Chelmsford and assisted in its promotion prior to the event. 320 people attended the show and support was provided to 7 Maldon Businesses while they were there.

It was also a good opportunity to network with other Essex Authorities and potential partners in the Business Support sector.

4. BRADWELL LEGACY PARTNERSHIP (BLP)

- 4.1 The Bradwell Legacy Partnership met at Newhall Vineyards on the 25th January 2017 to discuss and progress project ideas recently supported by the Planning & Licensing Committee. The agenda provided an opportunity for the Partnership to discuss details of the Wine Region project with local wine growers, with a view to using their proposals to support a formal bid to the Magnox Socio-Economic Fund.

The Magnox representative at the meeting commented that the fund excluded bids for marketing activity, and that the wine growers would need to become a formal association if they wanted to submit a bid to the Fund for their proposed project activity. However, a bid from the District Council to support a food and drink festival would be considered, as would bids from community groups.

RGE comments.....

We understand that Magnox are always pleased to support community groups to organise events which help to build sustainable communities. Grants of up to £1k are available for small projects neighbouring Magnox sites. Grants of over £10k are available to support large projects which make a significant contribution towards mitigating the impact of decommissioning.

Note: We are now working with and advising the wine growers (wine region project) to pursue the EAFRD (European Agricultural Fund for Rural Development) which makes particular reference to this industry in its call for bids, as per below:

EAFRD rolling call - Food Processing

Funding available: Up to £3.7m is being made available under this Call. Grants of between £50,000 and £1m are available to good quality eligible projects in the SELEP area.

Priority will be given to projects that:

- 1. adopt new techniques and processes*
- 2. create new "local food" products and/or involve the processing of local food*
- 3. open new product markets to the business including new products in new market categories*
- 4. **increase the productivity of the emerging wine industry***

Priority will also be given to projects from micro and small breweries based in rural areas increasing their productivity.

Projects can submit an Expression of Interest at any time before the closing date.

Projects will be expected to make a contribution to both national and local priorities.

Deadline: 31 January 2018

Further work is now required to work with the project sponsor to review options to develop and progress the wine region project, which has the potential to bring significant benefits to the locality.

- 4.2 The Partnership also received progress updates on the work of the Dengie Enterprise Support (DES) service, which included a detailed update on new business start-ups achieved by DES, and from Officers on the recent Economic Impact of Tourism study for 2015. Jobcentre Plus reported that the key employment sector for them was construction and that they were experiencing significant growth in demand for skilled labour. Care was also a significant growth sector as far as local job opportunities were concerned.
- 4.3 During the meeting the Infrastructure Manager at Bradwell Power Station stated that the Bradwell site would move into the Care & Maintenance phase during November 2018. Dengie Enterprise Support was to be invited to meet with staff at the site during the coming months to provide business start-up advice. Magnox confirmed their intention to retain some of the existing car park covering, along with the river walk. However, the nature trial would need to be transferred to another body.

- 4.4 Officers are now progressing formal bids, as previously reported to the Planning and Licensing Committee, to the Magnox Socio-Economic Fund, and seeking match funding opportunities with partners from the public and private sectors.

5. MAGNOX / NUCLEAR DECOMMISSIONING AUTHORITY (NDA)

- 5.1 Individual Bradwell Legacy Partnership project bids continue to be worked up by the Economic Development team in conjunction with Dan Davies from Essex County Council's Funding Team and as information becomes available for each they will be submitted as soon as possible this year.
- 5.2 The Secretariat costs bid of £33k in relation to the Bradwell Legacy Partnership between 2017 and 2019 has now been submitted for consideration to Magnox.

6. ENTERPRISE CENTRE FOR THE MALDON DISTRICT

- 6.1 Investigation continues in relation to the feasibility of an Enterprise Centre in the Maldon District and meetings are currently being held by Officers with stakeholders to establish how to unify various proposals, in terms of its purpose, funding and resource.

External funding is being sought to commission further and detailed works on preparation of a business plan for the proposed centre. This will provide recommendations for service model, location, management arrangements and will assess financial viability. Officers have met with representatives from the Nuclear Decommissioning Authority (NDA) and Magnox, with a view to securing grant funding for the project through the Socio-Economic Fund.

7. SENSE OF PLACE (SOP)

- 7.1 MDC Officers are periodically meeting with the Place Board Chairman to update on Council activities and seek to ensure they work in a coordinated way.
- 7.2 The Maldon District Council Communications Team in consultation with the Economic Development and Tourism Teams are currently developing some Sense of Place promotion aids that are complementary to business brands, quick and easy to implement and at little or no cost to the business. These will be delivered in a phased approach, with the initial applications to include a logo, with short and simple guidelines for use; templates for headed paper, reverse of business cards and adverts. This should be available by the end of February 2017. A standalone Sense of Place website is also currently being developed with its own branding and functionality which will link to the corporate website.

Phase 2 will be implemented once Maldon District Council has its own marketing strategy agreed. It could include window stickers for business windows and cars provided by the Council subject to available funding.

Phase 3 will be developed dependent on feedback from previous applications.

The ED Team continue to promote the use of the brand through use of bunting, banners and SOP photography mount boards at local events and meetings. The ED Team are promoting awareness and encouraging adoption of the Sense of Place graphic language at network events and meetings with businesses.

8. SKILLS

8.1 Employers Apprenticeship Event

The Economic Development Team is organising an Apprenticeship Event in co-ordination with the 3 local Secondary Schools (Plume Academy, Ormiston Rivers Academy and the Heybridge Alternative Provision School) and Directions IAG. The aim is to increase the recruitment of apprentices from the local schools by local business. The event will be delivered in 3 parts, one each for students, parents and employers and will take place from 2 -7pm on Tuesday 28 March 2017 at Plume Academy. Presentations planned for the employer's section are:

- An independent overview of the recruitment process including the Apprenticeship Levy.
- Maldon District Council's experience.
- A local small business's experience.

50 training providers that deliver training in the Maldon District are being invited to apply for stands. This will include those that deliver training for the Care and Maritime sector, which are currently of particular focus for the Maldon District.

8.2 Essex Employment and Skills Board Education and Industry STEM Programme and CEC - Greater Essex Enterprise Adviser Network.

The Economic Development Team is continuing to work with Essex County Council Employability & Skills Unit to facilitate Maldon District industry links with the 3 local secondary schools through the STEM Project and the Enterprise Adviser Network (EAN). Blackman & White have committed to work with Plume Academy. A meeting between Senior Managers of both organisations is due to take place shortly to plan the way forward. Heybridge Alternative Provision School is also committed to the Enterprise Advisor Network and is interested in becoming engaged in the STEM programme whilst Ormiston Rivers Academy has now agreed to discuss further the STEM programme with its organisers.

8.3 Care

- a. Centre for Excellence in Care – investigation is currently being carried out in relation to the development of a business case to deliver the vision for a Centre of Excellence in Care Training in the Maldon District.

The Economic Development Team is also in discussion with Anglia Ruskin University to see if a meeting can be arranged to establish whether they are able to partner with Maldon District Council on this proposal.

- b. Essex Skills Board Care (ESB) Action Group Activities - Maldon DC Economic Development Team is currently working with the ESB Care Action Group to deliver a Gift of Care Campaign event late February/early March in the Maldon District. It will give local residents the opportunity to meet care organisations who are actively recruiting in Maldon. Employers attending the recruitment events will be available to provide information on current and future job opportunities in the District and 1:1 information and advice on working within the care sector. The Economic Development Team is assisting in spreading the word in conjunction with Maldon District Council Communications Team.

- c. Care Sector Return Worker Pilot - This is an Essex County Council pilot seeking to address some of the emerging pressures in the care sector from an economic growth perspective and how it could support the development of providers in the sector.

One of the main areas identified was that of workers who may have previously had careers in care and may be seeking a route back into the sector. This is likely to be on a self-employed basis to enable a greater level of flexibility to fit around other commitments.

The project brief for this pilot exercise is to pull together a cohort of participants to receive training to give them the support and tools to enable them to develop their own business to serve a small number of clients. The Eastern Enterprise Hub has been appointed to undertake the work.

Essex County Council is currently pulling together a cohort of circa 20 participants to undertake a training programme that will commence from February 8th 2017. It will focus on the Colchester, Maldon and Tendring local authority areas.

The training will consist of a number of modules that will provide information on the skills to enable people to start their own businesses, but will also provide an update on the current requirements and practices in the sector. This programme will be completed in April 2017.

Again the Maldon Economic Development Team in conjunction with Maldon District Council Communications Team is assisting in promoting this initiative across the Maldon District.

- d. SEAS 2 Grow Project

Researchers at Anglia Ruskin are involved in a cross-EU project called SEAS (Silver Economy Accelerating Strategies) 2 Grow, which aims to speed up the development of innovative products and services to improve the health and wellbeing of older people and ease the pressure on healthcare providers as well as promoting independence, health and wellbeing among older people within their own homes.

The Leader of Maldon District Council and the Economic Development Officer attended the SEAS 2 Grow 'Realising the Potential of the Silver Economy Workshop' on Monday 16th January, at Anglia Ruskin University.

Areas that were being looked at during the workshop were the prioritised age-related needs or demands of those organisations present, what innovative products or services are the best way to meet these needs and what obstacles or hurdles stand in the way of an organisation's ability to take these ideas forward.

As well as break-out sessions for discussions on these subjects there were various speakers connected to the SEAS 2 Grow Silver Economy Accelerator programme representing various aspects of the project, providing an overview of the project and including topics such as

- Smart Technology for the elderly
- Ways of working together to advance the way we care

- How to combine public and private sector funds to overcome the challenges of innovation and adoption in the Silver economy
- User needs and how they are embedded within the design and development of smart technologies
- Better apps for better health outcomes

The Economic Development Team has expressed its interest in being involved with the programme in relation to identifying potential technology companies in the District that may wish to be engaged in the technological innovation of the project as well as promoting the opportunity for elderly representatives of its community to become involved in the testing of the innovative products.

8.4 The Maritime Sector

With our growing maritime sector in mind, meetings are currently taking place involving Members of the Planning & Licensing Committee, Officers from the Economic Development Team, representatives from the Coastal Community Teams, local stakeholders and other potential external working partners, with a clear focus on addressing the District's skills training needs in the growing maritime sector.

- a. The meeting held with the Chief Executive Officer of Thames Skills Academy, <http://www.thamesacademy.london/> a subscription based organisation held on the 27th March 2017 at Maldon, identified a number of clear linkages with local skills training needs of young people and adults. The Chairman of Maldon and Blackwater Estuary CCT agreed to share contact details with local businesses and CCT members, and Officers agreed to inform them of any future organised meetings with the District's maritime sector.
- b. A meeting with the Chief Executive at National Maritime, Peter Green, <http://www.nmdg.co.uk/blog/making-the-maritimeconnection> has also been arranged for **Tuesday 7th April 2017**, with a view to engaging their services in developing the maritime skills project. Maldon District Council is a member of National Maritime and participates in meetings of their Blue Council. National Maritime promotes and develops excellence in all activities related to business, skills and education in the maritime sector.
- c. A suggestion for the District Council to facilitate a meeting of District businesses and stakeholders which are wholly or significantly reliant on our rivers and associated activities for income is now being progressed by Officers. The proposed 'Maldon District Maritime Summit - 2017' would seek to identify the skills and other business challenges being experienced by our employers, investors and stakeholders, and identify possible solutions through partnership working.
- d. The Economic Development Unit currently maintains a database containing some 165 contact details of businesses and organisations who have a keen interest in the economic vitality and sustainability of the maritime sector. The event, which will be held during May 2017, will have a keynote speaker to set the scene, and would seek to engage attendees in open discussion and debate on their key challenges. The meeting will capture headline challenges which will be summarised and addressed through an Action Plan developed during the workshop sessions. It is hoped that the meeting outcome will be a District approach to tackling identified skills deficit issues in the maritime sector, and other perceived barriers impacting on business growth.

- e. Officers would very much welcome Members' ideas on a potential location to hold the Summit, and thoughts on a keynote speaker and other potential attendees.

8.5 Building Better Opportunities

Maldon District Council Economic Development Team are a working delivery partner with Abberton Rural Training on this ESF funded initiative for 45 year+ unemployed in Maldon to get them into employment, training or the voluntary sector. After successfully getting through to Phase 2 of the bid and providing additional evidence and answers to further questions we are currently awaiting the final decision. We are not however competing against any other bidder for this project and we should know mid February 2017 if we have been successful.

9. ECONOMIC RE-GENERATION

The Council has been approached with an idea to develop a scheme to support ex service personnel (Blue Light and Military) to re-integrate into civilian life. The proposal would be to create a centre in which ex service personnel would gain valuable skills which could further future employment. Part of the scheme would include social facilities and it would be necessary to create family residential accommodation for those attending the centre.

The proposal links closely with objectives within the Maldon & Heybridge Central Master Plan area and may kick start some of the desired regeneration within the area. Officers are looking into potential partners for the scheme as well as funding sources. The LIBOR fund has been identified as a possible major funder for this scheme, the project sets out to provide a 'soft landing' for ex-service personnel. Maldon's geographic location near to the garrison Town of Colchester makes Maldon an ideal location for addressing this need. Further updates will be given to this Committee when the concept has been developed further and a business case emerges.

10. INDUSTRIAL STRATEGY

The Government has recently published its Industrial Strategy Green Paper, outlining the broad measures which it said would help 'build a stronger, fairer Britain'. The paper sets out 10 pillars of a modern industrial strategy, including 'developing skills', with an emphasis on technical education, and 'driving growth across the whole country'. The LGA have published [a briefing](#) summarising the main announcements in the Industrial Strategy of relevance to local government, and will be submitting a formal response to the consultation, which is open until 17 April 2017. This is attached as a background paper to the report.

11. ESSEX ECONOMIC COMMISSION

The Essex Economic Commission is a new advisory body set up to help shape the Essex Economy. Local Authorities across Greater Essex have an ambition to become one of the strongest and fastest growing economies in the Country. Dr Andrew Sentance CBE was invited by local authority leaders to lead the independent Economic Commission that would provide advice and analysis of over three years, following its launch in May 2016.

The Commission will provide analysis that is objective, independent and evidence based. It will seek to inform and influence investments that affect Greater Essex, whether these are made at a local, national or European level.

The Commission has recently launched its first report, Enterprising Essex: Challenges and Opportunities. This report aims to develop a comprehensive analysis of the Greater Essex economy. Key challenges identified within the report include:-

The key challenges include:

- Raising skills and qualifications.
- Developing opportunity sectors and technologies.
- Improving transport Infrastructure.
- Expanding availability of suitable workspace and commercial premises.
- Supporting coastal districts.

The Commission will be reviewing how best these challenges can be addressed, taking into account initiatives that are already in place.

The full report is also available on the Commission's website
http://www.essexgrowth.co.uk/Portals/70/Essex_Economic_Report.pdf



**REPORT of
DIRECTOR OF CUSTOMERS AND COMMUNITY**

**to
COMMUNITY SERVICES COMMITTEE
28 FEBRUARY 2017**

AND

**PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

COMMUNITY LED HOUSING PROGRAMME

1. PURPOSE OF THE REPORT

- 1.1 To present to Members proposals for a Community Led Housing Programme for the District.

2. RECOMMENDATIONS

- (i) that the draft programme for the Council's Community Led Housing (CLH) as detailed in **APPENDIX 1**, be approved;
- (ii) that a seminar be held for community groups to promote the concept of CLH and consult on the Council's draft programme.

3. SUMMARY OF KEY ISSUES

- 3.1 The Department for Communities and Local Government (DCLG) funding has been provided to the Council to support the implementation of a programme of Community Led Housing (CLH) in the District. CLH can be defined as a way for local communities to provide good quality housing to help certain groups in their community, for example older people, young families and others who are in need of affordable housing. Although this only accounts for a very small proportion of housing development, the benefits it can provide are often seen as being much greater than more conventional methods of development. Those benefits include a stronger link between new development and local housing need; greater community involvement in planning local development; improved options to support local businesses and provide opportunities for training and skills; and an opportunity to strengthen community cohesion.
- 3.2 In recognition of the need to improve the supply of affordable homes and the additional benefits of doing this with the support and involvement of local community groups, the government has launched a funding programme to encourage local housing authorities to work with local communities to meet local housing need.

- 3.3 As an authority that has often had only a small number of new homes and therefore unable to rely on planning gain to provide a consistent supply of new affordable homes, the Council is already experienced in working with local communities and other partners to use innovation and other options to meet local housing need, such as:
- Rural exception schemes;
 - Securing new homes to meet identified need within rural areas;
 - Working with local communities to help identify the need for affordable housing and homes for older people;
 - Facilitating events to help promote options for identifying and responding to housing need;
 - Working with housing associations to acquire properties that may become vacant;
 - Tackling empty homes – both privately owned and owned by housing associations;
 - Supporting smaller local housing charities to gain access to new developments;
 - Urban exception sites to meet the need for affordable homes;
 - Using funding from other sources to meet local need.
- 3.4 These are all options that the government suggest could help local communities meet local housing need, accepting that each community will have different requirements, interests and priorities. This variety is something that the Council's Housing Service has provided help and support within the past and with the right capacity could expand to meet the government's objectives for CLH.
- 3.5 This lends itself to developing a programme that can respond to a variety of possible options for local communities to meet local housing needs. This also complements the need of the Council to retain a programme of housing development to address any residual shortfall of affordable housing outside of that planned and expected windfall sites.
- 3.6 **APPENDIX 1** provides an indication of actions and activities that could be funded through the CLH funding provided by the DCLG to deliver these shared aims, with the primary objective of building up a framework that can support a variety of different options. Most of these actions would be funded from the CLH grant with the intention of ensuring there also remains some funding to meet the cost of more detailed level of working on individual projects as they come forward.
- 3.7 The funding can be used for either capital or revenue funding in the current year and 2017 / 18. It is proposed that it is too early at this stage to commit specific amounts to anything other than capacity building as local organisations may have their own resources to contribute towards the cost of future development, expanding the potential for further projects in the future. The Council also has other resources that could be used to support Community Led Development such as commuted sums paid to provide affordable housing and the CLH funding helps ensure there is capacity to effectively prioritise and allocate other sources of capital funding to the most appropriate schemes.

- 3.8 It is proposed that once the concept and potential of Community Led Housing is explained to local groups, and a framework for delivery is in place, the second half of the next financial year can focus on progressing with local schemes. By this stage, there will be greater clarity as to the need for further resources to support tasks such as identifying particular housing need, outline assessment of financial viability and other technical services. Some of this can be met from the current allocation of funding which it is proposed to hold in reserve for these reasons. It will also help indicate the need for additional funding for the allocation of CLH funding in future years.
- 3.9 The Council may also wish to consider, as it widens the scale of its work in supporting a potentially larger number of strategic developments than it has undertaken before, the benefits of creating a more formal role for itself as a developing organisation which could also provide a source of income generation in the future. The government guidance refers to the possibility of a ‘local enabling and support vehicle so that your use of funding creates a legacy of delivery capacity within your authority and in the local community’ (see DCLG’s guidance on Community Led Housing provided as a background paper). It is also therefore proposed that a proportion of funding could be allocated towards exploring and investigating this as a longer term project to help deliver CLH in the longer term.

4. CONCLUSION

- 4.1 The national programme of Community Led Housing fits well with the Council’s previous activities as a local housing authority that has worked innovatively with local organisations to meet local housing need and the need to expand this to meet a growing need and interest for affordable housing within local communities throughout the District. Most of the proposed actions reflect this existing level of knowledge and expertise, combined with experience of what has worked in the past.
- 4.2 The net shortfall between anticipated supply of affordable homes through the Local Development Plan (LDP) and the Council’s requirement is about 450, or 30 additional homes a year. Some of this may be delivered through other means but a target of at least half, 225 homes equating to 15 a year, to be delivered through various forms of Community Led Housing would be a stretching target that would demonstrate to partners and the government the Council’s commitment to this initiative.
- 4.3 The first stage is to build up capacity and understanding so that the Council is using the CLH fund and other resources fairly and effectively, with some provision made to help launch individual projects in the second half of 2017 / 18. Consultation and promotion of the programme would begin with a seminar for local groups which would help provide more detail on the possible opportunities in the District for CLH and how this can link with existing resources and support future applications for funding.
- 4.4 The recently established Strategic Housing Board would provide a framework for more detailed planning and monitoring by Members and Officers.

5. IMPACT ON CORPORATE GOALS

- 5.1 Strengthening communities to be safe, active and healthy – Community Led Housing is intended to strengthen local communities.
- 5.2 Protecting and shaping the District – Community Led Development is intended to reflect local need and provide more sustainable communities.
- 5.3 Creating opportunities for economic growth and prosperity – Community Led Development is seen as a way of supporting local economies, including the potential to work with other local agencies and businesses providing services, skills and training.
- 5.4 Delivering good quality, cost effective and valued services – Community Led Development is intended to be cost effective and provide additional benefits and value to local communities.
- 5.5 Focusing on key projects – development of this type is suited to meeting the need for affordable housing, better homes for older people and also creating an opportunity for the Council to invest in local development.

6. IMPLICATIONS

- (i) **Impact on Customers** – There would be a number of positive impacts on customers beyond the most obvious one of improving their housing options, including access to training, skills, better employment prospects and stronger ties with their communities.
- (ii) **Impact on Equalities** – those with protected characteristics are highlighted by the government as groups that would benefit from CLH.
- (iii) **Impact on Risk** – CLH provides an opportunity to meet both the Council's and local communities' requirement for housing, strengthens the potential to secure additional resources and provides an opportunity for making best use of existing funding including commuted sums; failure to demonstrate a commitment to this initiative and support interested local community groups could have an adverse impact on the Council's reputation.
- (iv) **Impact on Resources (financial)** – Funding has been provided to implement the proposed actions to launch a programme of CLH in the District which has to be accounted for to the DCLG. This also provides a framework for making best use of other existing sources of funding and potential creates an opportunity for the Council to generate additional funding from development opportunities.
- (v) **Impact on Resources (human)** – Some of the funding allocated can be used to ensure that the Council's Housing Service has the capacity to deliver the actions proposed.
- (vi) **Impact on the Environment** – New housing development inevitably has some impact but the intention is that in some cases this would be offset by

making best use of existing properties such as empty homes, homes that need refurbishment or could be converted. CLH development can also reduce the overall level of development needed to meet local need by encouraging the development of what is needed to meet local need rather than demand from elsewhere.

Background Papers: Department of Communities and Local Government Final Guidance on Community Led Development <https://www.gov.uk/government/news/60-million-boost-for-communities-affected-by-second-homeownership>

Enquiries to: Paul Gayler, Strategic Housing Manager, (Tel: 01621 875753).

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Proposed Actions for Implementing Community Led Housing (CLH) Programme

Issue	Action	Timescale	Resources
Capacity to develop and deliver programme	Extend existing Housing Projects Officer post for additional two-year period	From April 2017	CLH Grant
Engagement with local groups to identify interest and options for CLH	CLH Seminar	From March 2017	CLH Grant
Access to specialist skills	Review of existing partnerships with housing associations, framework agreements with consultants and others who could help support local community groups	April 2017	Within existing capacity
Interest from Community Groups	Survey of local groups to establish interest, need for resources and options for development	April to June 2017	Housing Projects Officer
Indicative programme of CLH	Options to be collated and assessed for initial feasibility and financial viability	June 2017 and ongoing	Housing Projects Officer
Options for enabling CLH			
Use of empty properties	Identify need for additional funding to existing Empty to Decent Homes scheme and review of policy for scheme.	April to June 2017	Housing Projects Officer / Strategic Housing Manager Supplement to existing fund of £32,000
New build	Complete land review undertaken by ECC and combine with review by local housing associations and other community groups	March to May 2017	Housing Projects Officer / Strategic Housing Manager
	Review policy and procedure for use of exception sites to meet CLH need	March to May 2017	Housing Projects Officer / Strategic Housing Manager
	Identify and agree terms for engaging possible development partners including local developers, land-	April to July 2017	Housing Projects Officer / Strategic Housing Manager

Issue	Action	Timescale	Resources
	owners and developing housing associations		
	Explore opportunities for establishing a Council led development vehicle to enable the development of property, joint investment and profit sharing	April to September 2017	CLH Fund
	Establish in-house financial viability resource for assessing feasibility of possible schemes.	June / July 2017	CLH Fund
Wider community benefits	Identify possible partners to provide access to training and skills especially associated with construction to combine with future development and refurbishment schemes	April – September 2017	Housing Projects Officer
	Identify possible partners to provide housing management service	April – September 2017	Housing Projects Officer
Implementation			
Review and revise action plan	Report to Members on progress, identified opportunities and allocation of resources	September – October 2017	Strategic Housing Manager
Initial allocation of CLH Fund for:	<ul style="list-style-type: none"> • Access to technical advice • Needs assessments • Detailed financial appraisals • Surveys • Commissioning partners • Capacity building 	October 2017	CLH Fund
Future funding	Review and submit application for further funding requirements	From October 2017	Strategic Housing Manager

Additional Funding Sources to Support CLH

	Amount	Purpose
Commuted sums	£406,000	Capital investment for affordable housing
Empty to Decent Homes	£32,000	Renovation of long-term empty properties
Better Care Fund (Disabled Facilities Grants)	£20,000	Discretionary funding for adaptation of homes to meet the need of those with disabilities
Total	£458,000	

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REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

STRATEGIC DESIGN CODES - NORTH HEYBRIDGE GARDEN SUBURB

1. PURPOSE OF THE REPORT

- 1.1 To seek the Committee's approval of the Strategic Design Codes as a basis for the determination of planning applications for development within the North Heybridge Garden Suburb. Reserved matters applications are anticipated shortly following resolution to grant approval of outline applications subject to signed Section 106 (S106). The determination of Reserved Matters applications is conditional on the prior approval of the Strategic Design Codes.

2. RECOMMENDATION

That the North Heybridge Garden Suburb Strategic Design Codes (**APPENDIX 1**) be approved for Development Management purposes in the determination of planning applications within the Garden Suburb.

3. SUMMARY OF KEY ISSUES

- 3.1 Policy S3 Place Shaping in the Local Development Plan (LDP) sets out important principles to guide the development of the North Heybridge Garden Suburb [and South Maldon Garden Suburb] to ensure it is planned and developed as a high quality neighbourhood that will enhance the character of the District and protect and enhance the environmental quality of the surrounding area. The North Heybridge Garden Suburb Strategic Masterplan Framework (SMF) was endorsed by the Council in October 2014 and is a material planning consideration in the determination of planning applications. Likewise, the South Maldon Garden Suburb Strategic Masterplan Framework was endorsed by Council in September 2014. The Council requires all planning applications to be in compliance with the principles set out in the LDP and Strategic Masterplan Frameworks.
- 3.2 Reports to the Committee on 14 November 2014 and 1 October 2015 set out the governance arrangements for implementation of both Garden Suburbs and the requirements of planning applications to deliver the design quality expected. Planning applications have now been submitted in respect of all six of the strategic sites located in the Gardens Suburbs. The Strategic Masterplan Frameworks have assisted the Council in ensuring that planning applications satisfy the requirements of Policies S3 Place Shaping and S4 Maldon and Heybridge Strategic Growth to deliver the necessary infrastructure and a high quality development in accordance with

garden suburb principles. Planning Performance Agreements (PPA) with applicants has ensured that applicants have been appropriately informed about the requirements of the Council, the LDP and the recommendations of the Strategic Masterplan Frameworks prior to the submission of an application. This has included the production of Design Parameter Plans for outline applications to secure the key structuring elements for land use, green infrastructure, movement and access, density and storey heights and Strategic Design Codes (**APPENDIX 1**) to inform detailed planning and Reserved Matters applications.

The status of planning applications in respect of the sites within the Garden Suburb is summarised in the following table. All strategic applications are reported to extraordinary meetings of the Council for determination taking into account relevant planning policies and responses to statutory and public consultation.

Site	Application Reference	Proposal	Current Status
North Heybridge Garden Suburb			
Site S2(d) North of Heybridge	OUT/MAL/15/00419	Mixed use development comprising 1138 dwellings, residential care, neighbourhood uses, primary school, strategic flood relief infrastructure, relief road and open space	Resolution to grant outline consent approved at an Extraordinary Meeting of the Council (EMC) subject to Section 106 (7 December 2016)
Site S2(e) land north of Holloway Road	OUT/MAL/14/00990	Up to 100 residential dwellings	Resolution to grant outline consent approved at an EMC subject to Section 106 (31 March 2016)
Site S2(f) land west of Broad Street Green Road	FUL/MAL/15/00885	145 residential dwellings, open space and ancillary infrastructure	Resolution to grant outline consent approved at an EMC subject to Section 106 (19 May 2016)

- 3.3 With the submission of planning applications for individual sites, it is important to ensure applications are fully coordinated and there is a joined up and comprehensive approach to design and delivery of the Garden Suburbs over the longer term. In order to ensure design quality throughout the Garden Suburbs, consultants have been appointed by the Council in May 2015 to prepare Strategic Design Codes for both Garden Suburbs taking into account the endorsed Strategic Masterplan Frameworks and Design Parameter Plans submitted with outline applications. The cost of the consultants has been funded through the Department for Communities and Local Government (DCLG) Capacity Funding. The Strategic Design Codes for the South Maldon Garden Suburb were endorsed by the Committee via the report of Director of Planning and Regulatory Services on 3 March 2016.

- 3.4 The North Heybridge Garden Suburb Strategic Design Codes (**APPENDIX 1**) take forward the vision and development framework set out in the North Heybridge Garden Suburb Strategic Masterplan Framework and the Design and Access Statements and Design Parameter Plans prepared by developers in support of outline

planning applications. The purpose of the North Heybridge Garden Suburb Strategic Design Codes is to provide greater certainty on aspirations for the design quality of the key structuring elements of the SMF and to ensure that the key garden suburb principles are carried through to the detailed design and implementation.

- 3.5 The Strategic Design Codes are split into design codes and design guidance. The design codes will be mandatory and set standards for strategically important elements of the North Heybridge Garden Suburb notably in the design and landscaping of principle streets and green spaces and the creation of an integrated network of green spaces and pedestrian and cycle routes throughout the garden suburb. The design guidance is less prescriptive and sets out key principles in response of other aspects of the development which are central to achieving the vision for the garden suburb such as character areas, car parking, materials and public realm, cycle accommodation and bin storage. Reserved Matters applications will be required to comply with both the design codes and the design guidance.
- 3.6 The design of streets and spaces between buildings will have a key role in delivering the vision for the garden suburb and ensuring that they accommodate all necessary functions of movement and parking as well as creating an attractive high quality and comfortable environment throughout the garden suburb. A Structuring Plan identifies four Strategic Design Codes for Primary Streets, Green Edges, Green Spaces and Built Edges.
- **A Structuring Plan** – the basic development framework and infrastructure including streets, pedestrian / cycle networks, location of community facilities, green infrastructure network, density and height parameters and urban design principles.
 - **Primary Street Codes** – design criteria for the primary road network including street tree planting.
 - **Green Edge Codes** – design criteria for how buildings interface with the rural green edge.
 - **Green Space Codes** – design criteria for greenways and greenspaces.
 - **Built Edge Codes** – design criteria for how buildings relate to existing streets and development to ensure integration with the wider area.
- 3.7 The document will be used by applicants and by the Development Management team in ensuring that development proposals come forward in line with the Strategic Design Codes and Design Guidance. In practice this means that:
- infrastructure and plot development reflect the spatial distribution of the elements as defined by the Structuring Plan.
 - street cross-sections for primary streets, green spaces and built edge and green edge treatments are compliant with the mandatory Strategic Design Codes.
 - proposals for individual plots reinforce the identity of the character context.
 - parking, materials and bin storage arrangements take due regard of the design guidance.
- 3.8 **Primary Street Code** – The treatment of the primary street established the character of the Garden Suburb at important gateway entrances and within the Garden Suburb.

The code will ensure continuity of character and identity across plot boundaries. The Strategic Design Code sets out performance specification in respect of street dimensions, design criteria, public transport and materials and landscape including street trees and street furniture. The codes states that no more that 50% of the plot frontage of individual houses will be occupied by parking spaces and a single species clipped hedge will be provided to the property boundary. Key elements of the primary street cross-section which must remain constant include:

- A consistent landscape strip with formal tree planting on both sides of the street to create an Avenue.
- Parallel pedestrian and cycle access.
- Pedestrian and cycle access separated from the carriageway by a landscape strip or verge which will also accommodate parallel or bay parking.
- A flexible strip adjacent to the carriageway for; parking, bus layby, additional lane on approach to junctions or extension to landscape strip.

- 3.9 **Green Edge Code** – The green edge code will ensure that the relationship between green space and development and the interface with open countryside is attractive and well planned. Green spaces will be overlooked by development and designed in accordance with principles of ‘Secured by Design’. Landscape treatment will be required to reflect the character context areas by responding to the wooded character of Heybridge Wood in the west and the more open rural character to the north where it merges with the landscape corridor to the south of the relief road and flood alleviation infrastructure. Provision is made for shared streets adjacent to green edges to create an attractive interface between the development and green space.
- 3.10 **Green Space Code** – Green infrastructure is an essential component of the Garden Suburb and will be required to fulfil a number of roles including recreation and opportunities for increased biodiversity. The code sets minimum requirements for green spaces and the creation of a network of greenways and green fingers which will include provision for pedestrian and cycle routes, play space, allotments and Sustainable Drainage Systems (SuDs). Provision is made for shared streets adjacent to green spaces to create an attractive interface between the development and green space.
- 3.11 **Built Edge Code** – The built edge code requires all frontage development to be orientated to face the street or public realm in order to provide attractive and animated frontages. The code includes the treatment of the Maypole Road, Holloway Road and Broad Street Green frontages and ensures integration of new development with existing built and natural features.
- 3.12 **Design Guidance** – Three Character Context Areas are defined: *Village Edge*, that has a close physical relationship with the rural villages north of Heybridge and reflected in a looser grained rural edge of dwelling sizes arranged around informal and irregular shaped green spaces; the *Garden Suburb* is towards the centre of the site where a more formal, planned layout of regular, formal street patterns with vistas and characteristic tree lined streets and formal open spaces and *Woodland Edge* where a protective green buffer provides an opportunity for development to face towards the woodland with informal shared space creating a soft edge between the built area and

ancient woodland. Design guidance is provided in respect of urban form, planting, colour and materials and boundary treatment.

- 3.13 Guidance is provided on parking. This will be provided in accordance with adopted standards and the guidance states that it should be achieved predominantly with on-street and on-plot parking with the occasional use of courtyard parking areas or where there are flatted blocks but with maximum of ten spaces, but preferably six to eight spaces. Courtyard parking should always be visible from the street and well overlooked by the fronts of buildings.
- 3.14 Guidance is provided on materials and public realm design. Proposals will be required to incorporate materials which visually link and unify all parts of the development with a consistent treatment of carriageways and footways across each phase of development. Materials will be required to be robust and durable and allow for easy maintenance and replacement.

4. CONCLUSION

- 4.1 Outline applications that have been approved in the Garden Suburbs have planning conditions attached whereby any future Reserved Matters applications must be in accordance with the approved Strategic Design Codes. Reserved Matters applications are anticipated shortly in both Garden Suburbs. The South Maldon Garden Suburb Strategic Design Codes are already in place to inform Reserved Matters applications. Likewise, it is important that the North Heybridge Garden Suburb Strategic Design Codes are in place to inform the preparation of detailed design proposals for Reserved Matters applications.
- 4.2 The Strategic Design Codes will ensure delivery of design quality throughout the North Heybridge Garden Suburb in accordance with the LDP and North Heybridge Garden Suburb Strategic Masterplan Framework.

5. IMPACT ON CORPORATE GOALS

- 5.1 Planning supports corporate goals which underpin the Council's vision for District and in particular protecting and shaping the District and balancing the future needs of the community, meeting the housing needs of the District and enabling, supporting and empowering communities to be safe, active and healthy.

6. IMPLICATIONS

- (i) **Impact on Customers** – Planning applications for development within the Garden Suburb will be determined in accordance with the principles set out in the Strategic Design Codes ensuring delivery of necessary infrastructure and a high quality of development.
- (ii) **Impact on Equalities** – The development of the North Heybridge and South Maldon Garden Suburbs will provide sustainable and well planned

communities which promote social cohesion and help to provide for the future needs of the community.

- (iii) **Impact on Risk** – The Strategic Design Codes have been prepared in accordance with the Submitted Local Development and the Strategic Masterplan Framework which has been endorsed by the Council as a material consideration in the determination of planning applications. From the day of publication decision makers may give weight to relevant policies in emerging plans according to the stage of preparation of the emerging plan.
- (iv) **Impact on Resources (financial)** – The Strategic Design Codes have been funded with external resources. Without the Strategic Design Codes the Council could be at increased of planning appeals which could potentially have resource implications.
- (v) **Impact on Resources (human)** – Without Strategic Design Codes for the determination of applications for development within the area, each application will need to be treated in isolation which will have resource implications and will also affect the ability to meet the objectives and principles set out in the LDP. The Strategic Design Codes will facilitate the Development Management process and make efficient use of available resources.
- (vi) **Impact on the Environment** – The Strategic Design Codes will assist the Council in promoting high quality and inclusive design and sustainable development, securing the necessary infrastructure to support new and existing communities and safeguarding the local environment in accordance with the policies set out in the submitted LDP and the Strategic Masterplan Framework.

Background Papers:

Report to Planning and Licensing Committee, 13 November 2014

Report to Planning and Licensing Committee, 1 October 2015

Report to Planning and Licensing Committee, 3 March 2016

North Heybridge Garden Suburb Strategic Masterplan Framework October 2014)

Local Development Plan 2014 - 2029 as submitted for examination

Enquiries to:

Jackie Longman, Urban Design Officer, (Tel: 01621 875731).

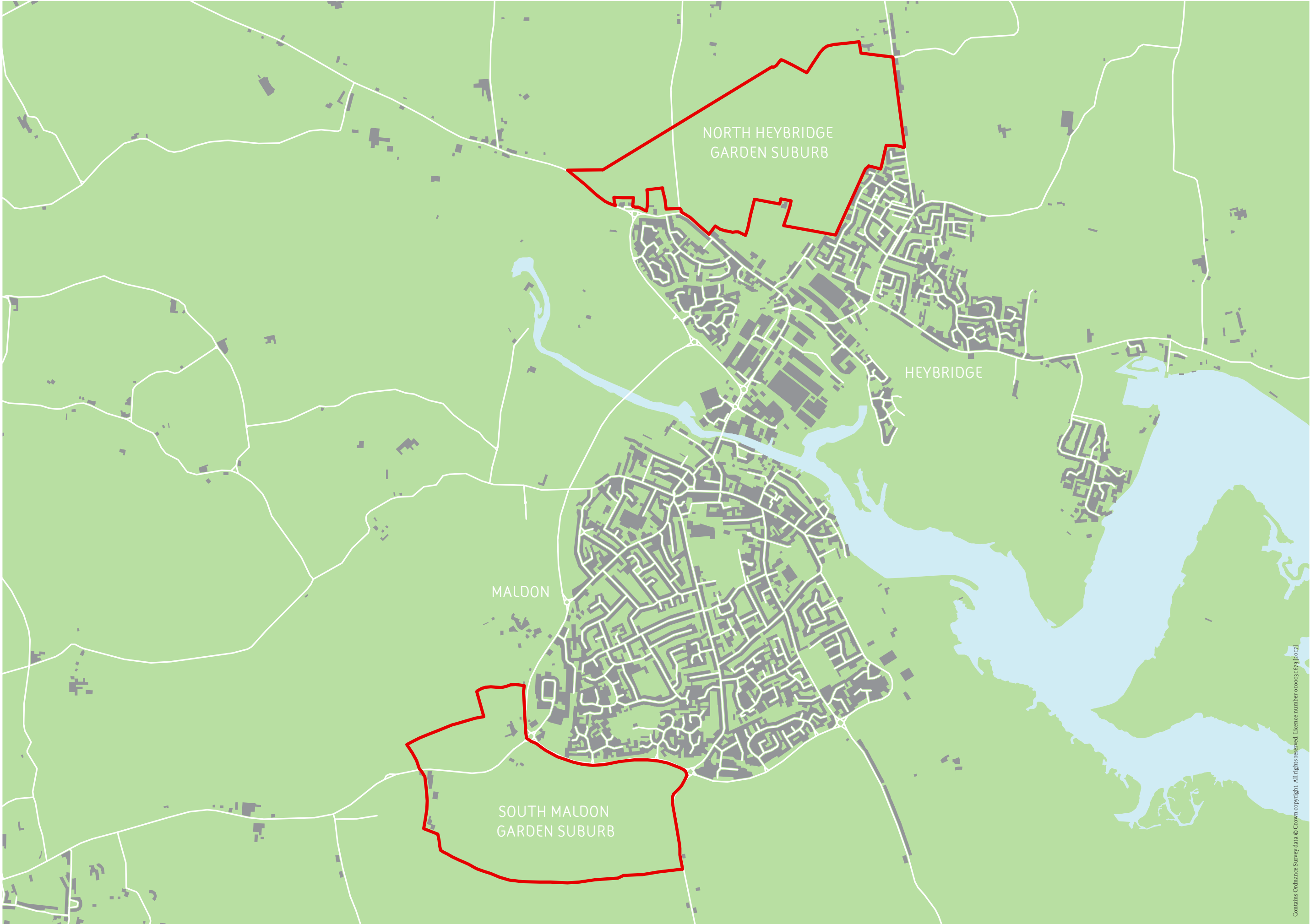
STRATEGIC DESIGN CODE
Maldon District
NORTH HEYBRIDGE GARDEN SUBURB





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1.0 INTRODUCTION
1.1 Background
1.2 Purpose of this document
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PREFACE

The creation of a great place does not require a singular architectural vision; it requires a beautifully conceived physical framework to determine form and transform the assets of a site into a living, thriving and sustainable place. It requires clear and simple standards that can be applied consistently over decades to provide houses, shops and work environments that people want to invest in, use and look after whilst at the same time building character, coherence and richness.

The Strategic Design Codes will facilitate the creation of a high quality, vibrant and distinctive neighbourhood based on garden suburb principles. By adhering to the framework and codes set out in this document, the layout and identity of the North Heybridge Garden Suburb will set it apart as a contemporary sustainable garden suburb and establish Maldon District at the forefront of successful place making.

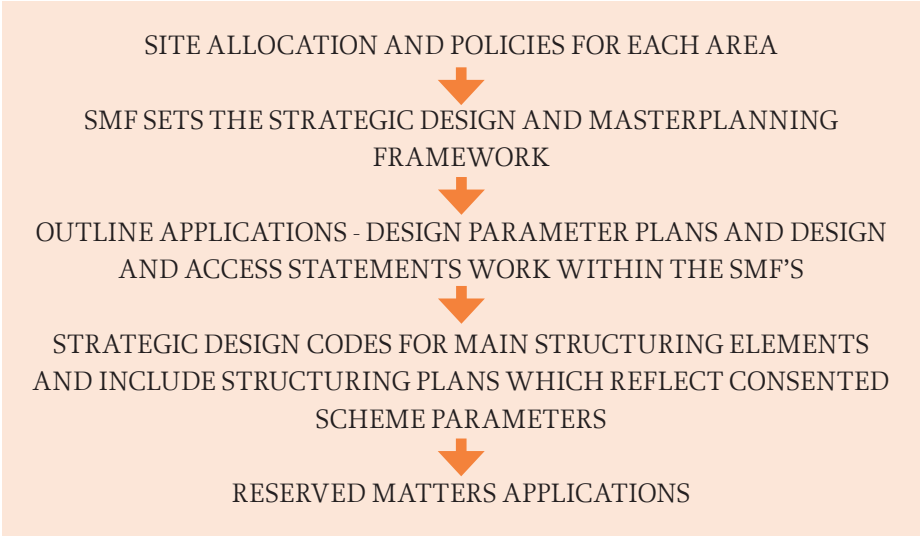


SECTION 1.0
INTRODUCTION

The Maldon District Local Plan (LDP) 2014-2029 sets out how growth can be accommodated in the District up to 2029 including two large allocations in the main settlements of Maldon and Heybridge. It outlines the aspiration to achieve a garden city character and spatial attributes in two new garden suburbs. The LDP sets out important development principles to ensure they are planned as developed as:

- High quality neighbourhoods which are well integrated within existing settlements
- Enhance the character of the District
- Protect and enhance the environmental qualities of the surrounding area

Strategic Masterplan Frameworks (SMFs) have been prepared to set the context for more detailed planning applications and establish design principles which will need to be resolved at reserve matters stage. The SMFs set out how garden city principles could be applied within Maldon District to create two new Garden Suburbs. The SMFs have been endorsed by the Council and are a material consideration for development management purposes and in the determination of planning applications within the designated Garden Suburbs. Outline consents will be conditioned to require submission of any reserved matters applications to accord with Design Codes developed in conjunction with and approved by the Local Planning Authority. They will take full account of the principles of the approved SMFs and the subsequent reserved matters applications will need to accord with the approved design codes in all respects.



PURPOSE

Codes have been developed to address the specific challenge of residential development delivery on green field land over 15-20 years. Both the South Maldon and North Heybridge garden suburbs are also in multiple land ownership and will be developed in phases by several different developers or site promoters. The Strategic Design Codes have been developed as two documents - one for each suburb at South Maldon and North Heybridge to ensure that within each garden suburb each land parcel and development phase is coordinated with the one adjacent to it, and ensure that when both garden suburbs are completed they have a strong and recognisable identity. The SMFs will be elaborated on in the respective Design and Access Statements. Those that have already been produced have been considered in the production of this document.

The creation of great places in this context requires a strong vision and a beautifully conceived physical framework which is capable of transforming the assets of a site into a distinctive, living, thriving and sustainable place. It requires clear and simple standards than can be applied consistently over time to provide houses, shops and work environments that people want to invest in, use and look after whilst at the same time building a garden suburb character, coherence and richness.

Many design codes in the past have focussed on the architecture and have been very prescriptive about design outcomes, often working against commercial realities and even buildability. Such codes have often proved time-consuming, expensive to implement, inflexible and stifled creativity at the design stage. This Strategic Design Code has been prepared to complement the North Heybridge SMF, setting the design parameters for fundamentally important aspects of the new communities relating to quality of place. However, it leaves significant flexibility with regard to housing product and architectural design.

The vision and the framework contained in the North Heybridge SMF sets the overarching physical structure for the new community; it was formed through a process of collaboration and consultation and encapsulates Maldon District Council's aspirations for the Garden Suburb community. The purpose of the Strategic Design Code is to provide a greater degree of certainty to all involved on the aspirations for the design and quality of these structuring elements of the plans. The Strategic Design Code will also provide a level of guidance which can be used to ensure coherence and consistent high standards of design across the new community, irrespective of when it is built out or by whom.



SCOPE

The Strategic Design Code covers the North Heybridge Garden Suburb. A Structuring Plan is presented to establish the basic framework of the place – it is not a new plan but drawn from the SMF, parameter plans of consented schemes, and emerging information from non-consented schemes into a robust plan to establish the basic urban framework of the place. The structuring plan is fixed in order to deliver development in line with the SMF.

The Strategic Design Code is split into ‘design codes’ and ‘design guidance’. The design codes will be mandatory; they set minimum requirements and prescriptive standards and rules for strategically important aspects of the plan. The design guidance is less prescriptive; it describes the design intent of elements of the development, effectively setting the design brief and allowing a degree of flexibility in the specific design outcome. Design statements submitted alongside planning applications will need to explain how designs comply with both the design code and the guidance.

The design code has been written to cover the strategically important items of infrastructure which will be central in defining the garden suburb character of North Heybridge. The following elements of the plans will be covered by the Strategic Design Code:

- Street Codes will set out objectives, core functions and design criteria for the primary network
- Green Edge Codes will set out objectives and design criteria for how buildings overlook green edges
- Green Space Codes will set out objectives and design criteria for how buildings overlook and enclose greenways and green fingers
- Built Edge Codes will set out objectives and design criteria for how buildings overlook or relate to existing streets and development

The design guidance has been written to cover other elements of the plan which are central to achieving the vision for North Heybridge or are of significant importance in local planning policy. They are not specifically coded but will require particular attention at outline application. The following elements will be covered by the Design Guidance:

- Character Context Areas & Gateways & Entrances
- Car Parking
- Materials and Public Realm
- Bin Storage

HOW TO USE

The intention of the code is to provide simple and clear coding for strategically important aspects of the plan. The diagram at the start of each code should be referred to for the location of the area which each code applies to. This document provides the design parameters for each of these elements of the plans, they still require good design at all stages of work to ensure the desired outcome is achieved. There is therefore a degree of flexibility within the code and guidance to allow a full design response to be developed at an appropriate stage.

The document will be used in different ways by different parties.

PLOT DEVELOPER:

Each developer will bring forward their scheme on individual plots within the context of the overarching Structuring Plan including key pieces of primary infrastructure. The Structuring Plan provides the indicative layout of primary infrastructure including main forms of access and strategic green space provision. It also shows how the various land uses should be arranged across the site and densities envisaged. In designing primary infrastructure the plot developer will locate primary infrastructure elements, including primary streets and strategic green infrastructure in accordance with the framework plans.

Each plot developer will use this document to:

- Design primary infrastructure elements in line with the design codes for streets, spaces and edges;
- Understand how their plot relates to the overarching vision and framework for the development of the site and the requirement to achieve a positive relationship between different landholdings and that the site is unified through a constant design approach to primary infrastructure and development edges
- Identify whether the framework plan has implications for their plot, for example the incorporation of strategic green infrastructure.
- Understand what character context area their plot falls within and how it should be developed to respond to that character. Reference should be made to the character context area statement and guidance on urban form, materials and planting.
- Refer to the coding plan to determine which elements of the design code are relevant to their plot and ensure proposals meet the mandatory requirements
- Plan development in accordance with the principles set out in the design guidance and demonstrate how this design intent has been translated into detailed design
- Determine parking arrangements in accordance with the parking design guidance and Essex County Council parking standards
- Ensure bin storage is fully considered in line with design guidance

DEVELOPMENT MANAGER:

Development management processes will ensure development proposals come forward in line with this document. In practice this means ensuring that:

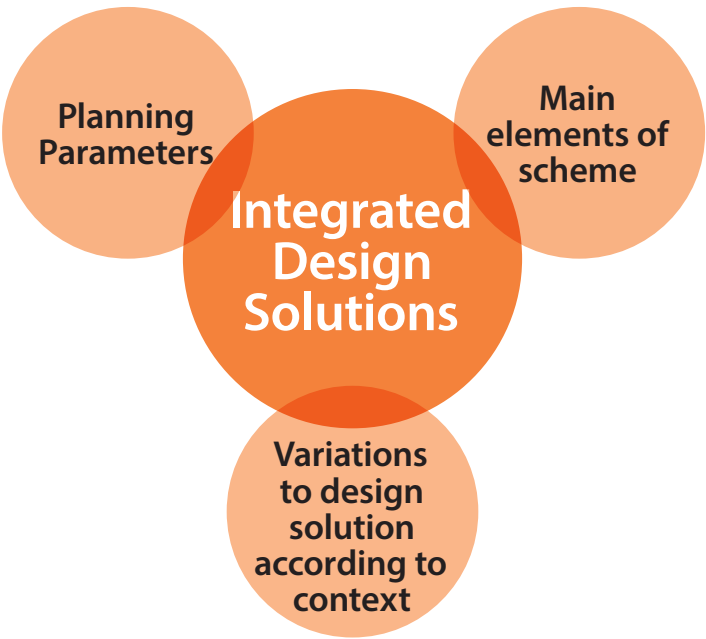
- Infrastructure and plot development reflects the spatial distribution of elements as defined by the structuring plan;
- Street cross sections for primary streets are compliant with the mandatory design codes with reference to the coding plan;
- Green spaces are designed in accordance with the mandatory requirements set out in the green space code
- Edge treatments are compliant with the built edge and green edge codes;
- Proposals for individual plots reinforce the identity of the character context area in which they sit
- Parking, materials and bin storage arrangements take due regard to the design guidance

CODES KEY ELEMENTS

Whilst architecture is important to any scheme identity, the wider quality and coherence of any scheme requires a clear strategy for dealing with a relatively small number of key elements. The approved SMF is intended to accommodate schemes which are visionary and well-conceived with regard to its context and site characteristics. The key elements are:

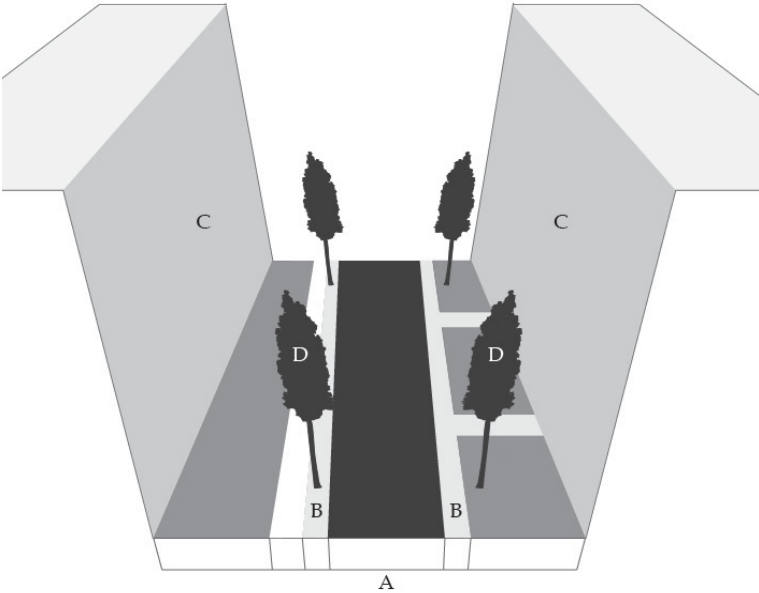
- Ensuring the design of development is guided by a clear vision that is applied over time.
- The design of streets and spaces between buildings will have a key role to ensure that they have a sense of hierarchy and accommodate all functions, such as movement and parking, whilst also creating an attractive, high quality and comfortable environment. Well-thought through street cross sections, which give clarity to the role and function of all areas of the public domain will be an essential part of this. This document will illustrate how the design of streets and spaces can fulfil their potential to add value to the various character context areas and development opportunities at Heybridge.
- The treatment of edges (frontage development) so that they support the overall garden suburb aspiration, for example; ensuring that the scale, massing, rhythm and setback of a street frontage supports the character and hierarchy of the street. The document includes generic street sections for Primary Streets to demonstrate how the development parameters can be met through an integrated design solution. These are supported by a number of variations that illustrate design responses to different conditions across the development.

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PHOTOGRAPH EXAMPLES

The photos opposite show good examples of where key coding principles promoted at Heybridge have been delivered elsewhere.



CODING: WHAT MATTERS

- A: GETTING THE STREET SECTION RIGHT
- B: CONFIGURING THE PARKING
- C: DEFINING EDGES
- D: USING THE RIGHT LANDSCAPE, MATERIALS AND FURNITURE

I. WELL THOUGHT OUT STREET SECTION



- A - DEFENSIBLE ZONE
- B - GENEROUS FOOTWAY
- C - VERGE AND TREE PLANTING
- D - ON STREET PARKING

2. BUILDINGS ADDRESSING LINEAR GREEN SPACE



- A - LIVELY, ARTICULATED EDGE
- B - DEFENSIBLE ZONE
- C - FOOTWAY
- D - OVERLOOKED LINEAR LANDSCAPE

3. FLEXIBLE COMMUNITY STREET



- A - FLEXIBLE PARKING
- B - SHARED SURFACE FRIENDLY STREET
- C - LEAFY ENVIRONMENT UNIFIES ARCHITECTURE

4. PEDESTRIAN FRIENDLY COMMUNITY STREET



- A - DIVERSE EDGES OF CONSISTENT SCALE
- B - SHARED SPACE WITH DEFINED FLEXIBLE PARKING CREATE PEDESTRIAN FRIENDLY STREET
- C - VERGE PROVIDING BUFFER TO STREET AND TREE PLANTING ZONE



SECTION 2.0 VISION

“The Garden Suburbs at Maldon and Heybridge will be planned as high quality, vibrant and distinctive neighbourhoods that will complement and enhance the character of the District and protect and enhance the environmental qualities of the surrounding area” (Policy S3 LDP)

The Maldon District Development Plan sets out proposals for two new ‘garden suburbs’ at South Maldon and North Heybridge. The vision is contained in full in Policy S3: Place Shaping and further developed in each SMF; it is intended to be an expression of what the garden suburbs will be like. The vision, along with accompanying high level objectives, sets Maldon District Council’s aspirations for South Maldon and North Heybridge and what the new community must achieve.

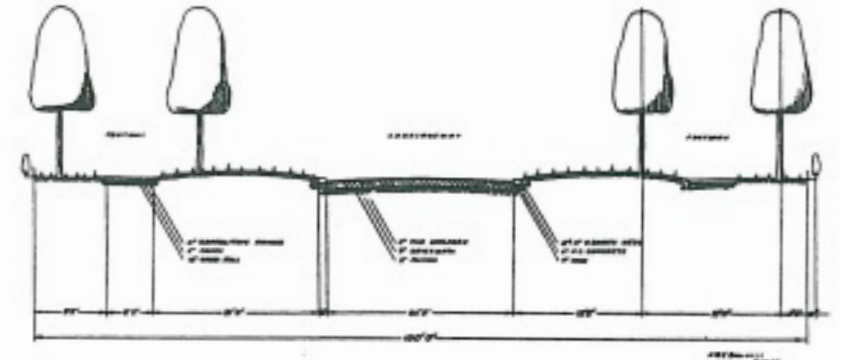
The Strategic Design Code has been prepared to sit alongside the vision, it provides a greater degree of certainty to all involved on the aspirations for the design and quality of the structuring elements of the plan which are central to achieving the vision.

2.1 PRINCIPLES OF GARDEN SUBURBS

National planning policy makes reference to the benefits of garden cities and garden suburbs of the past and the opportunities these highlight for new communities in the future. The Town and Country Planning Association (TCPA) set out the key principles for achieving this in their original report “Creating Garden Cities and Suburbs Today” and have refined their thinking in subsequent publications. The original Garden Cities were borne of a desire to tackle the social and environmental issues of the late 19th and early 20th centuries. The garden suburbs today must equally address the challenges of the 21st century. The garden suburb vision will combine the best of town and country living and creating healthy homes in vibrant communities. The principles and vision that are set out in the SMF follow the key principles of Garden Suburbs.

Key principles of a successful Garden Suburb (as outlined within the TCPA’s publication) include:

- A strong vision, leadership and community engagement;
- Capture land value for the benefit of the community;
- Community ownership of land and long-term stewardship of assets;
- Mixed-tenure homes that are affordable for ordinary people;
- High-quality design, combining the very best of town and country living to create healthy homes in vibrant communities;
- Generous green space linked to the wider natural environment, including a mix of public and private networks of well-managed, high quality gardens, tree lined streets and open spaces;
- Opportunities for residents to grow their own food, including generous allotments;
- Access to strong local cultural, recreational and shopping facilities in walkable neighbourhoods; Integrated and accessible transport systems – with a series of settlements linked by rapid transport providing sustainable access to destinations.



Reference to historic garden suburbs shows how key street sections were designed that help to structure the place and give a consistency to development. In this example from Letchworth the key sections and built street show the consistent street section and regular tree planting. In a similar way the focus of this Strategic Design Code is on coding key sections through the Garden Suburbs.

2.2 PHYSICAL QUALITIES OF GARDEN SUBURBS

The SMF sets out the key principles of Garden Suburbs. These are set out in Section 4.1 of the North Heybridge SMF. They are broad principles. Underlying these there are a number of special physical characteristics that make Garden Suburbs recognisable and that differentiate them from other towns or suburbs. These must form an integral part of the design approach that developers should achieve. A number of key qualitative features are identified as follows which the Strategic Design Code seeks to achieve and embody within each code section to ensure that fundamental characteristics of garden suburbs such as street trees are incorporated into the strategic infrastructure of each site:

- Traditionally tree lined streets
- Approach vistas which lead visitors to the most significant buildings or gateways
- Trees and grass are planted in the part of the road not required by traffic
- Front gardens on main streets
- Formal hedge boundaries
- Clear hierarchy of roads range from grand and formal to tight knit and intimate streets
- Landscape character is an integral part of the whole approach
- Natural environment is brought into the heart of the community through a network of linked green spaces and avenues giving views of the countryside
- Housing layouts respond to natural conditions and topography

PHYSICAL CHARACTERISTICS OF GARDEN SUBURBS



A - TREE LINED STREETS

B - TREES AND GRASS IN THE PART OF THE ROAD NOT REQUIRED BY TRAFFIC

C - FORMAL HEDGE BOUNDARIES

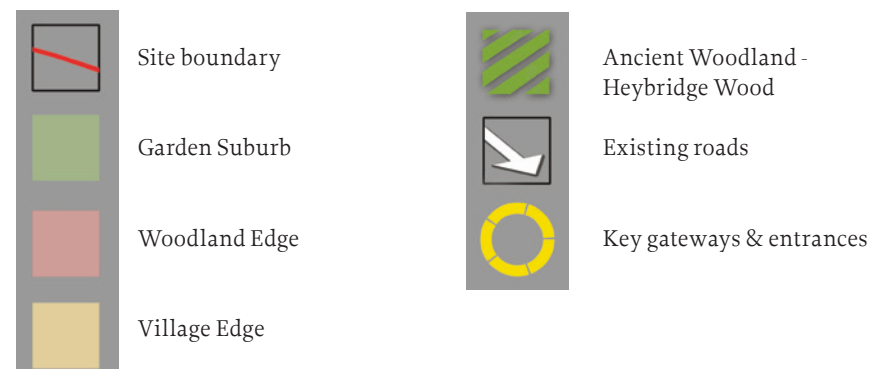
D - LANDSCAPE CHARACTER AN INTEGRAL PART OF THE WHOLE APPROACH

2.3 CHARACTER CONTEXT

A key part of the garden suburb model is the landscape led approach. This concept seeks to bring built form and landscape together harmoniously by allowing the site features to drive the physical form of the new neighbourhood. At North Heybridge the character of each of the different neighbourhoods which make up the new community is derived from an understanding of their landscape and built setting. The benefit of this will be a new community with an identity rooted in place rather than an anonymous 'anywhere development'.

NORTH HEYBRIDGE

Aerial Photography - Bing Maps Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



North Heybridge will provide strong links with the surrounding landscape, incorporate natural features such as Heybridge Wood and other areas of ecological value, mature woodland and hedge, ditches and augmentation of the characteristic field boundaries supplemented with locally distinctive, substantial new planting and green spaces. These steps will combine to define a development character driven by landscape.

The character context areas identified are not development neighbourhoods as referred to in the SMF and consented DAS; they are broad areas that are influenced by their context, setting and landscape which should be reflected across the different residential neighbourhoods that the DAS identifies. For example the northern part of the site has a close physical relationship with the rural villages of North Heybridge which should be reflected in a looser grained urban edge comprising a range of dwelling sizes and arranged around informal and irregular shaped green spaces as can be found at villages such as Little and Great Totham and Tolleshint D'Arcy.

Towards the centre of the site the opportunity exists to create a more formal, planned layout clearly recognisable as a planned Garden Suburb. Building should be arranged in regular, formal street patterns with vistas and characteristic tree-lined streets and formally laid out open spaces. There is a distinction between this area and the edge of Heybridge Wood. Here a protective green buffer provides an opportunity for development to face towards the woodland with informal shared space creating a soft edge between the built area and woodland.

Three broad character context areas are:

- Village Edge
- Garden Suburb
- Woodland Edge

A description of the design and character of each area are explained within 5.0 Design Guidance section of the report, which sets out the expectations for the overarching elements used to convey consistency across the character context areas and aspects that overlay variation and distinctiveness.

SECTION 3.0 FRAMEWORK

3.1 STRUCTURING PLAN


The Structuring Plan underpins the North Heybridge SMF. It sets out the physical structure of development and defines areas of different character across the site. It explains the framework of each place. It defines broad areas of land use which are set by ecological, landscape, hydrological, noise, transport, viability and social constraints. It shows a strategic network of streets necessary to allow traffic movements and provide access to development blocks together with the green infrastructure connections which link the development with its surroundings and encourage sustainable transport choices.

The urban structure set by the Structuring Plan should be considered as fixed to ensure appropriate site development in line with the policies of the North Heybridge SMF. The content of the Strategic Design Code and guidance clearly set out the requirements for elements of the Structuring Plan but leave flexibility as to the detailed arrangement and alignment of these elements and how individual blocks and plots that lie within the framework may be developed.

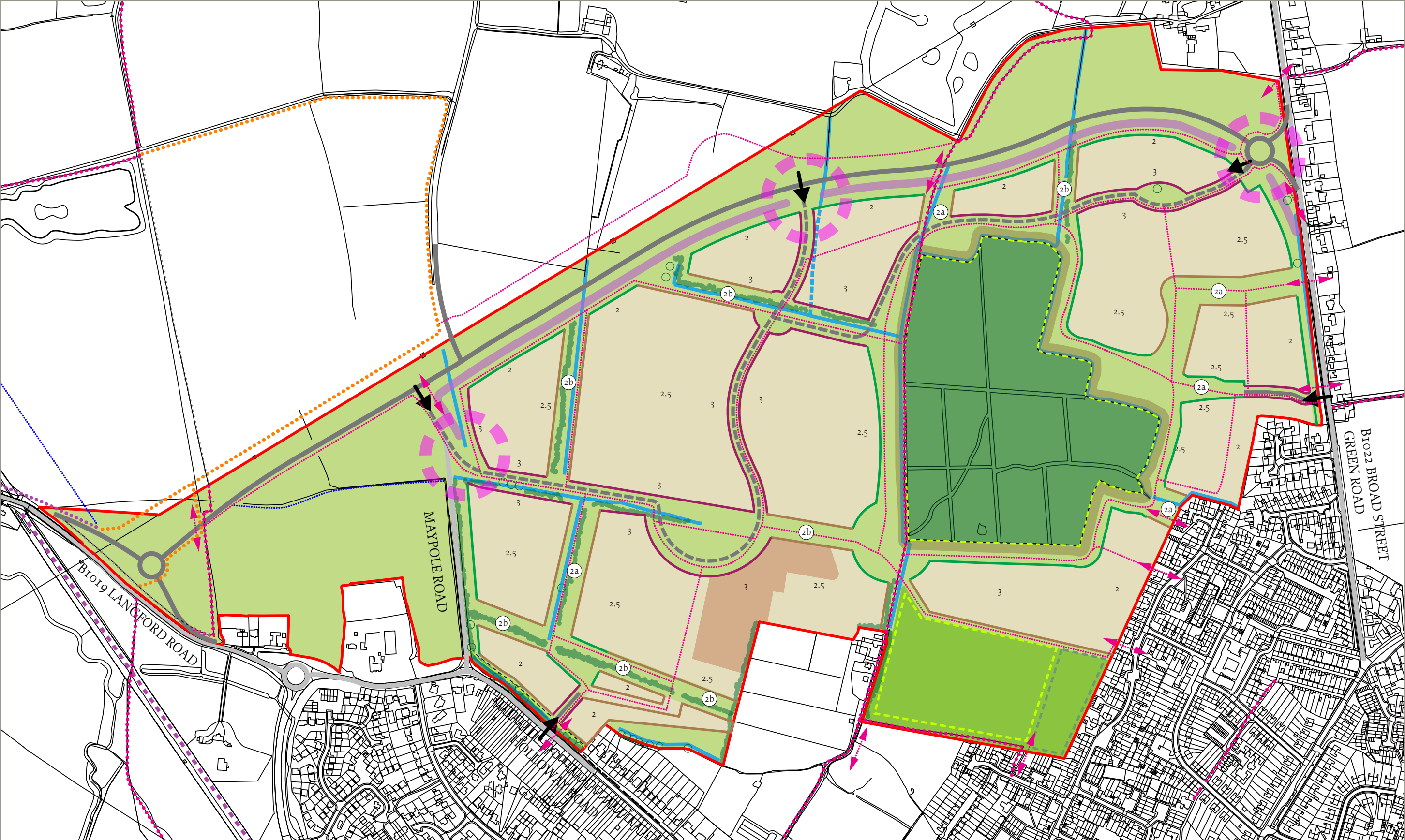
The basic framework of the plan comprises:

- Strategic movement including strategic streets, public transport and pedestrian/cycle networks
- Distribution and nature of land uses, including the disposition of community facilities
- Green Infrastructure network
- Urban design and legibility principles
- Density/height parameters and development extents

NORTH HEYBRIDGE KEY

LAND USE		Boundary of North Heybridge Garden Suburb	FRONTAGE		Primary frontage along main streets
		Residential			Secondary frontage along streets and formal open space
		Mixed Use: Local Centre, to include employment, retail and community uses, primary school, early years facility, residential and infrastructure uses			Green edges with more permeable layout
ACCESS & MOVEMENT		Number of storeys (n.b. max. height)	GREEN INFRASTRUCTURE		Landscaping, Flood Alleviation and/or openspace.
		Gateways and entrances			Heybridge Wood & 15m buffer
		Existing Roads			Local wildlife site
		Future relief road			Sports pitches
		Primary streets - main vehicle, cycle & pedestrian route			Allotment
		Primary streets -bus route (shown indicatively) from DAS			Existing hedge line
		Principle Vehicular Access Points	WATER		Greenways
		Existing PROW/footpaths			Green Fingers
		Key pedestrian and cycle routes / connections			Existing waterbodies/watercourses
		Existing Public Right of Way (to be realigned)			Re-routed watercourse
		Proposed Public Right of Way (Realigned)			Noise attenuation bund
		Potential strategic footpath routes			
		Existing rail trail			
		Permissive footpath			

NORTH HEYBRIDGE STRUCTURE PLAN



SECTION 4.0

STRATEGIC

DESIGN CODES

Design Codes are a set of specific rules or requirements to guide the physical development of a site or place. The aim of design coding is to provide clarity as to what constitutes acceptable design quality and thereby a level of certainty for developers and the local community alike that can help to accelerate the delivery of high quality new development.

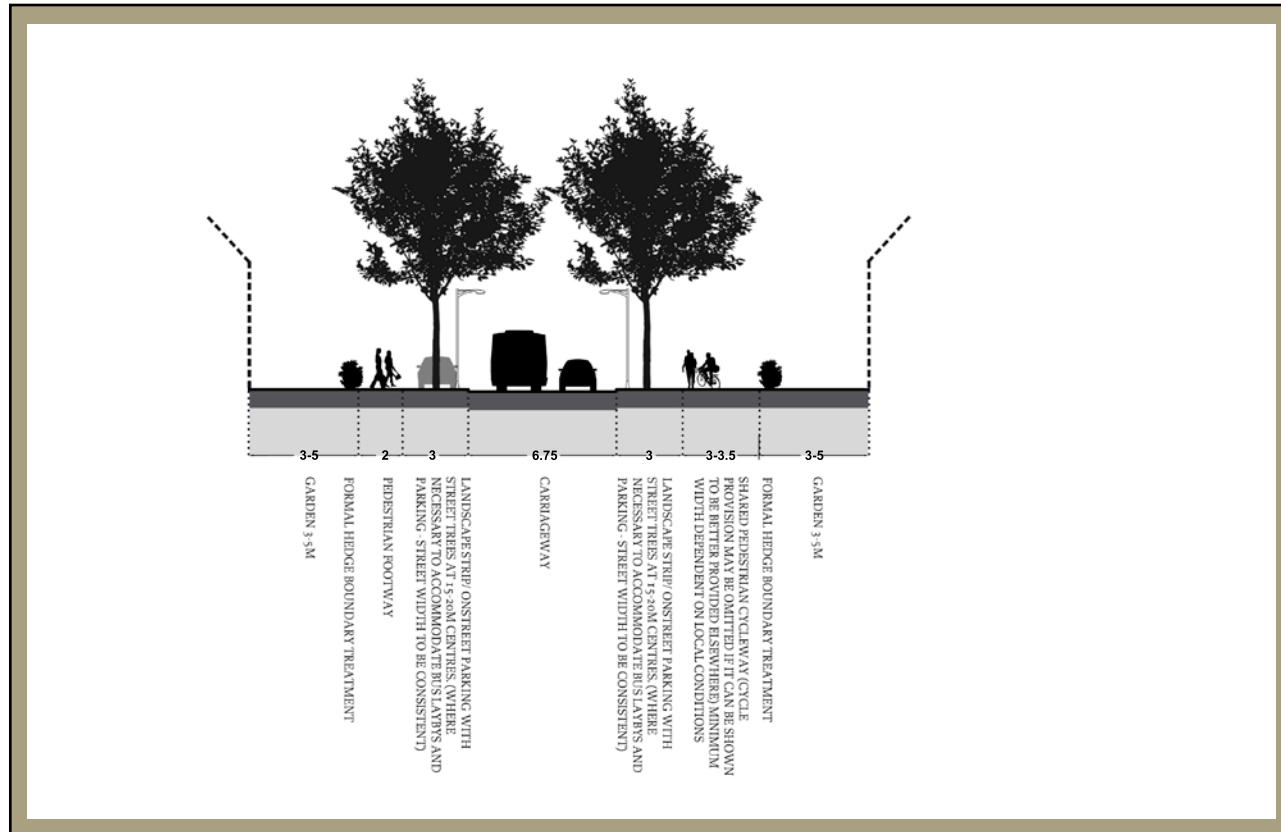
The design codes provide mandatory requirements for how buildings, streets and spaces relate to each other and the public realm. These mandatory requirements will, if consistently applied over time, ensure that the scheme is developed to achieve the aspirations set out in the vision whilst maintaining flexibility for each developer to decide how to bring forward individual plots. In this part of the document the strategic design codes that relate to main structuring elements that pass through and influence the development overall are addressed including:

- Roads and Streets
- Green Edges
- Green Space
- Development Edges

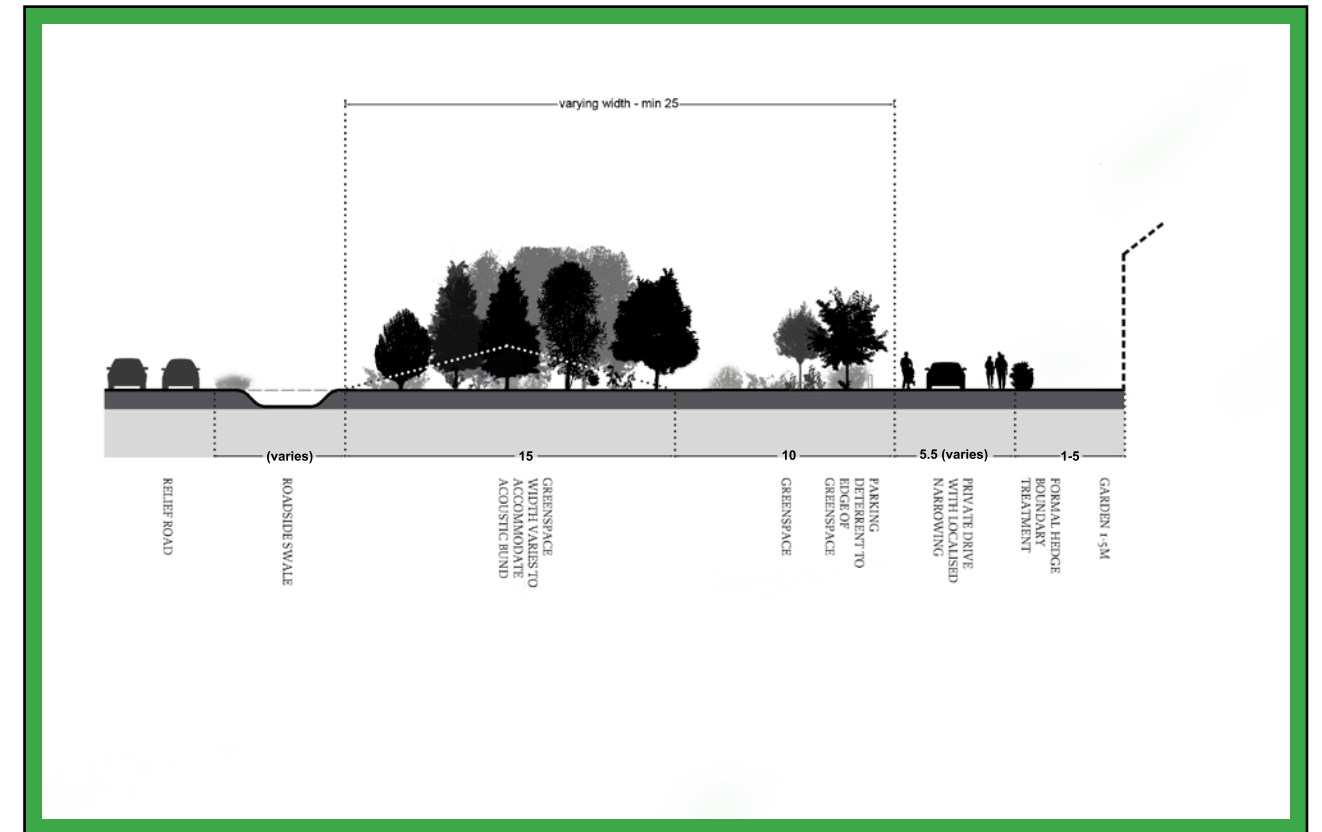
For each of the four main structuring elements there is a common basis for the design response set out on these pages. Each one is driven by an integrated approach to the key parameters of the approved North Heybridge SMF. Each code chapter has this design response set out first as the generic or ‘typical’ designed section with an associated table that summarises the key parameters and the design features incorporated to respond to them. It is followed by a number of illustrative variations.

The local context of the proposed development and the desired character will influence these structuring elements. For example a Primary Street will be different in a residential area to when it is within a mixed use area, and may include variations across North Heybridge because of the use, scale, functions and activities that these conditions need to accommodate. Variations to the typical design code are included that demonstrate that the design parameters can be met in given locations and with adaptations that ensure the local character and conditions are integrated.

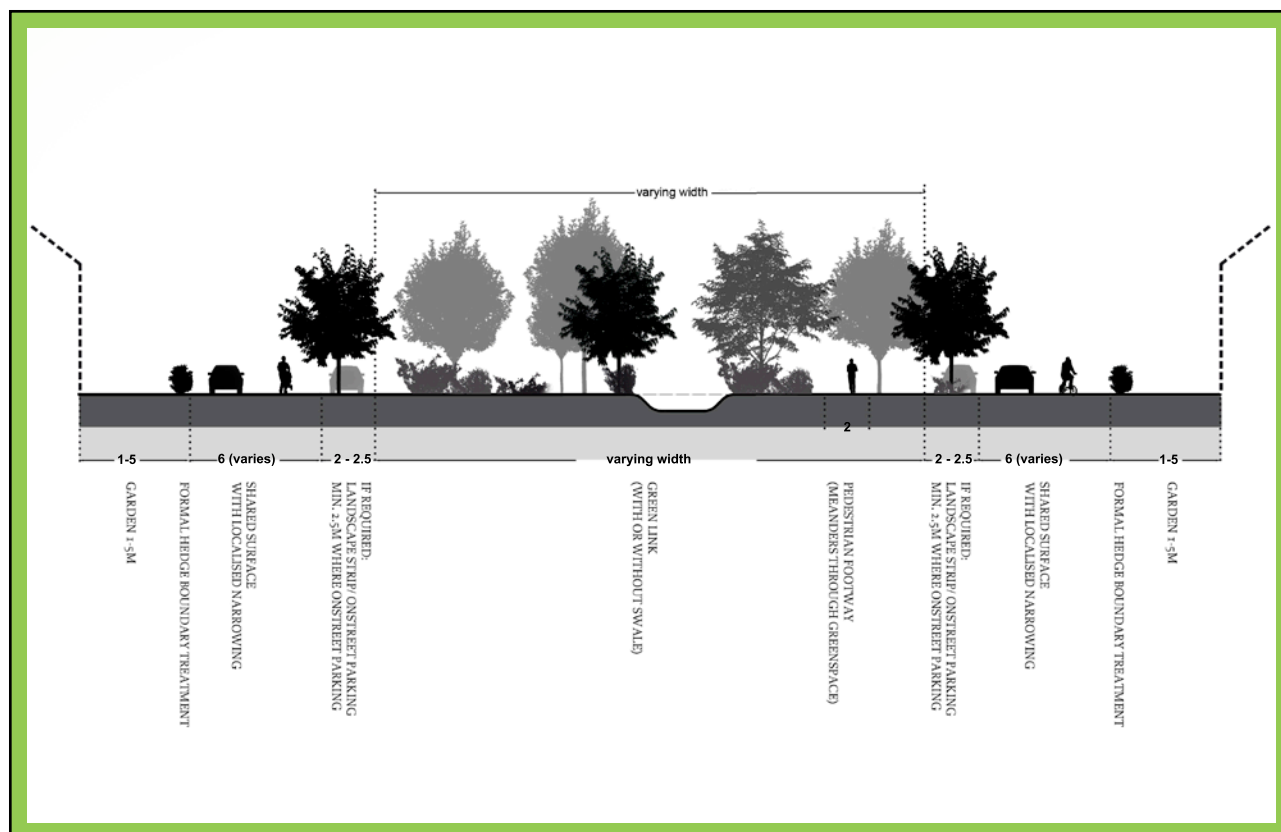
EXAMPLES:



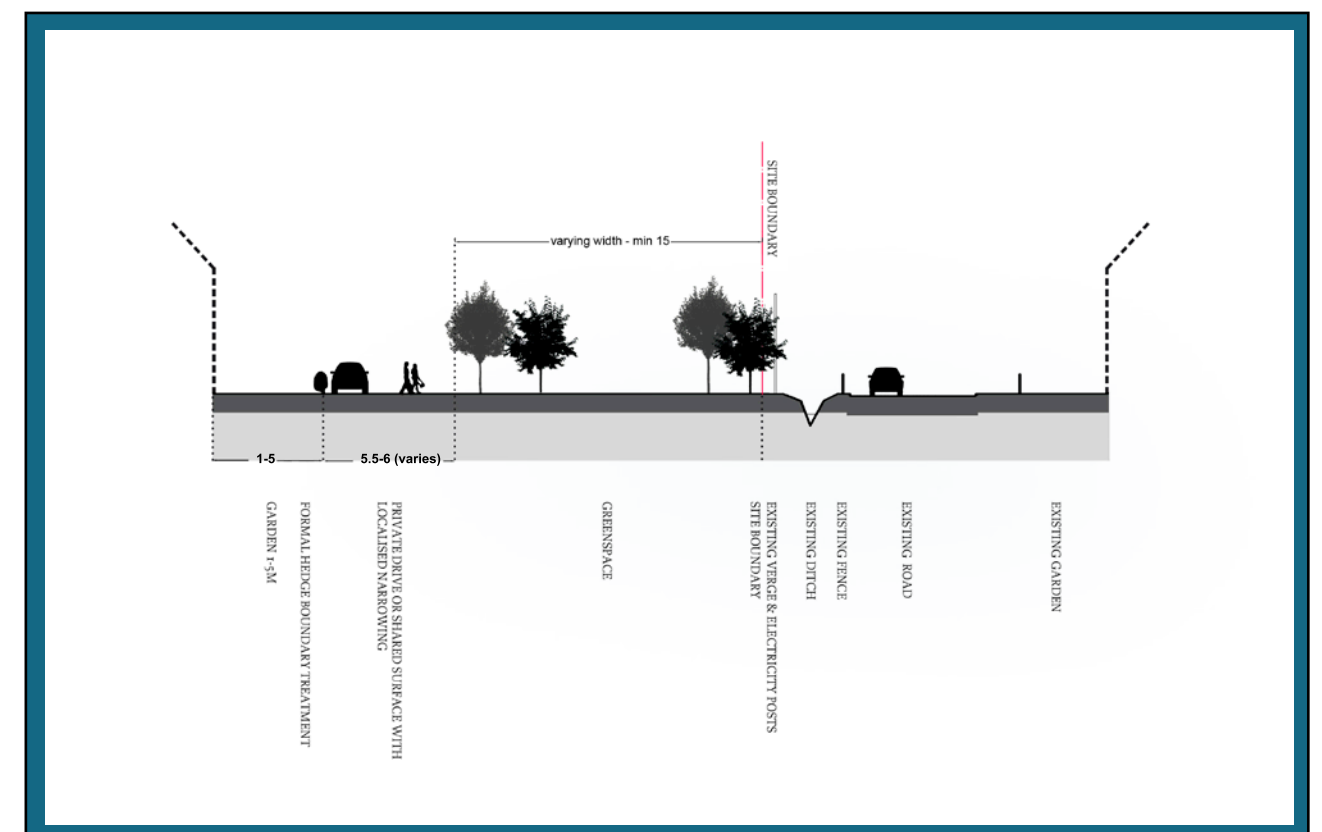
PRIMARY STREET CODE



GREEN EDGE CODE



GREEN SPACE CODE



BUILT EDGE CODE



4.1 STREET CODES:

DESIGN STREETS AS SPACES

The design of all streets at North Heybridge should comply with the design principles set out in the Manual for Streets (DfT 2007) , Manual for Streets 2 (CHIT 2012) and Essex Design Guide Road Types table (refer to Appendix A of this document) which will be adopted in April 2017. Design should equally contribute to the place making and movement functions of the street. The street cross section and the function of all parts of it in terms of providing for movement, parking, landscape, private gardens, drainage and so on should be agreed with the planning authority. There should be clarity as to the function of all parts of all streets.

The North Heybridge SMF will be elaborated on in the respective Design and Access Statements as development comes forward. Those that have already been produced have been considered in the production of the design codes.

CREATE A PERMEABLE NETWORK AND FACILITATE WAY FINDING

All streets and footways including private drives should be barrier free and lead directly to where people want to go. Footways should be overlooked and wide enough for two pedestrians to pass in comfort. Street design should incorporate frequent pedestrian crossings. The use of pedestrian barriers should be avoided. Existing Rights of Way should be incorporated into the site, as outlined in the North Heybridge SMF and the Structure Plan, with the details to be agreed with Essex County Council.

INTEGRATE TRAFFIC CALMING INTO THE STREET SCENE

The arrangement of buildings, spaces and activities can act as a natural traffic calmer and create a pleasant environment for pedestrians and cyclists. Design speeds on secondary and tertiary streets should be kept below 20mph.

REDUCE CLUTTER

The use of traffic signs and other street furniture should be considered carefully and the excessive use of lighting, kerbing, signage and road markings should be avoided. Examples of reducing clutter include:

- Using the minimum of highway design features necessary to make the streets work properly;
- Locating service inspection boxes within buildings or boundary walls;
- Specifying the location and orientation of inspection covers in the footway;
- Ensuring that household bins and recycling containers can be stored off the footway;
- Designing street furniture to be in keeping with its surroundings; and
- Avoid the use of guard railing unless a clear need for it has been identified.

Reference should be made to Local Transport Note 1/08 – Traffic Management and Streetscapes published by the Department for Transport in March 2008, which provides guidance on the de-cluttering of streets and the tightening up of junction layouts.

SELECT APPROPRIATE MATERIALS

One of the main purposes of the code is to ensure that the various development areas are coordinated through their public realm treatment to strengthen local identity and aid way-finding. In the same way, materials choice should reflect the position of each street in the hierarchy. Well-designed shared surfaces encourage low vehicle speeds and make it easier for people to occupy the space without feeling intimidated by motor traffic.

DESIGN FOR PUBLIC TRANSPORT

Streets defined for public transport use should be designed to allow for bus use. Pavements along strategic streets should be wide enough to accommodate bus stops and queues.

DESIGN FOR CYCLISTS

Cycling should be promoted as an alternative mode of transport. The green space codes set out a strategic network of routes which will be designed to be attractive to both cyclists and pedestrians. It is anticipated that these will form the primary provision; however cyclists should also be accommodated either on carriageway or on shared footway-cycle paths running alongside carriageways. Where possible the alignment of routes should avoid the need for cyclists to dismount. The headroom over routes used by cyclists should be 2.7m (min. 2.4m).

PRIMARY STREET CODE

The underlying principle of the code for the primary street is to immediately establish the garden suburb identity. It is a unifying feature which requires continuity of character and identity across plot boundaries. The design of this street should retain overall continuity to strengthen its character and legibility and role in the street hierarchy. The primary street code shows how the same basic street section can be adapted to reflect adjacent land uses. The elements of the cross section which must remain consistent include:

- Consistent landscape strip with formal tree planting parallel on both sides of the street to create an Avenue.
- Parallel pedestrian and cycle access provided
- Pedestrian and cycle access separated from the carriageway by a landscape strip or a verge which also accommodates parallel or bay parking
- A flexible strip adjacent to the carriageway which must be included as either parking, bus layby, additional lane on approach to junctions or extension of the landscape strip.

Primary Streets provide the strategic traffic movement function and link between the existing strategic routes. They link neighbourhoods and also serve non-residential or industrial uses. There is no parking except where off carriageway provision is made. They are wide and spacious streets with a strong landscape component with formal hedge lined boundaries.

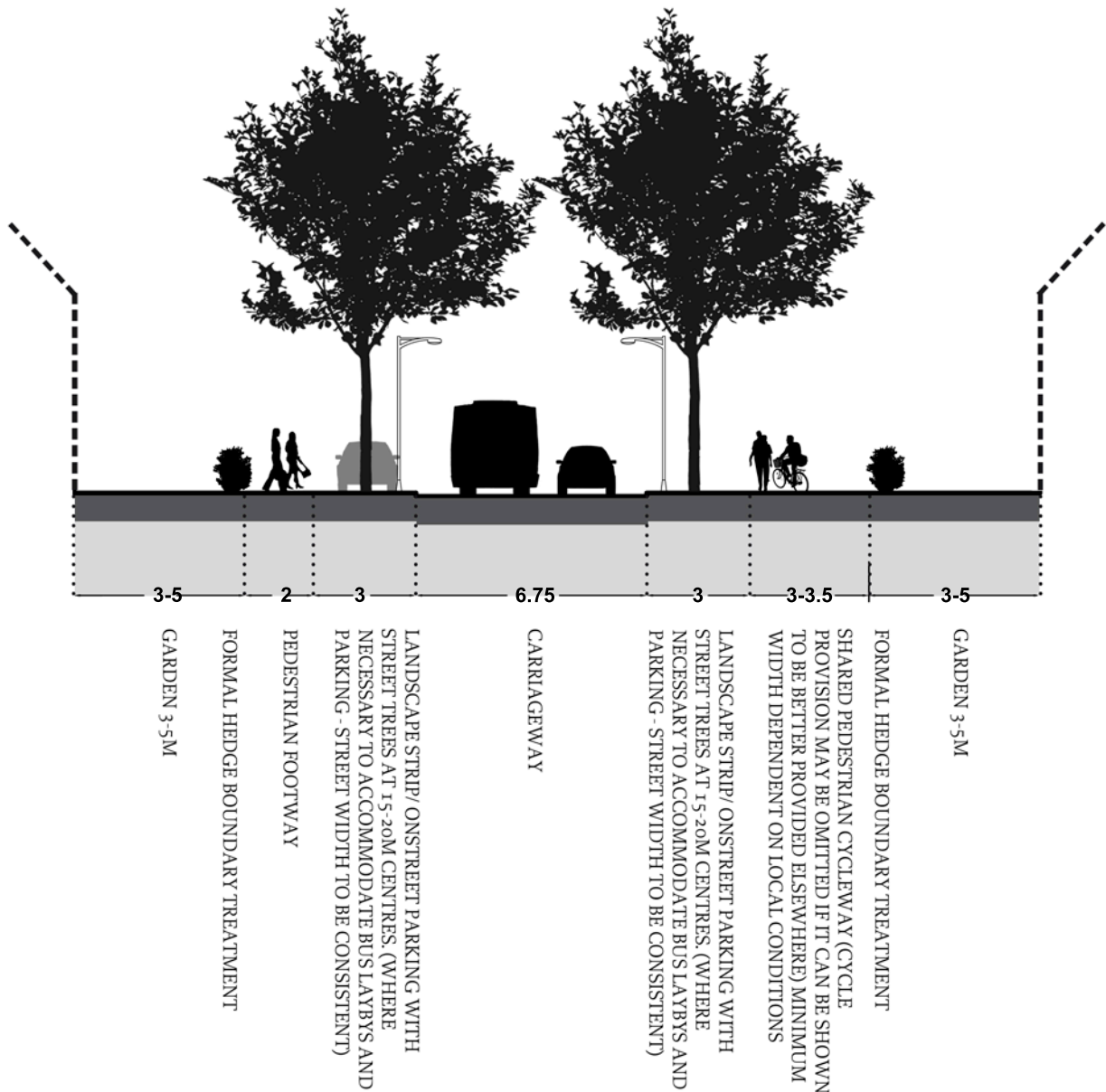
The identity of the street is driven by the physical qualities of garden suburbs outlined in 2.2. It is about creating a spacious and elegant street with a significant landscape component including regular and evenly spaced avenue trees, landscape strips and a hedge boundary.

The landscape elements are key to the garden suburb identity. Formal hedges must be given sufficient space to allow them to flourish and survive as they provide a uniform element along each side of the street. Avenues must be achieved through regular spacing of trees mirrored on both sides of the street and interruptions such as street lighting, side roads, parking and bus laybys should not be prioritised over the regular spacing of trees. The trees must be of sufficient stature to achieve a presence within the street; the species must be selected to achieve the avenue affect whilst also considering the resilience of the trees. Species must be selected from the list included in the Performance Specification table or as otherwise advised by Maldon District Council. Trees will need to be managed under private licence within adopted areas.

Further information on the priorities for street character can be found illustrated on page 30.



NORTH HEYBRIDGE



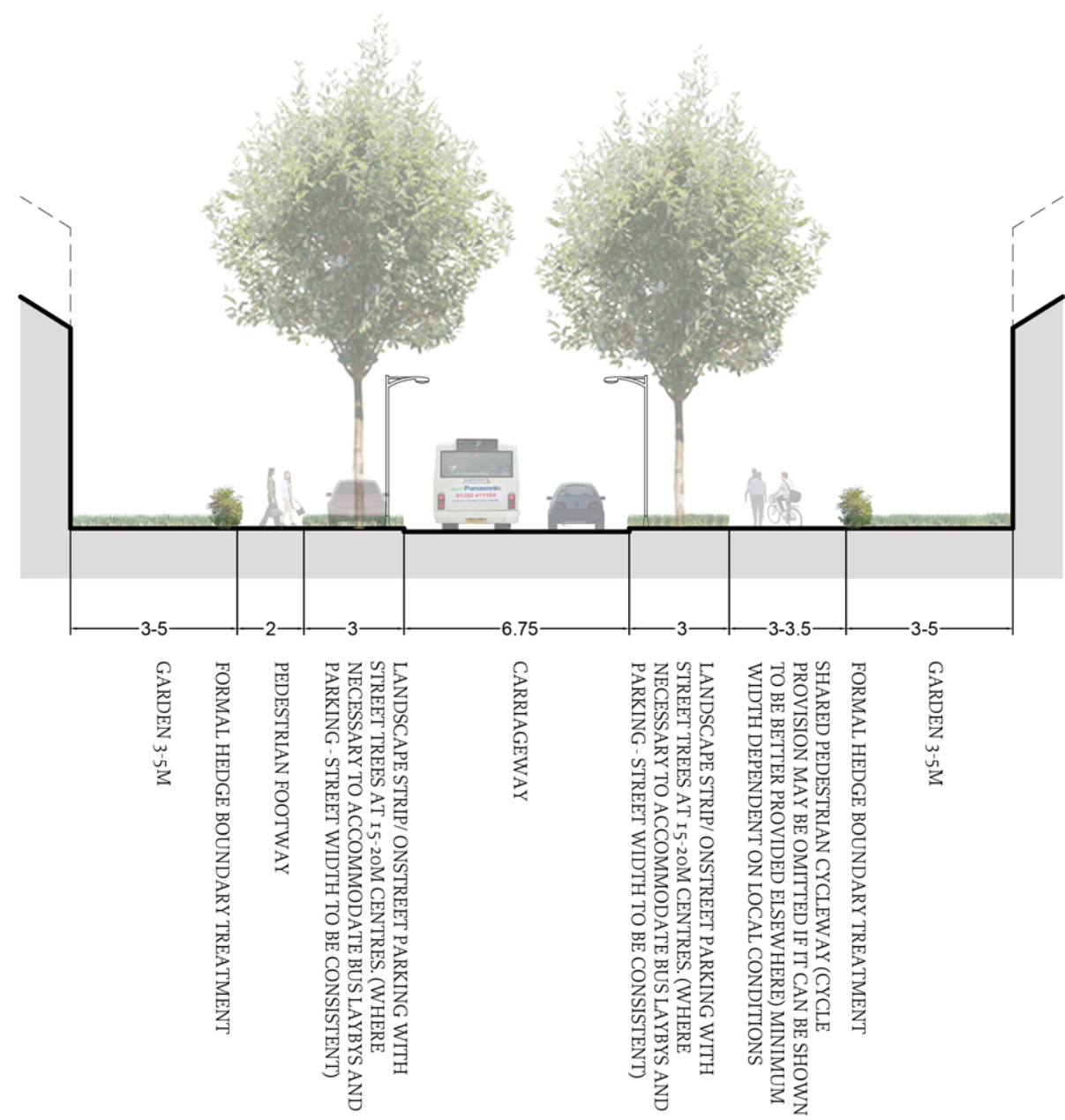
PS PERFORMANCE SPECIFICATION	
Type	Primary Street - Essex Design Guide Road Type Table: C
Core Functions	<ul style="list-style-type: none">Distribute vehicular traffic from the strategic route into and through the heart of the development at North Heybridge.Adequate provision for the effective running of the bus route.Provision of attractive pedestrian and cycle access including regular crossing points.Links neighbourhoods.
Objectives	<ul style="list-style-type: none">Create a spacious and elegant street with a strong and consistent character.Contain a strong landscape component, to contribute to creating a garden suburb by creating a spacious green street with generous front gardens.Achieve a strong relationship with the landscape that it goes through. Any driver or pedestrian using the street should have the sense of passing through a 'green designed' landscape.Designed to be identifiable as a primary route in the hierarchy of internal streets within North Heybridge..To provide overlooking and activity along the street to make it feel safe and attractive to pedestrians.To create a strong urban identity using high quality public realm materials which serve to strengthen the garden city identity.To provide enclosure of the street and avoid 'back fence' development.A simple, low clutter street.Achieve parking standards.
STREET DIMENSIONS	
Carriageway	6.75m / 5.5m (possibility to reduce to 6.0m width, on Primary Streets indicated with dashed line on Structuring Plan, in agreement with council if a bus route were not to be provided) in accordance with Essex Design Guide Road Type table (refer to Appendix A of this document).
Landscape	A flexible strip adjacent to both sides of the carriageway which must be included. Potential for occasional use for parking or bus layby - 3m .
Footway	2m, set back and separated from carriageway by a parallel landscape strip directly adjacent to carriageway.
Cycleway	Shared cycle/footway 3-3.5m - minimum width dependent on local conditions, set back and separated from carriageway by a parallel landscape strip directly adjacent to carriageway. The shared cycle/footway adjacent to the road would provide a cycle route, but where it is demonstrated a better route (i.e. safer, more direct and pleasant) is possible this alternative will be considered. It is particularly important to have a dedicated (off carriageway) cycle route where buses use the road.
Private Curtilage to buildings	3-5m private curtilage clearly defined and enclosed - formal hedge boundary treatment. (Hornbeam or as required by Maldon District Council)
DESIGN CRITERIA	
Speed limit	20mph subject to appropriate traffic calming measures to be agreed with Essex County Council.
Vehicle types to be accommodated	All types allowed.
Direction of traffic	Two-way.
Priority	Vehicular.
Direct vehicular access to plots	Yes.
Traffic calming measures	To be integrated into the design of the street through the use of verge side tree planting / landscape to reduce dominance of traffic on the street, narrowing carriageway width, provision of regular crossing points, occasional parking and bus layouts.
Parking solutions	Permissible solutions - on street parallel, on street perpendicular, on plot. Allocated parking is not acceptable in adoptable areas.
PUBLIC TRANSPORT	
Bus access	Local bus service (except PSo4).
MATERIALS AND LANDSCAPE	
Materials	Blacktop carriageway, standard 125mm highways kerb, resin bonded or blacktop footway. Materials in adoptable areas to be agreed with Essex County Council.
Street furniture	Lighting - in accordance with ECC Operational plan. Bus shelters. As per Section 5.3 Design Guidance (Materials & Public Realm Design Principles).
Street trees	Formal tree planting, equal spacing (15-20m c/c), single species, single stem, high canopy. Some recommended species: Acer Platanoides 'Emerald Queen', Tilia 'Brabant' or Tilia Cordata or as otherwise advised by Maldon District Council. Trees to be managed under private licence within adopted areas.
Hedges	No more than 50% of plot frontage of individual houses occupied by parking spaces. Single species clipped hedge to property boundary. Hornbeam or as required by Maldon District Council.

PS01; PRIMARY STREET THROUGH RESIDENTIAL AREA:



To be used where the primary street is lined by residential development on both sides. The basic primary street section remains consistent through all development areas.

Where this section is used adjacent development should still provide enclosure and natural surveillance of the street.

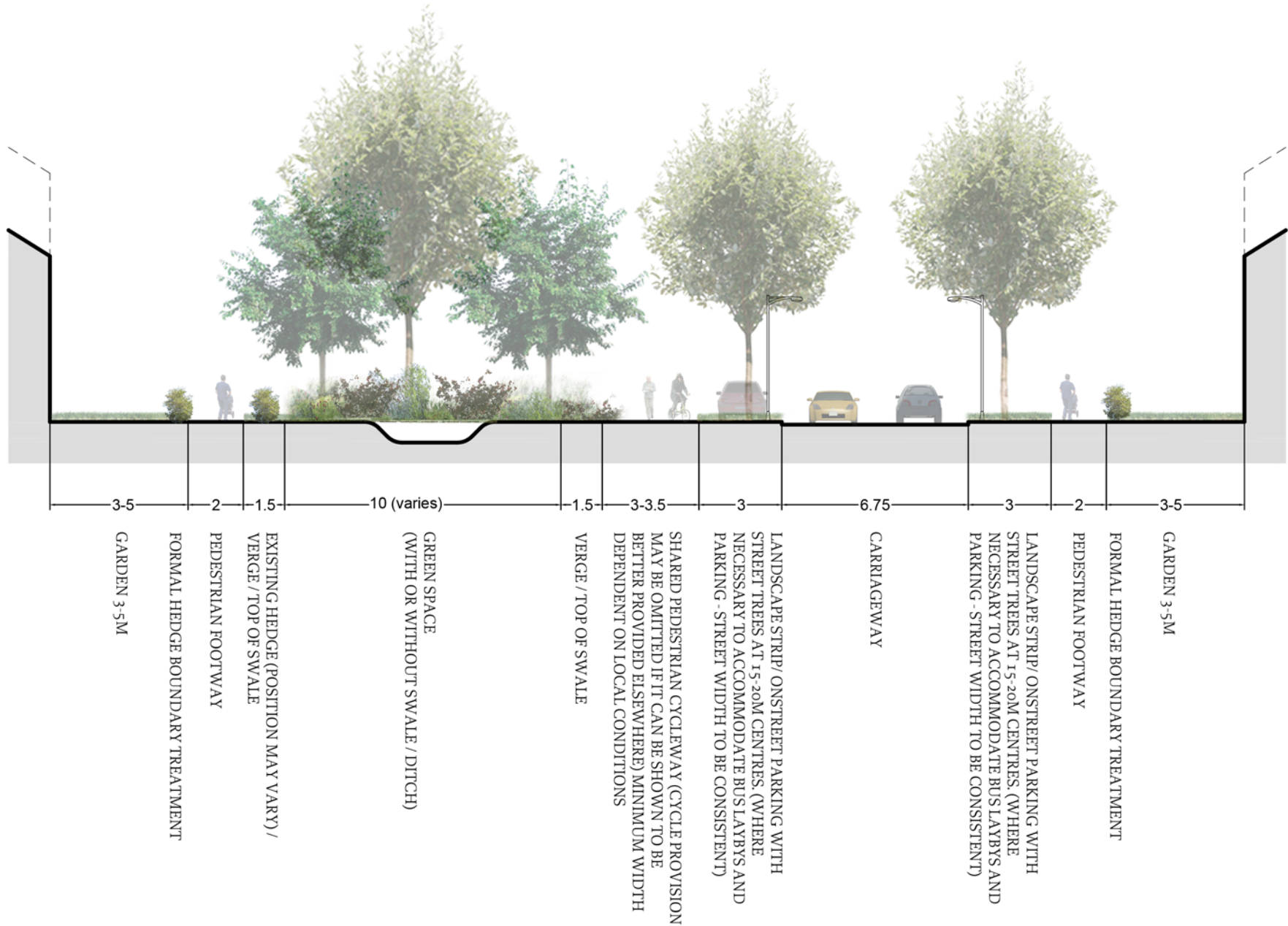


PS02; PRIMARY STREET THROUGH GREEN CORRIDOR



To be used where the primary street runs through a wide greenspace. The location of footpaths and cycle paths maybe altered from that indicated in this illustration provided this can be shown to provide a better solution. Any alternative solutions should not detract from the requirement to ensure continuity in the character of the primary street network.

In more urban areas such as the Local Centre the 3m landscape strip can be substituted with paving to provide a much wider section of footpath, street trees should still be provided along the same alignment within an urban tree pit.



PS03; PRIMARY STREET ADJACENT TO HEYBRIDGE WOOD



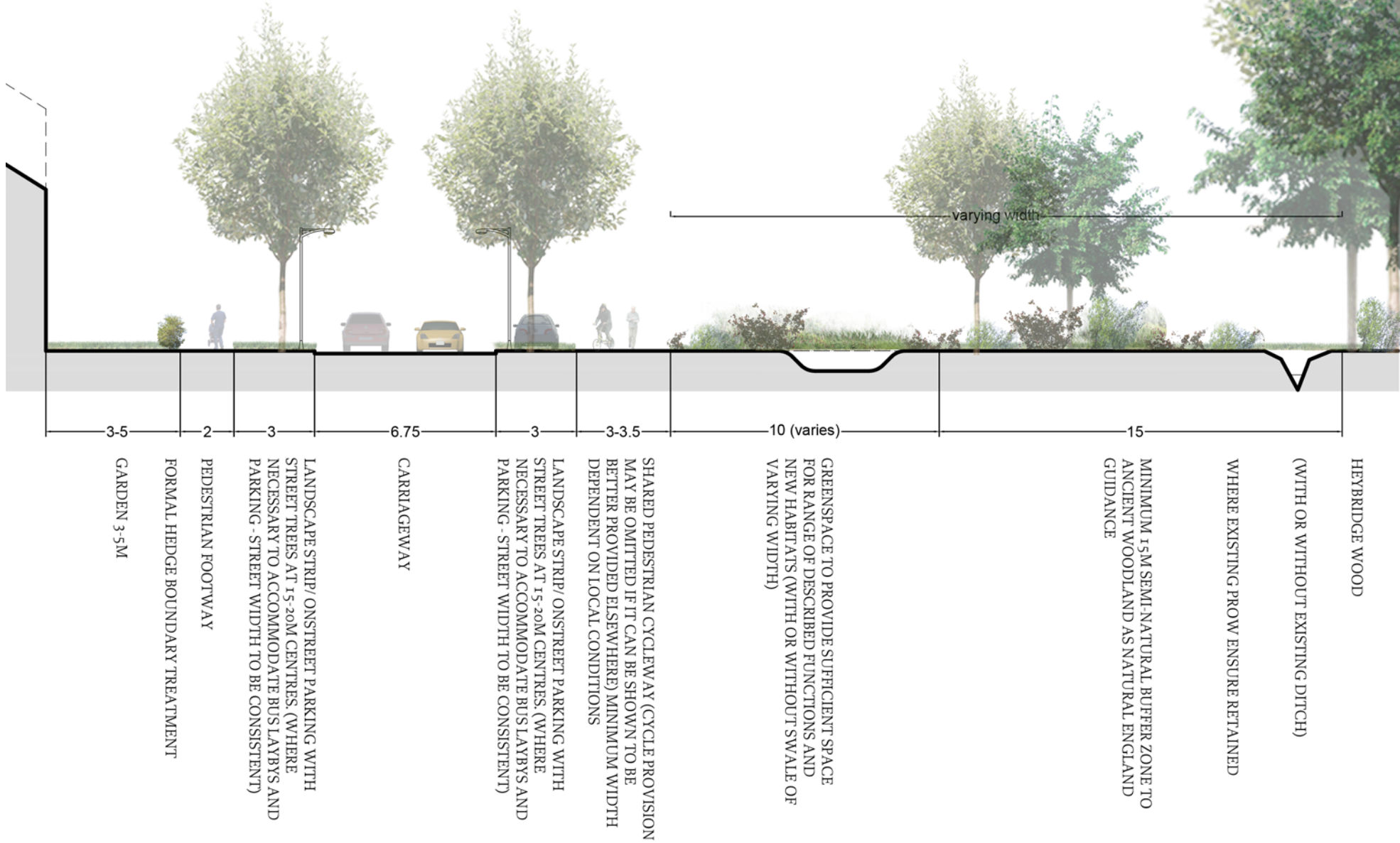
Use of the primary street section where development only runs alongside one side of the street should remain consistent to ensure continuity in the character of the primary street network. The street section can be adapted by providing the footpath and cycleway within the adjacent green buffer on the undeveloped side of the street adjacent to Heybridge Wood.

The minimum 15m buffer zone to Heybridge Wood (ancient woodland) is required by Natural England guidance and no footpaths or cycle paths should be created within this zone. Existing Public Rights of Way should be retained where they run through these areas.

The minimum width of greenspace provided should be sufficient to allow these spaces to provide for a range of functions such as surface water attenuation (swales/ditches), play (LEAPS, NEAPS, walking & cycling route) and planting (trees, hedges) outside of the minimum 15m buffer. The greenspace in conjunction with the minimum 15m buffer zone should provide an ecological buffer to protect the existing ecology and provide new habitats.

Reference should also be made to GE02 which provides an illustration of treatment around Heybridge Wood.

The location of footpaths and cycle paths maybe altered from those indicated provided it can be demonstrated that a better solution is provided . Any alternative solutions should not detract from the requirement to ensure continuity in the character of the primary street network.



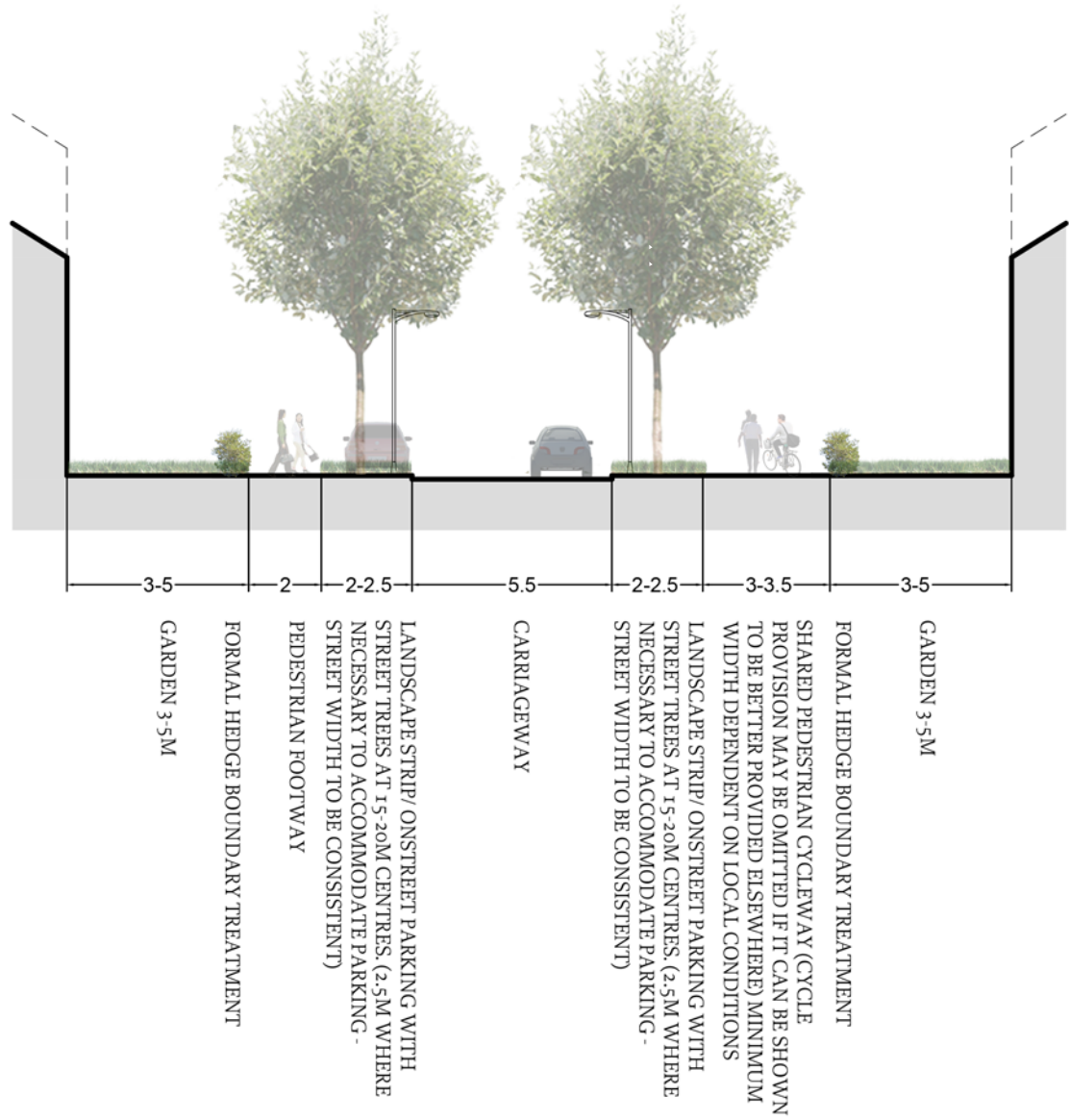
PS04; PRIMARY STREET FOR ACCESS



To be used where the primary street provides access from Holloway and Broad Street Green and the primary street does not include a bus route.

To be used where the primary street is lined by residential development on both sides. The basic primary street section remains consistent through all development areas, this illustration allows for a narrower carriageway (as Essex Design Guide Road Type D, Access Road) of 5.5m.

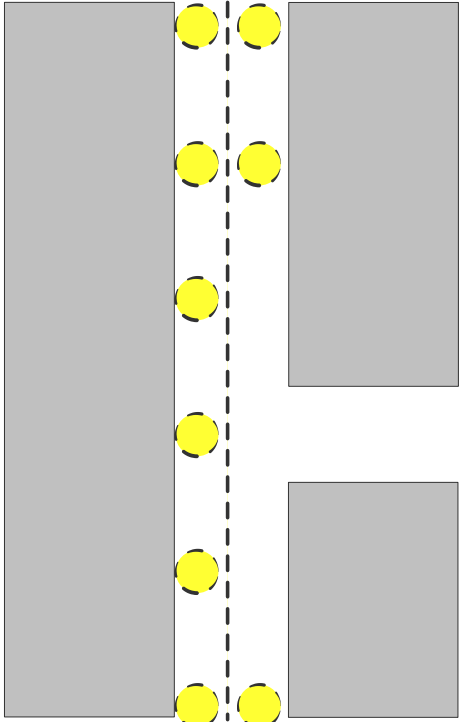
Where this section is used adjacent development should still provide enclosure and natural surveillance of the street.



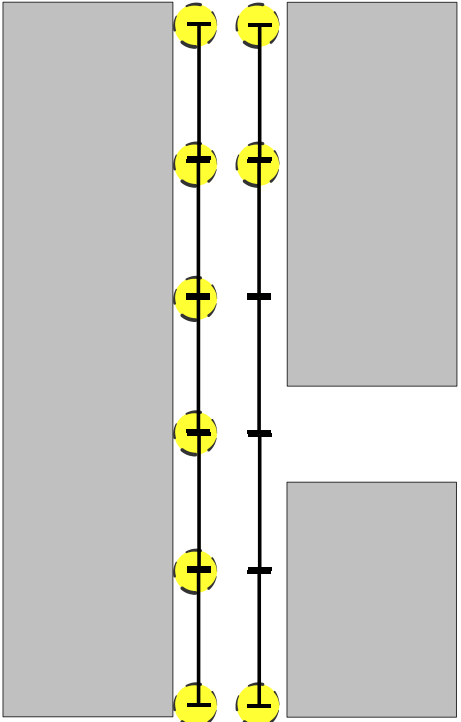
PRIORITIES FOR PRIMARY STREET CHARACTER

Primary streets are a unifying feature with continuity of character required across ownership and plot boundaries. The primary driver should be tree locations directly opposite each other in the street and the other elements (lighting, parking, crossovers etc.), have to fit around this.

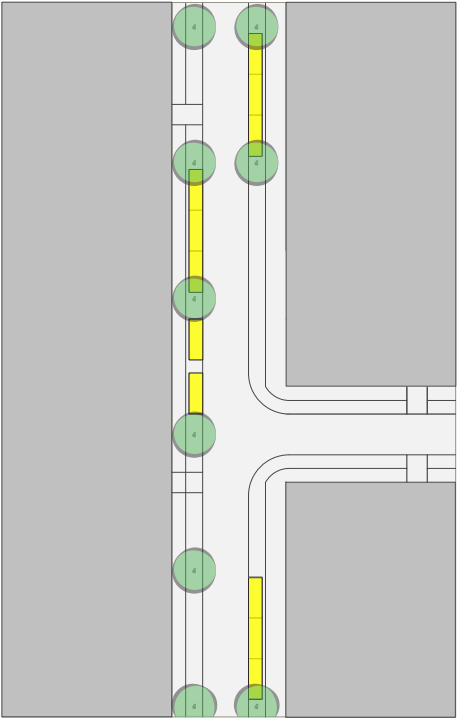
The priority is to achieve a formal mature avenue. Tree coverage must be even along the street. A gap toothed and uneven result will not be permitted. Note: Schemes will be reviewed against this principle and those deemed unable to achieve it will not be approved.



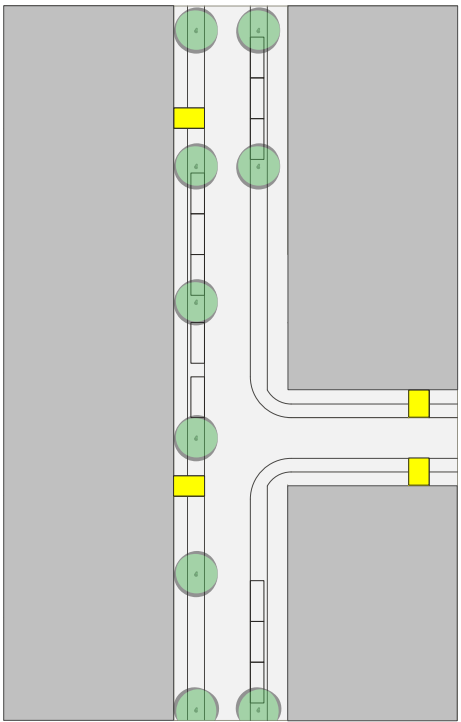
CONSISTENTLY SIZED TREES BOTH SIDES OF STREET, OPPOSITE EACH OTHER TO FORM AN AVENUE, ALONG THE LENGTH OF THE PRIMARY STREET



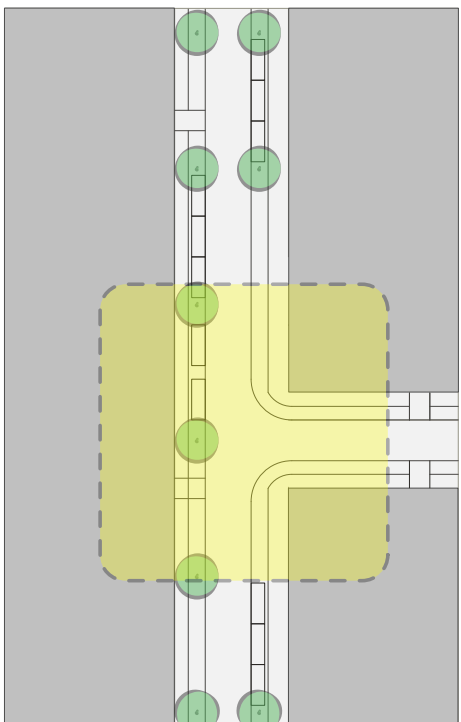
EQUALLY SPACED TREES, CONSISTENT CENTRES (BETWEEN 15-20M), ALONG LENGTH OF STREET WHERE POSSIBLE. REGULAR SPACING TO AVOID GAPS IN TREE COVERAGE



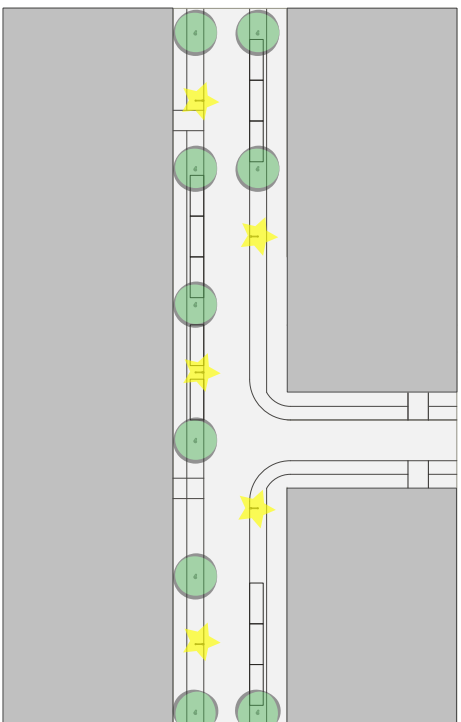
MAXIMUM 3 PARKING SPACES BETWEEN STREET TREES, PARKING TO FIT BETWEEN THE AVENUE TREES



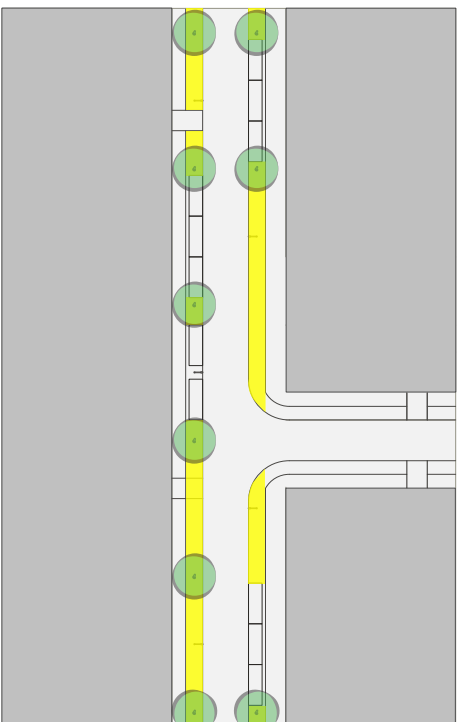
POSITION CROSSOVERS AND CONTROL THE NUMBER OF CROSSOVERS BETWEEN THE AVENUE STREET TREES



ENSURE CONSISTENT TREATMENT AND EQUALLY SPACED TREES AT ROAD JUNCTIONS



CONSIDER POSITION OF STREET LIGHTS AND FURNITURE WITHIN VERGES AND BETWEEN STREET TREES



LANDSCAPE CHARACTER OF STREET IS ACHIEVED BY ENSURING MINIMUM 60% VERGE TREATED AS LANDSCAPE STRIP (PLANTED OR GRASS)

4.2 GREEN EDGE CODE

This section of the code should be applied to all areas where development meets the woodland buffer or the transition with open countryside. Because of the amount of countryside edge significant areas of housing will abut green space, it is therefore critical that the relationship between green space and development is well planned. There is flexibility as to how the juxtaposition of housing and green space will be managed provided that a number of key principles are adhered to:

AVOID REAR GARDENS BACKING ON TO GREEN SPACE

In principle green spaces should be overlooked by development. Rear property boundaries should not be used to enclose or delineate green space except in exceptional circumstances; measures must be taken to ensure regular points of access and overlooking are still provided to the green space.

ANIMATE THE EDGES

Ensure buildings along green ways or surrounding green space are enlivened by providing active uses and entrances overlooking the green space.

SAFETY AND SECURITY

Ensure the juxtaposition of green spaces and development is designed in accordance with the principles of ‘Secured by Design’

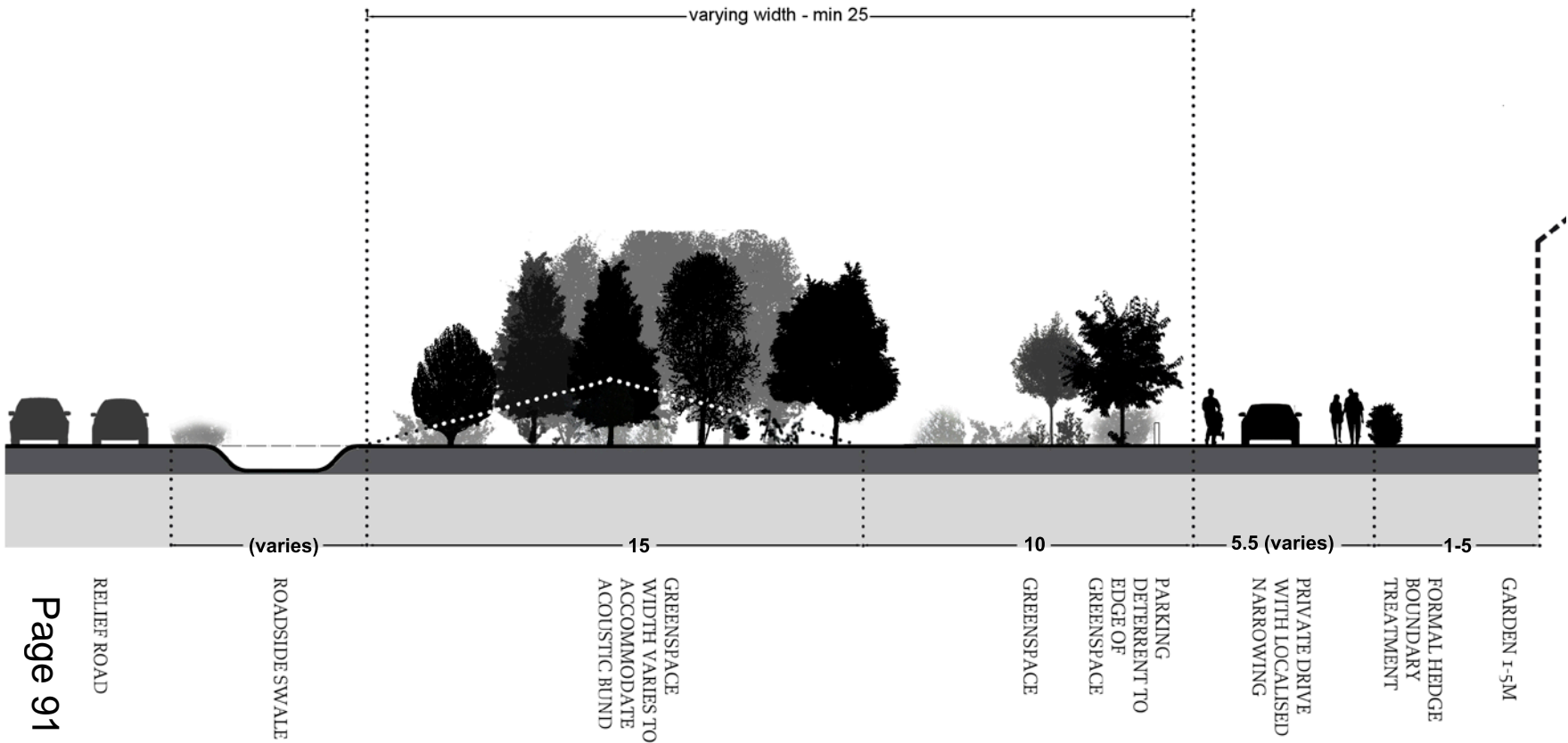
The Green Edge code is followed by a number of arrangements which illustrate these principles and the green edge conditions which will be acceptable.

NOISE MITIGATION

Orientating development so that buildings ‘front’ onto the Relief road edge will allow the buildings to offer a degree of noise mitigation (in combination with engineered solutions) to the private outdoor space to the rear of these properties and to development further into the site.



NORTH HEYBRIDGE



GE PERFORMANCE SPECIFICATION	
Type	<ul style="list-style-type: none">Green Edge.
Core Functions	<ul style="list-style-type: none">Present a development edge that relates to the countryside setting at the edge of the North Heybridge garden suburb and to Heybridge Wood, sports pitches and allotments.Vehicular, pedestrian & cycling access to residential plots.Informal shared space route.Low level of traffic.Support the delivery of open space and ecological enhancements.
Design Objectives	<ul style="list-style-type: none">Present a positive development frontage addressing routes and green edges.Support the appeal of shared surface streets as access and movement routes.Provide an integrated landscape design that combines ecology, recreation and amenity with a semi natural character.Protection to Ancient Woodland, green space / wildlife space.
STREET DIMENSIONS	
Carriageway	Private drive (Road Type G) 5.5m width for the first 6 metres but can taper down to a lesser width and/or with localised narrowing, in accordance with Essex Design Guide Road Type table (refer to Appendix A of this document).
Access	No dedicated footway or cycleway - shared surface.
Curtilage	1-5m garden with formal clipped hedge boundary treatment. Hornbeam or as required by Maldon District Council.
Landscape	Parking within the areas of open space must be deterred with the use of well integrated physical deterrents such as drainage ditches, planting and low bollards.
DESIGN CRITERIA	
Lighting	Low level lighting to be considered in agreement of Essex County Council.
Parking	None on street. Allocated parking not acceptable in adoptable areas.
Direct vehicular access to plots	Yes.
MATERIALS AND LANDSCAPE	
Materials	As per Design Guidance. Materials in adoptable areas to be agreed with Essex County Council.
Street trees	Occasional and informal. Trees to be managed under private licence within adopted areas.
Other landscape	Landscape treatment should reflect the relevant character context areas.

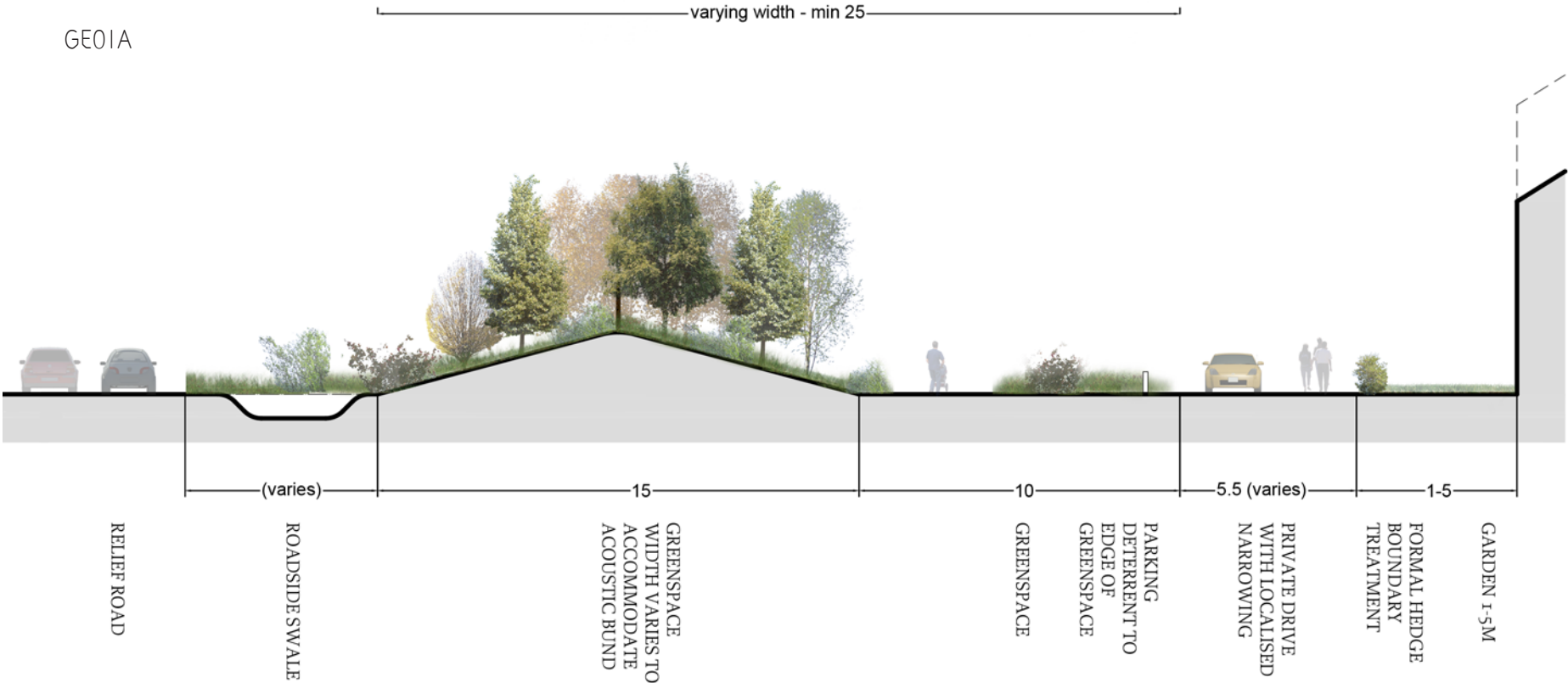
GE01; GREEN EDGE - RURAL EDGE



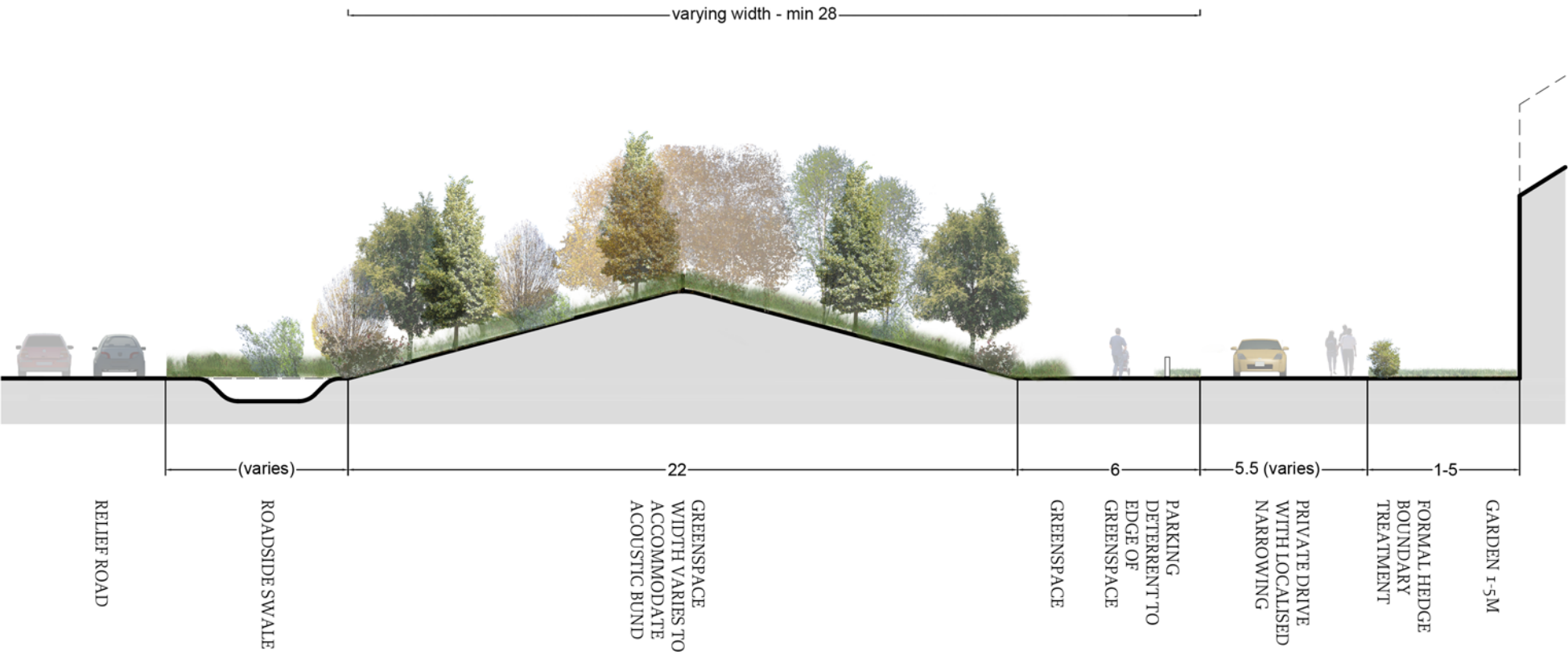
This arrangement allows for overlooking of the countryside edge through the provision of a shared access way alongside the green edge, and to provide vehicle access to parking in front of properties. It also allows for gable ends with regular access points in the form of side streets to encourage activity/use and provide an additional level of natural surveillance. The green edge codes encourage the use of this arrangement where quieter areas of open space would be advantageous in establishing attractive walking routes. Must include a clearly defined threshold between public and private land. Parking within the areas of open space must be deterred with the use of well integrated physical deterrents such as drainage ditches, planting and low bollards.

The minimum width of greenspace provided should be varies, relating to the different options of bund width, to allow these spaces to provide for a range of functions such as surface water attenuation (swales/ditches), play (LEAPS, NEAPS, walking & cycling route) and planting (trees, hedges). The greenspace also provides for the inclusion of an acoustic bund and fence as a buffer to the Relief road.

GE01A



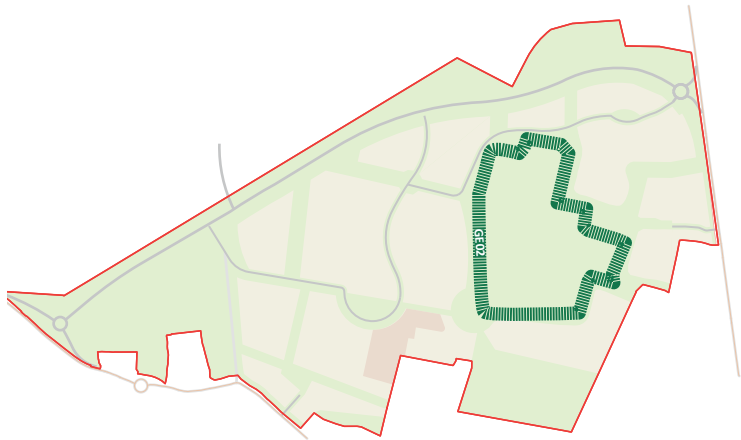
GE01B



GE01C



GE02; GREEN EDGE - ADJACENT TO HEYBRIDGE WOOD

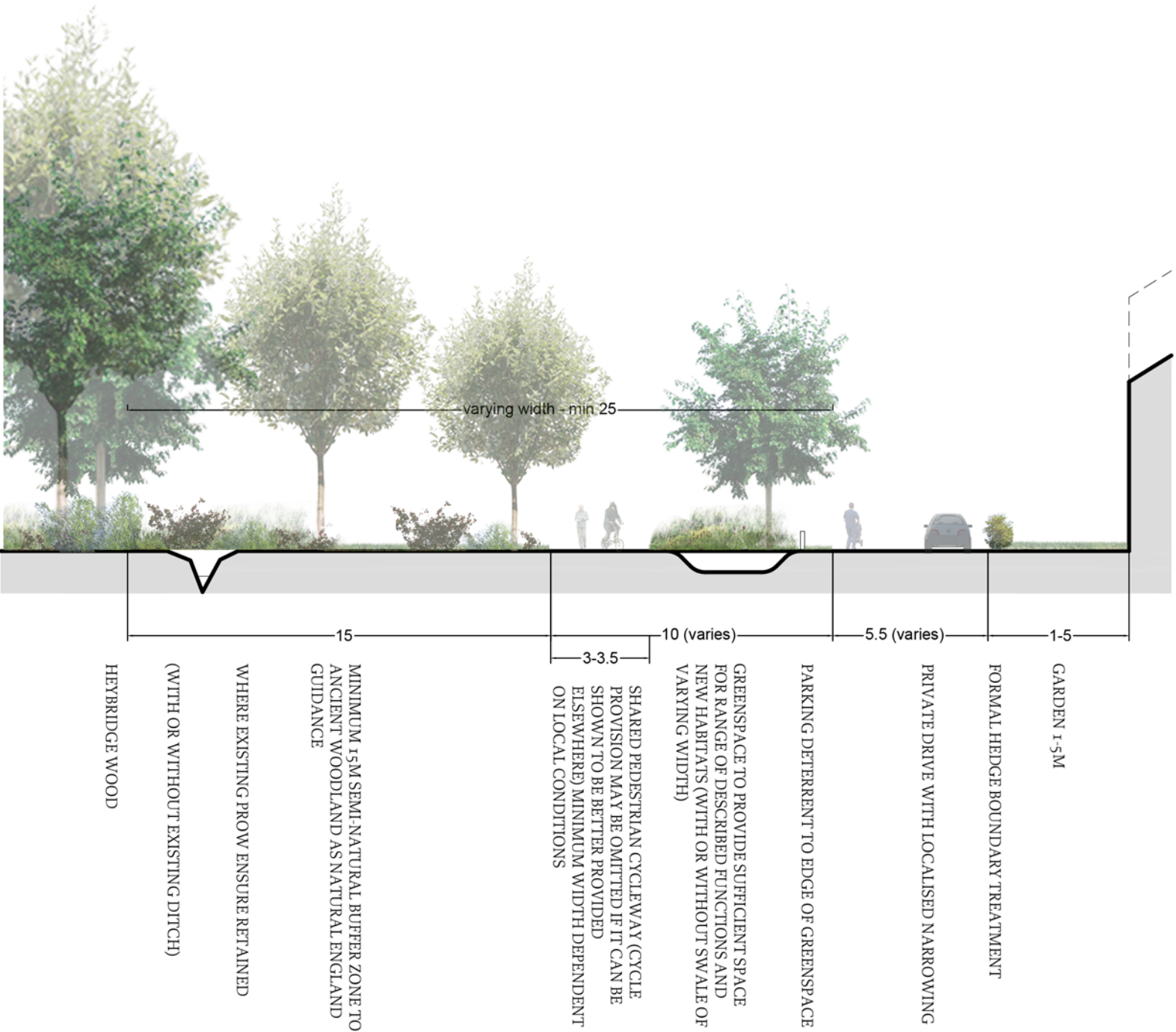


This arrangement addresses the location where development is located adjacent to Heybridge Wood. The minimum 15m buffer zone to the Ancient Woodland is required by Natural England guidance and no footpaths or cycle paths should be created within this zone. Existing Public Rights of Way should be retained where they run through these areas.

The minimum width of greenspace provided should be sufficient to allow these spaces to provide for a range of functions such as surface water attenuation (swales/ditches), play (LEAPS, NEAPS, walking & cycling route) and planting (trees, hedges) outside of the minimum 15m buffer. The greenspace in conjunction with the minimum 15m buffer zone should provide an ecological buffer to protect the existing ecology and provide new habitats.

Reference should also be made to PS03 which provides an illustration of treatment around the Ancient Woodland where the primary street runs adjacent to the woodland.

Where provided, the location of footpaths and cycle paths maybe altered from that indicated in this illustration provided this can be shown to provide a better solution.



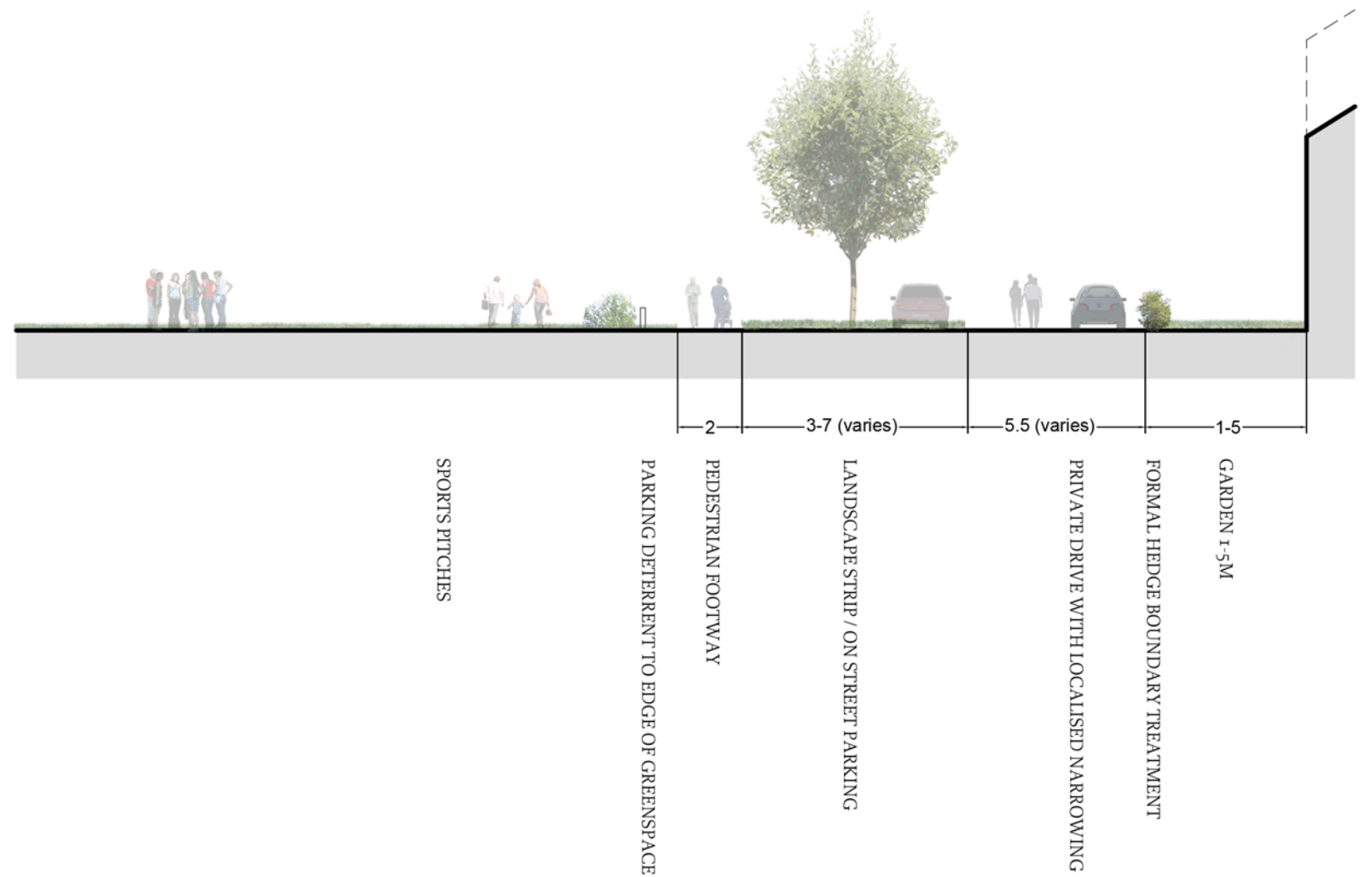
GE03; GREEN EDGE - ADJACENT TO SPORTS PITCHES / ALLOTMENTS



This arrangement requires overlooking of the sports pitches and allotments through the provision of a shared access way alongside the green edge to provide vehicle access to parking in front of properties. It also allows for gable ends with regular access points in the form of side streets to encourage activity/use and provide an additional level of natural surveillance. Must include a clearly defined threshold between public and private land. Parking within the areas of open space must be deterred with the use of well integrated physical deterrents such as drainage ditches, planting and low bollards.

On-street parking in this location may also be provided as perpendicular parking bays, as outlined where parking is required for the sports facilities and allotments.

As described in Section 5.2 Parking landscaping or other interventions work well to break up perpendicular parking bays, to reduce their dominance on the street. A maximum of six spaces should be provided in any group before being broken up by a landscape bay or street tree planting.



4.3 GREEN SPACE CODE

Green Infrastructure in North Heybridge needs to fulfil many important roles; it is essential for expressing the physical design principles of Garden Suburbs and providing new residents contact with nature. It will provide a buffer to residential areas and busy transport corridors, meet the majority of the recreational needs of new residents as well as providing opportunities for increased biodiversity, water management, amenity and shelter. In order to achieve these functions strategic green infrastructure corridors are identified in the North Heybridge SMF and set out in the Structuring Plan. The minimum requirements of these strategic elements are described in the following Green Space Codes. Masterplanning work which forms part of any development proposals will need to demonstrate how the strategic green spaces contained in the codes and the green infrastructure requirements set out in the North Heybridge SMF will be implemented. In addition to the strategic design codes, the following general principles should be applied to the design of green spaces within North Heybridge.

Connected: The strategic green spaces codes seek to provide a connected grid of greenways and green fingers, placing green space at the heart of all areas of development. This grid also connects into the surrounding landscape and neighbourhoods. It is essential that areas of development provide direct access into this network with a regular network of streets which lead to the green spaces. Road crossings to the strategic green spaces should be minimised to ensure that they are attractive to pedestrians and cyclists as continuous routes.

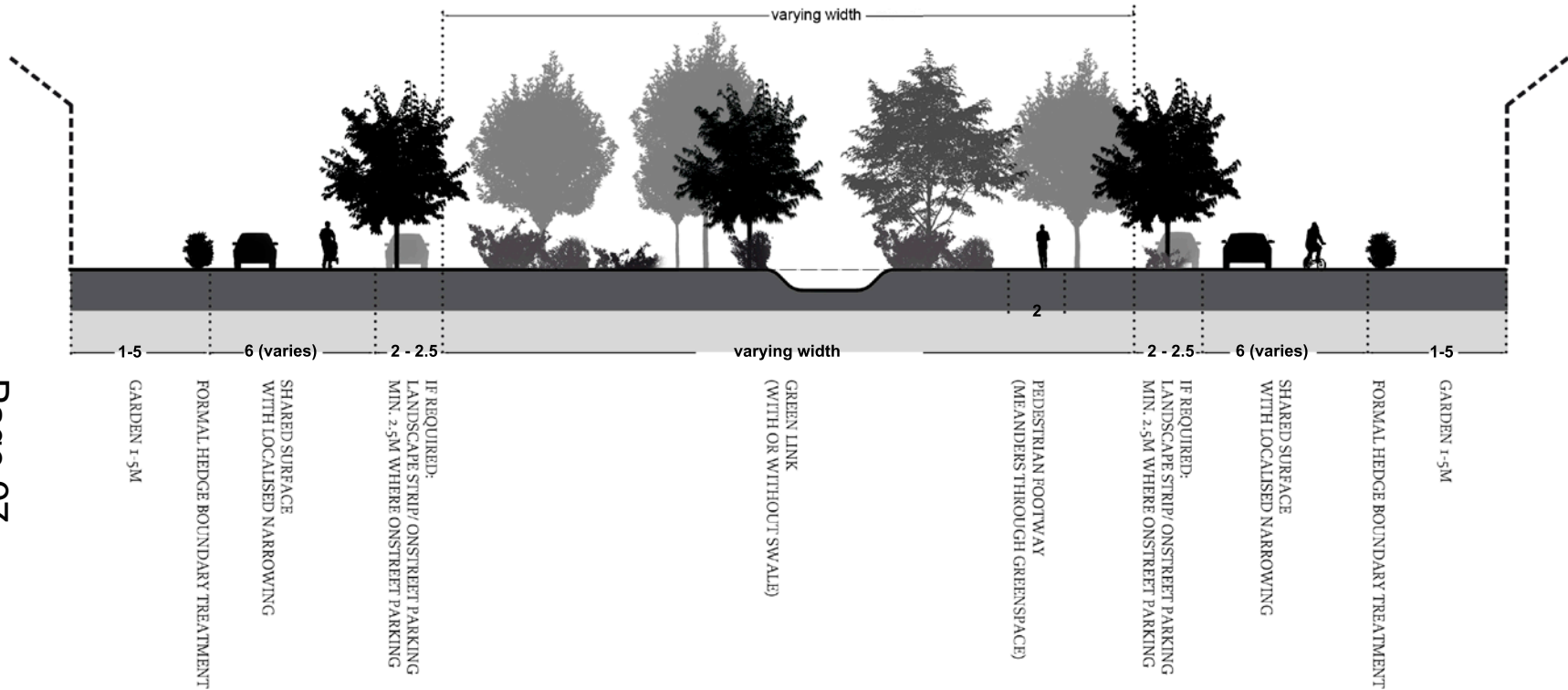
Multifunctional: This is one level of the green space required for North Heybridge. While it is anticipated that they will contribute towards meeting the green infrastructure requirement, additional types of green infrastructure will be required such as sports pitches, play areas etc. Where possible these areas should be provided in widened sections of the green infrastructure grid to ensure accessibility and that all green elements are connected and can provide multiple functions.

Overlooked: As set out in more detail in the green edge codes, the green spaces must be designed to be well enclosed and overlooked. This is fundamentally important in ensuring green spaces are well integrated into the built form which along with giving them a clearly defined role, will avoid provision of large areas of unneeded and unused open space.

Beautiful: Green spaces should be planned and designed as a central part of Garden Suburb concept offering value to surrounding development. Green spaces must be designed to be attractive and useable with the identity and materials being driven by the character of the area in which they sit.

The following illustrations provide a number of arrangements which illustrate these principles and the green space environments which will be acceptable.





GS PERFORMANCE SPECIFICATION	
Type	<ul style="list-style-type: none">Greenways and Green Fingers
Core Functions	<ul style="list-style-type: none">Large scale green infrastructure feature which bring a robust and defining structure to the Garden Suburb.Continuous and connected green corridors providing strategic pedestrian/cycle link with a wider network of walking and cycling.Visual break to development and providing long views.Connects people and nature.Ensure landscape character, natural conditions and topography are integrated into the suburb.
Design Objectives	<ul style="list-style-type: none">Use the existing landscape structure of the Ancient Woodland and existing watercourses and ditches, hedges and trees to provide a safe convenient and overlooked routes through the main body of the site.Active and vibrant multifunctional green corridor.Opportunity for ecological enhancements and informal play spaces,Corridor to have an informal, semi natural character to planting,Sustainable transport routes for walking and cycling.Accommodate SUDS.To offer protection to existing green space / wildlife space.
DESIGN CRITERIA	
Scale	Width specified in code for design function. In exceptional circumstances a minimum width of 13-16m may be acceptable where SUDs and swales are not included. Where this is the case it must be shown that this width does not impede the functions contained within the greenspace nor minimise the impact of the bold structuring role these greenspaces have within the Strategic Masterplan Framework.
Access	Pedestrian and cycle.
Lighting	Consider low level lighting close to connections to local centre/key landscape spaces, subject to agreement of Essex County Council.
MATERIALS AND LANDSCAPE	
Materials	As per Design Guidance. Pedestrian footways through parks, green spaces, greenways and green fingers to be self-binding aggregate or other porous surface dressing. Materials in adoptable areas to be agreed with Essex County Council.
Landscape	Landscape treatment should respond to the appropriate character context area.

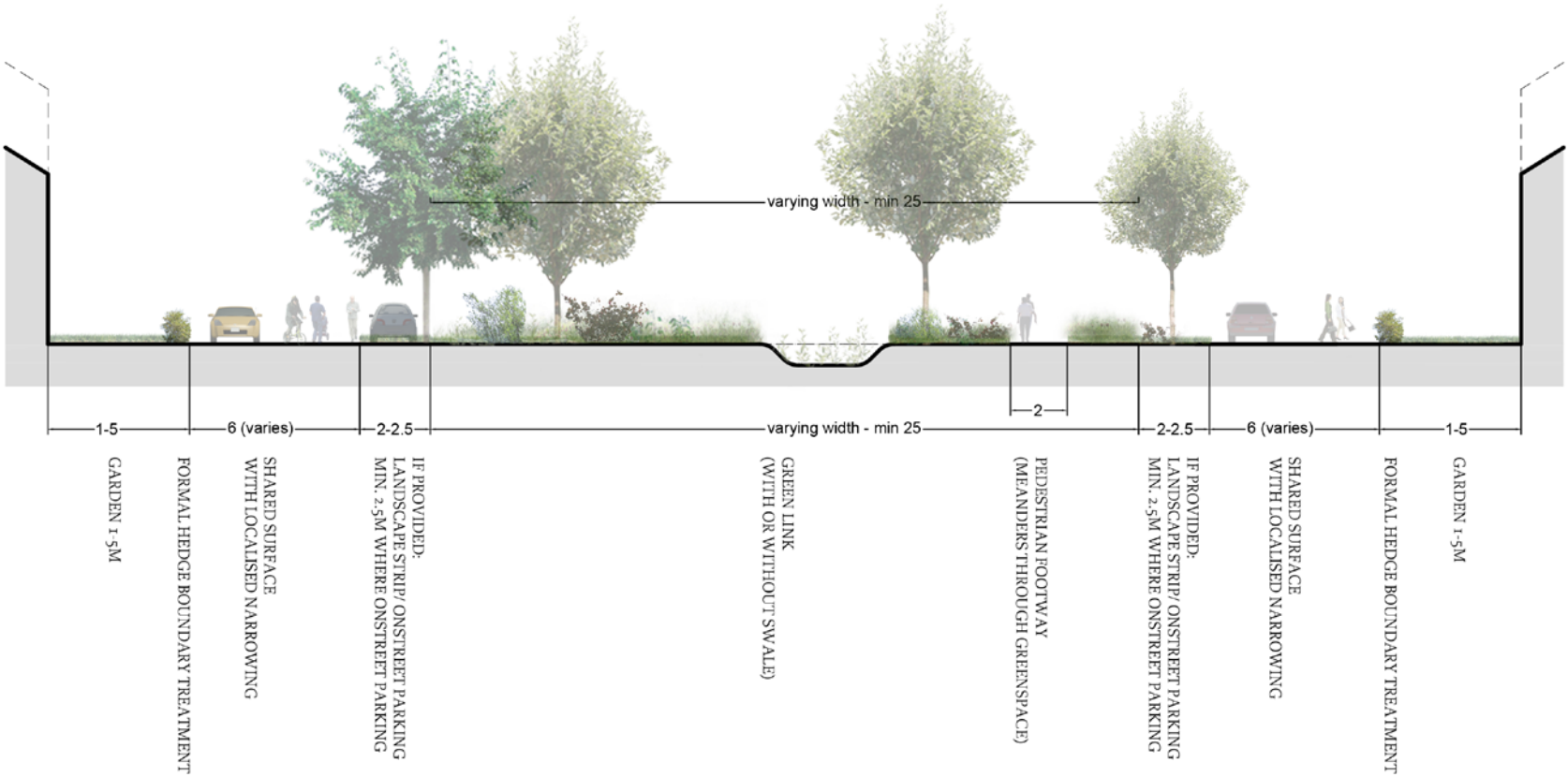
GS01: GREENSPACE - GREENWAYS



The greenway is a multi-functional green corridor with a minimum width of 25m, with shared streets to both sides. The greenway may accommodate a number of uses and functions including accessible greenspace, planting, pedestrian and cycling routes, play space, SUDs and wildlife corridors. Planting to incorporate existing trees and hedges and maintain and enhance biodiversity.

This design code provides shared streets on both sides of the greenspace. This solution gives the opportunity to provide vehicle access to plots, on-street and on-plot parking and overlooking of the greenspace. Alternative solutions, such as a footpath in place of the shared surface, may be acceptable where it can be demonstrated that this would work and does not lead to reliance on rear courtyard parking and a poor relationship between the housing and greenspace.

This solution may be adapted to include other lower order roads (in place of the shared surface streets) that run alongside greenways.



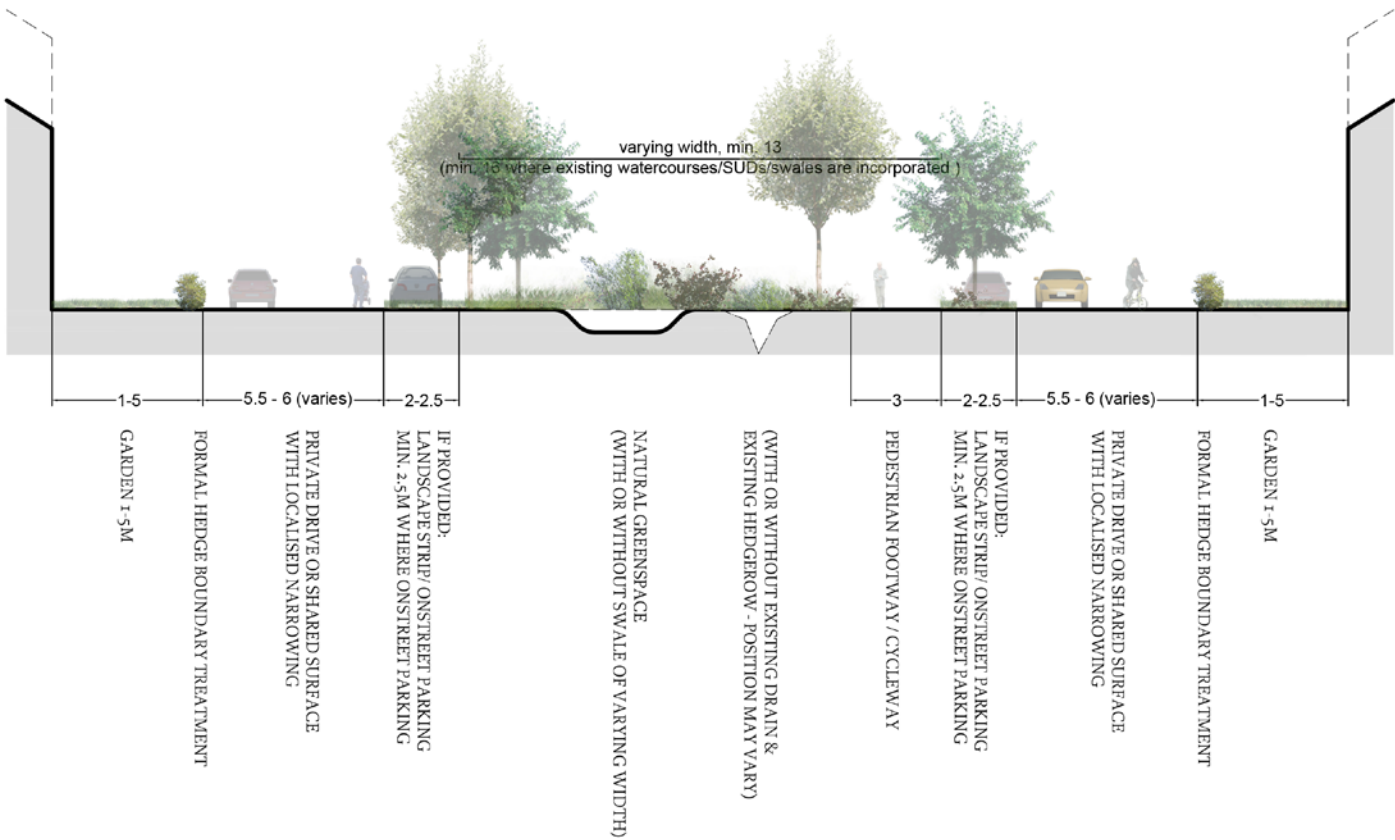
GS02; GREENSPACE - GREEN FINGERS



The green finger is a multi-functional green corridor with a minimum width of 16m (min. 13 where no SUDs/swale), with shared streets to both sides. The greenfinger may accommodate a number of uses and functions including accessible greenspace, planting, pedestrian and cycling routes, play space, existing watercourses, SUDs and wildlife corridors. The green fingers provide a second tier to the linear multifunctional space. The landscape will provide ecological enhancements and contain a significant proportion of native planting.

This design code provides shared streets on both sides of the greenspace. This solution gives the opportunity to provide vehicle access to plots, on-street and on-plot parking and overlooking of the greenspace. Alternative solutions, such as a footpath in place of the shared surface, may be acceptable where it can be demonstrated that this would work and does not lead to reliance on rear courtyard parking and a poor relationship between the housing and greenspace.

This solution may be adapted to include the higher order roads - shared space streets - (in place of the private drive) that run along green fingers.

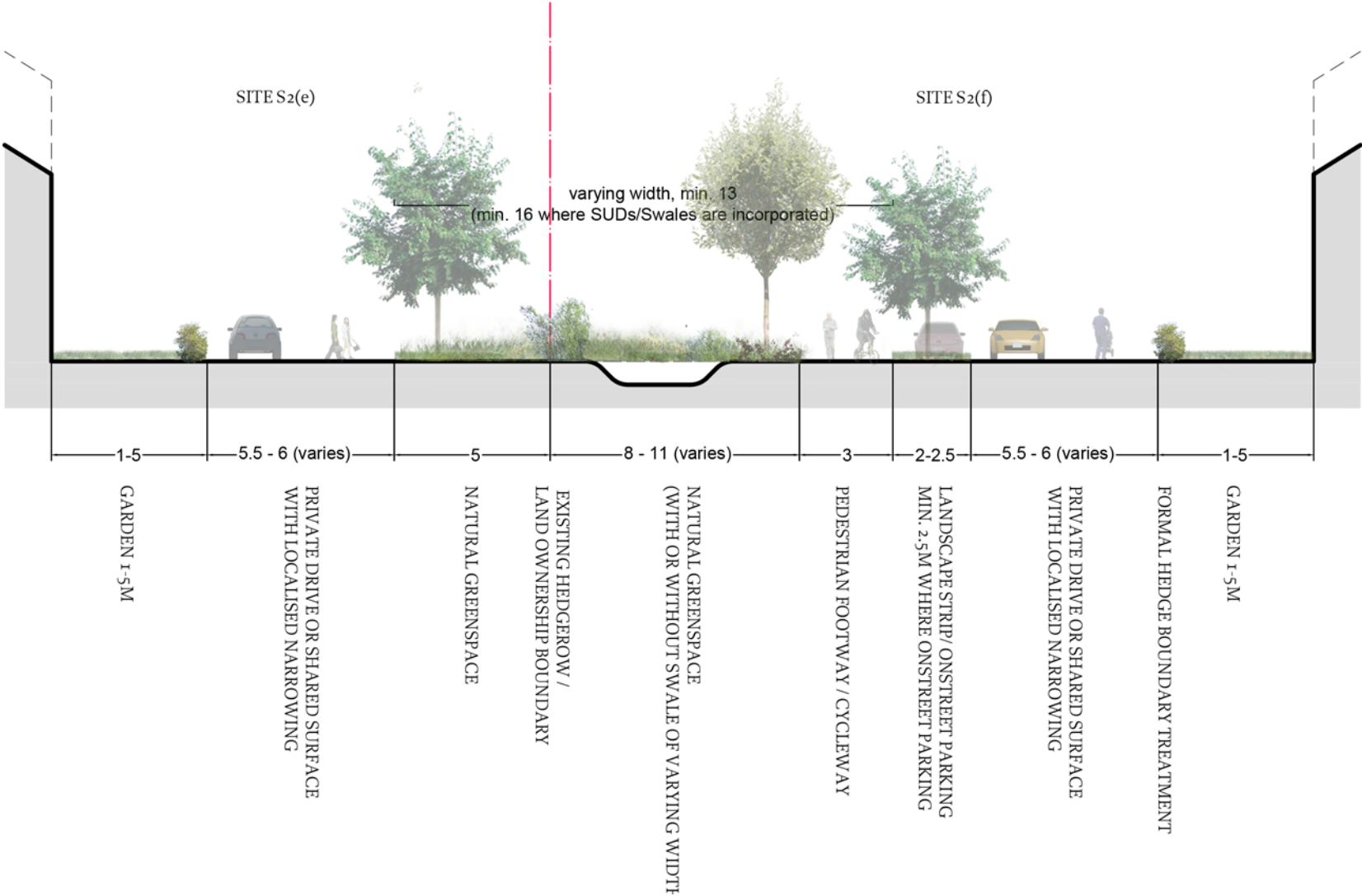


GS03: GREENSPACE - ADJACENT SITE BOUNDARIES



This green finger is a multi-functional green corridor with a minimum width of 16m (min. 13 where no SUDs/swale), with shared streets to both sides. The greenfinger may accommodate a number of uses and functions including accessible greenspace, planting, pedestrian and cycling routes, play space and SUDs. The green fingers provide a second tier to the linear multifunctional space. The landscape will provide ecological enhancements and contain a significant proportion of native planting.

This design code provides shared streets on both sides of the greenspace. This solution gives the opportunity to provide vehicle access to plots, on-street and on-plot parking and overlooking of the greenspace. Alternative solutions, such as a footpath in place of the shared surface, may be acceptable where it can be demonstrated that this would work and does not lead to reliance on rear courtyard parking and a poor relationship between the housing and greenspace.



4.4 BUILT EDGE CODE

This section of the code should be applied to all areas where development meets existing routes at Broad Street Green Road and at the interface with existing development near Heywood Way / Scylla Close and Holloway Road and Maypole Road where it is important to achieve a particular built edge outcome. Active frontage is referred to and is defined as frontage development with main doors and windows overlooking the public realm. All frontage development must be orientated to face the street or public realm. The location plan opposite identifies the location of built edge codes.

AVOID FENCES BACKING ONTO MAIN ROUTES

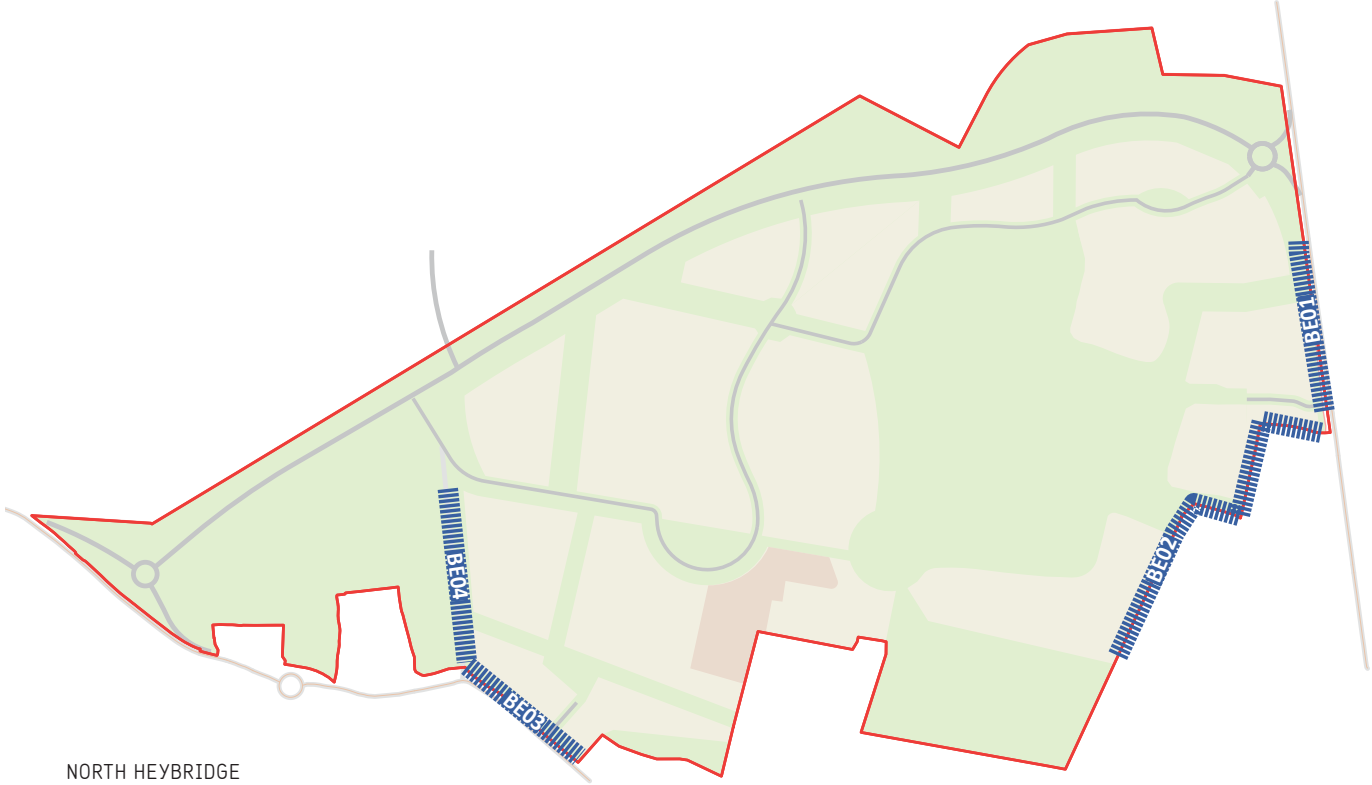
The perceived image of North Heybridge Garden Suburb will be greatly influenced by the appearance of development directly adjacent to Broad Street Green Road and Holloway Road. Buildings should therefore be set back but address these routes and provide a positive frontage.

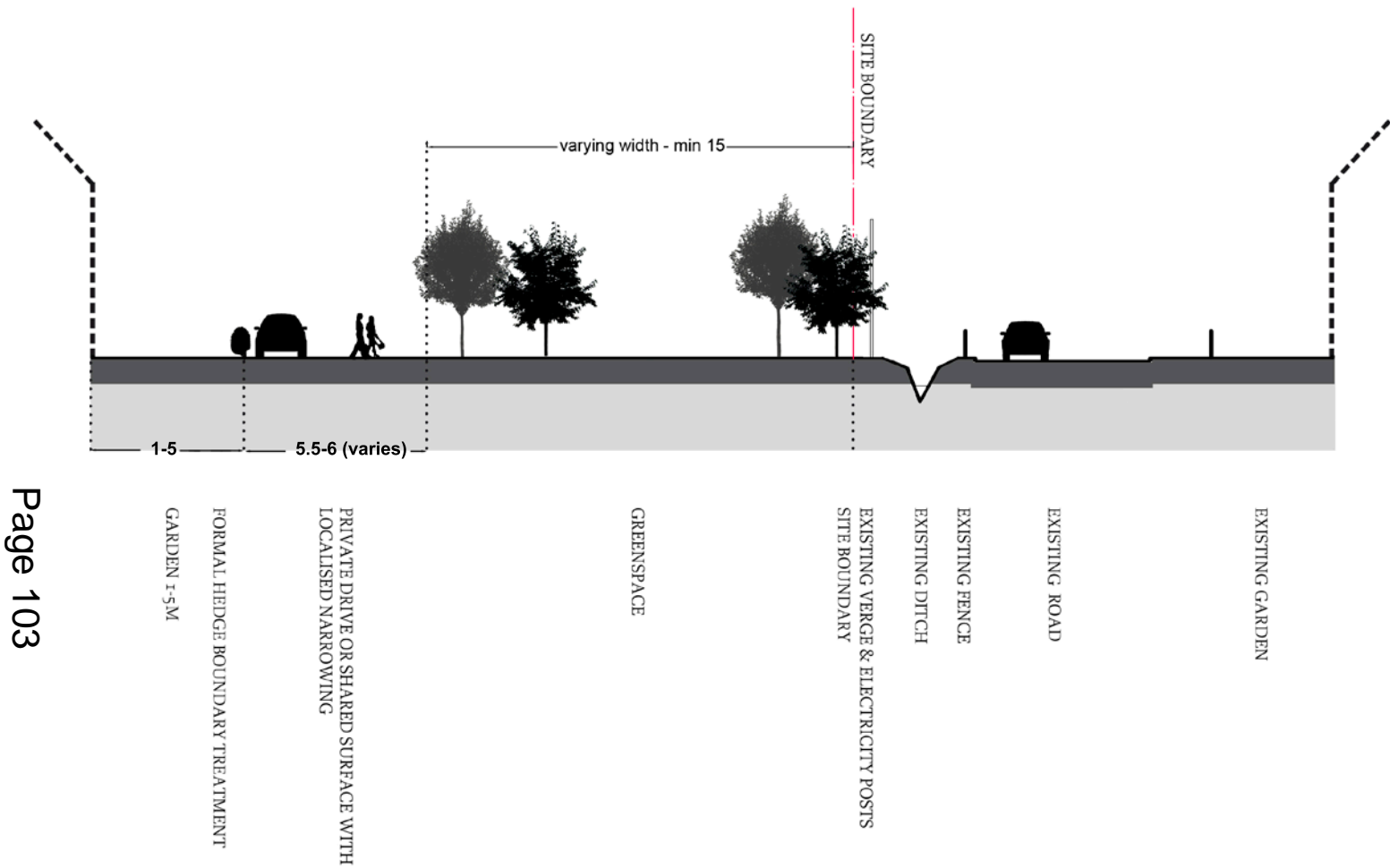
ANIMATE THE EDGES

Ensure buildings along main routes are enlivened by providing active uses and entrances overlooking the routes.

EXISTING REAR GARDENS

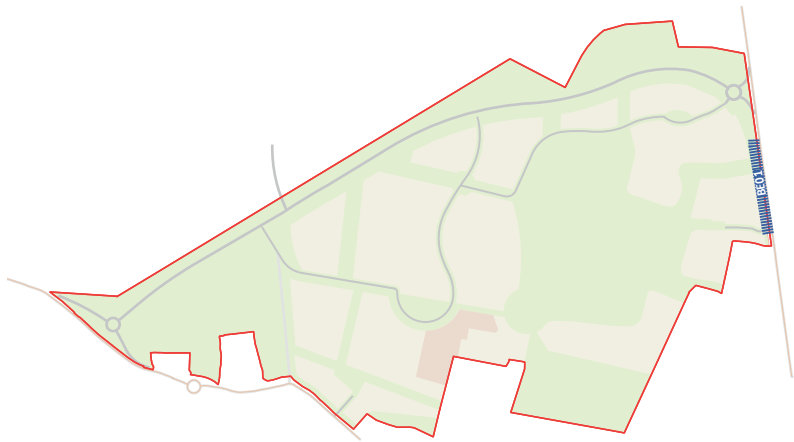
Allowing new development to back onto existing rear gardens is usually the best way to deal with this situation.



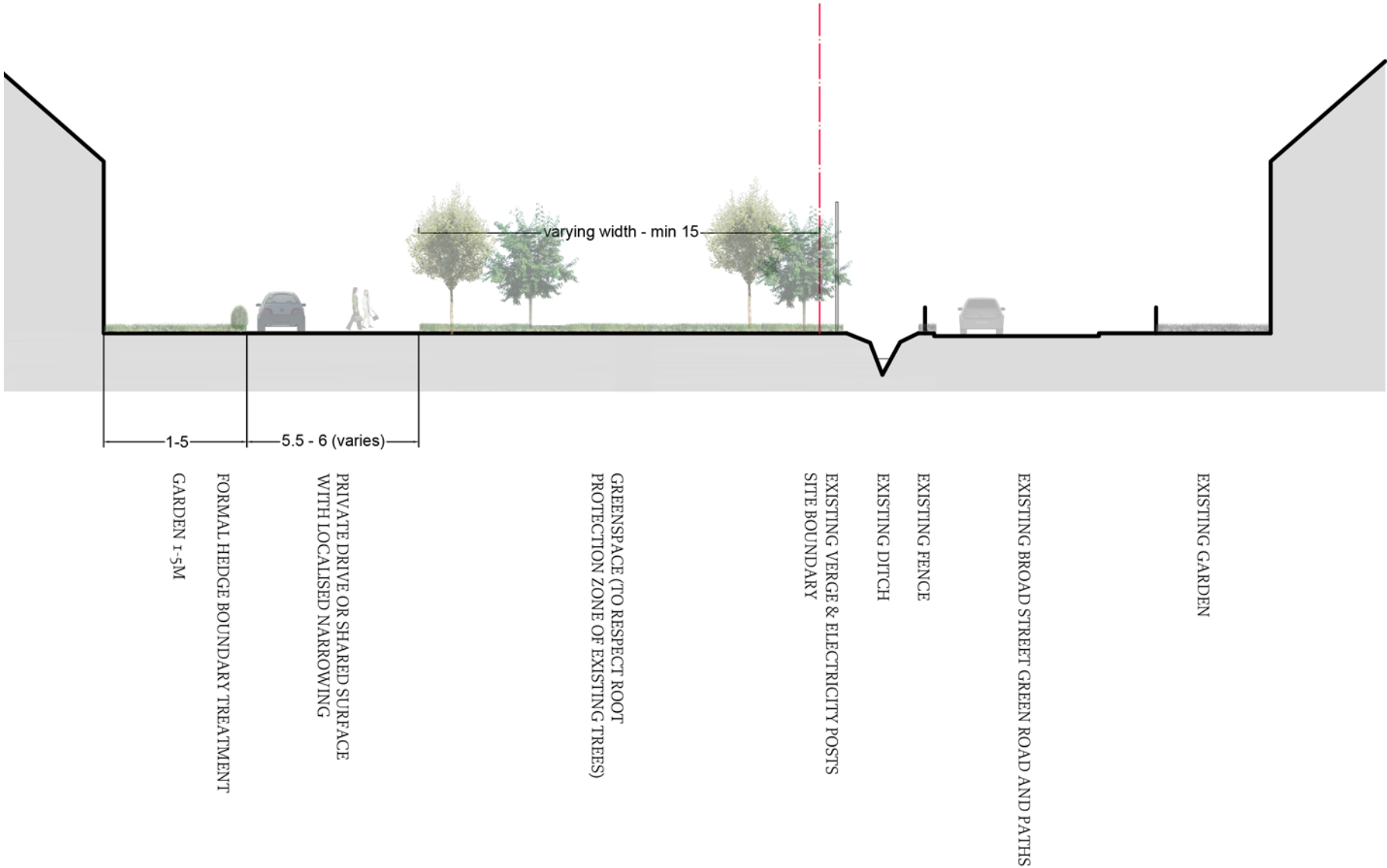


BE PERFORMANCE SPECIFICATION	
Type	<ul style="list-style-type: none">Existing Road / Housing
Core Functions	<ul style="list-style-type: none">Define the importance of frontage to existing routes, public and private space.To provide pedestrian access along the site.To provide perimeter blocks and active development frontage between the development and existing site edges and roads.
Design Objectives	<ul style="list-style-type: none">The main role of the frontage is to define the boundary between public realm and the private plot and to positively enclose the public realm.Frontage can comprise a building or other boundary feature (such as hedge or wall).To enable positive gateways and entrances, as arrival points into the site and into Heybridge.To offer good connections for pedestrians, cyclists and drivers between, through and around the site and its context.Maintain a clearly defined and positive edge to public realm.To ensure continuity of approach to different site ownerships within the Garden Suburb.
DESIGN CRITERIA	
Building height	Building height should create a human scale of development whilst providing appropriate levels of enclosure in keeping with surrounding urban form. The Structuring Plan sets out buildings heights in blocks.
Privacy	Development shall consider the distance between backs of properties and/or location of windows to ensure privacy for residents.
Cycleway	No.
Private Curtilage to buildings	1-5m.
Landscape	Minimum 15m greenspace adjacent.
MATERIALS AND LANDSCAPE	
Materials	As per Design Guidance. Materials in adoptable areas to be agreed with Essex County Council.

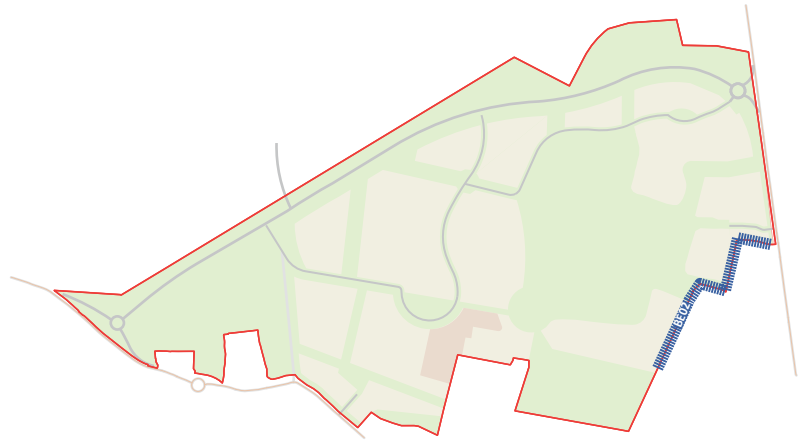
BE01; DEVELOPMENT ADJACENT TO BROAD STREET GREEN ROAD



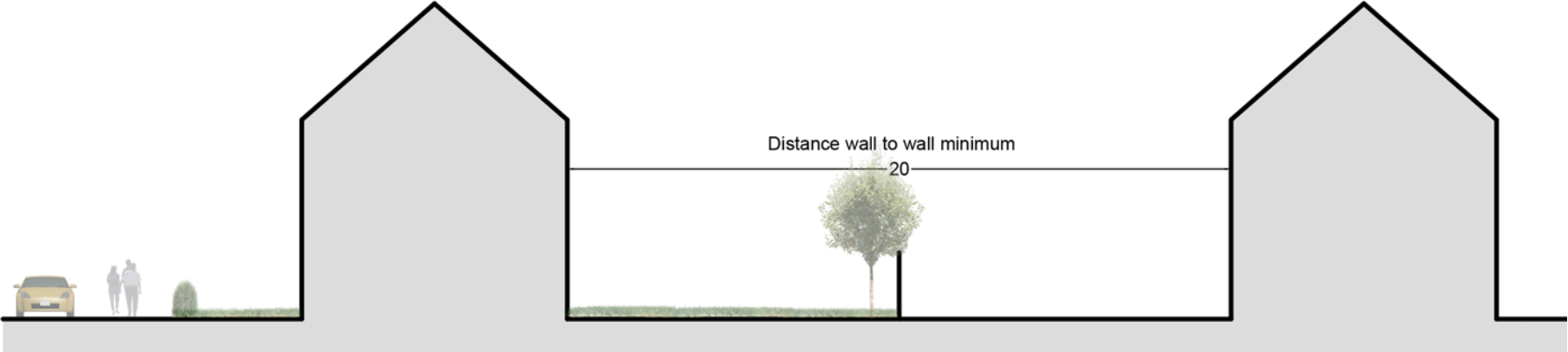
Where development is located along Broad Street Green it is important that the existing ditch, verge, hedge and trees are retained and enhanced along the boundary in order to maintain the character of the existing road. This frontage is important in connecting the suburb with Heybridge and retaining the character of the local road network. A buffer zone should be maintained as a ‘no dig zone’ to protect the roots of existing trees. This provides an opportunity for a greenspace to be provided and housing should front this space to provide overlooking and natural surveillance of this space.



BE02; DEVELOPMENT ADJACENT TO EXISTING HOUSING



This section runs along the east boundary of the site; residential development is located adjacent to existing housing that backs onto the site. The minimum back-to-back distances between existing rear elevations and proposed housing should be a minimum of 20m, with rear gardens located along the boundary to minimise overlooking and loss of privacy for both new and existing properties.



CARLAGEWAY

FORMAL HEDGE BOUNDARY TREATMENT

GARDEN

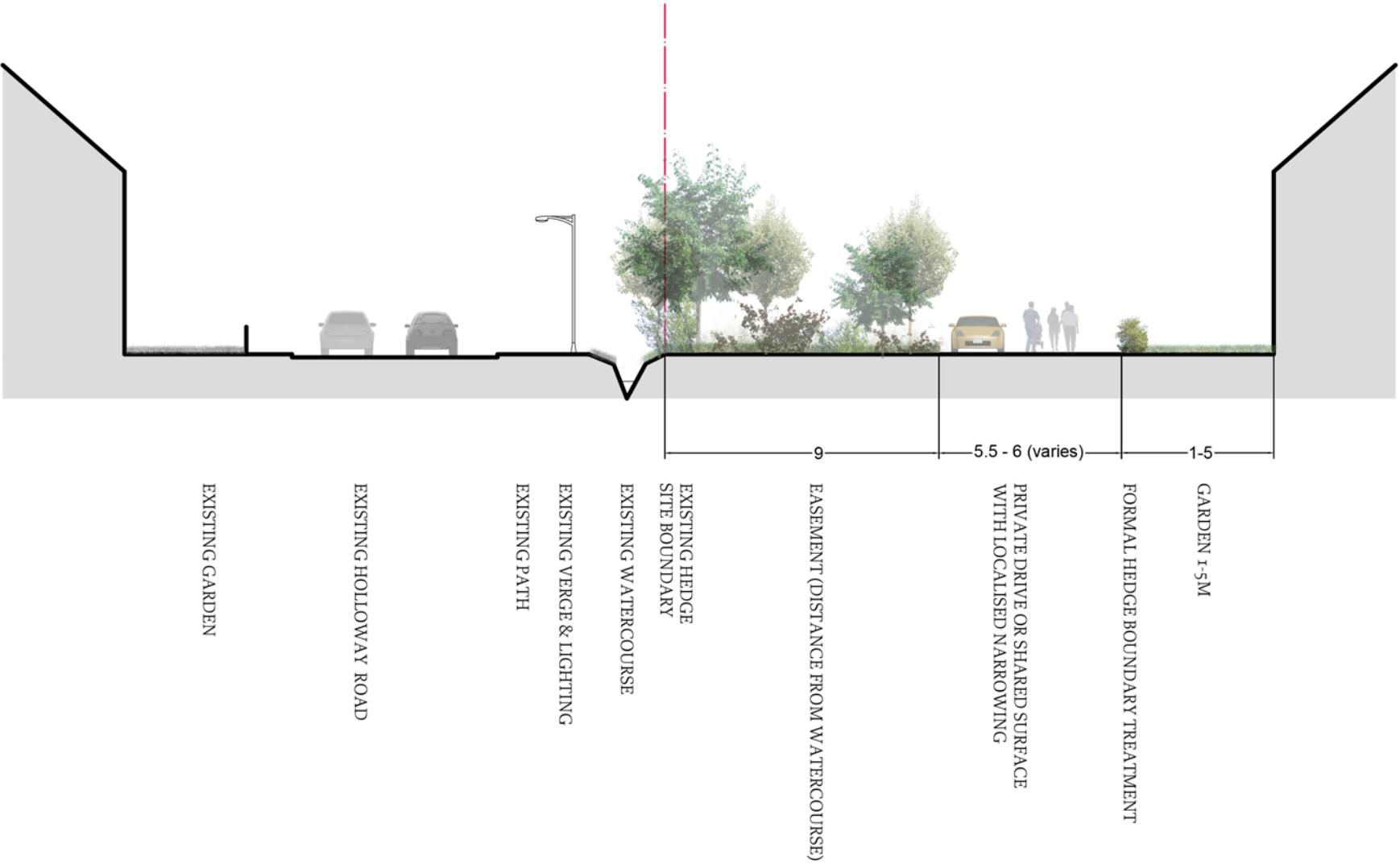
REAR GARDEN

EXISTING GARDEN

BE03; DEVELOPMENT ADJACENT TO HOLLOWAY ROAD



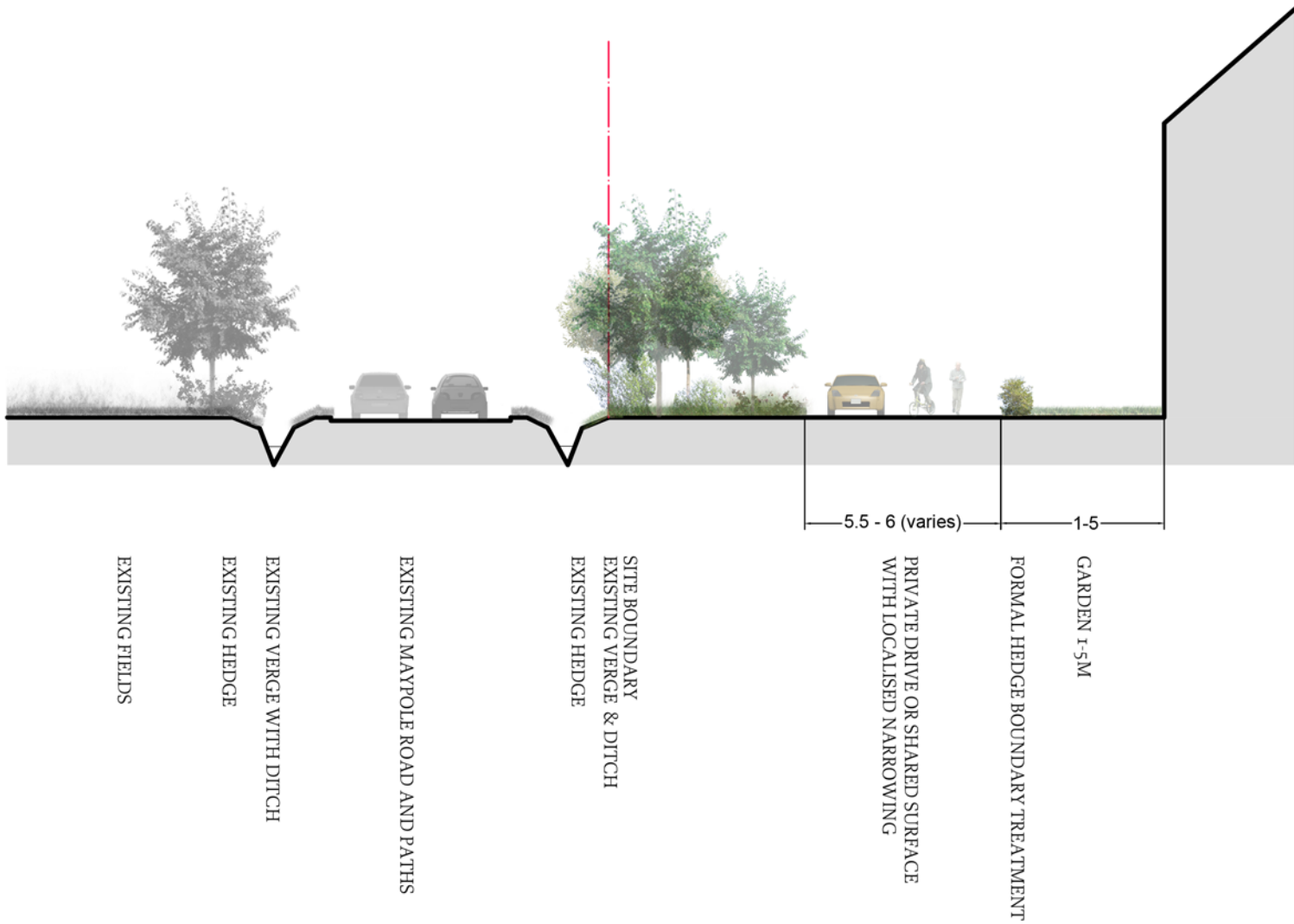
Where development is located along Holloway Road it is important that the existing watercourse, verge, hedge and trees are retained and enhanced along the boundary in order to maintain the character of the existing road. This frontage is important in connecting the suburb with Heybridge and retaining the character of the local road network. A 9m easement from the watercourse is required to be maintained. This provides an opportunity for a greenspace to be provided and housing should front this space to provide overlooking and natural surveillance of this space.



BE04; DEVELOPMENT ADJACENT TO MAYPOLE ROAD



Where development is located along Maypole Road it is important that the existing ditch, verge, hedge and trees are retained and enhanced along the boundary in order to maintain the character of the existing road. This frontage is important in connecting the suburb with Heybridge and retaining the character of the local road network. This provides an opportunity for a greenspace to be provided and housing should front this space to provide overlooking and natural surveillance of this space.





SECTION 5.0
DESIGN GUIDANCE

‘The Design Guidance’ provides qualitative advice on how those elements of the scheme that cannot be fixed in advance of plot development should be designed. The design guidance contained in this section illustrates design intent, design statements submitted alongside detailed proposals must demonstrate how the guidance has been applied. The solutions illustrated in this section are one way of applying the guidance, other solutions may be equally appropriate. Design guidance has been prepared for the following:

- Character context areas
- Car parking
- Bin Storage

GENERAL PRINCIPLES OF GOOD URBAN DESIGN

Whilst codes and guidance set out how developers and designers should address specific parts of the site, there remains an overarching requirement for all parts of any plot to be designed in accordance with the objectives of good design. These objectives are widely accepted and are set out in a variety of guidance documents on good design and summarised below. In addition to compliance with the codes, all development proposals will be judged against these design objectives.

OBJECTIVES OF GOOD DESIGN AT NORTH HEYBRIDGE:

- Natural heritage: Development should be designed to integrate with, protect and enhance the landscape and biodiversity of the site
- Compactness: Development should use land efficiently and provide local densities that offer maximum possible support for public transport and commercial/community services with clear and direct walking routes to these facilities
- Accessibility and ease of movement: Layout should be accessible to all, make links to surrounding areas, create new links and ensure that it is easy to get from A to B within the development
- Legibility: Layouts should be easy to understand and find one’s way around
- Character and context: Development should respond to the character and local distinctiveness of site context
- Continuity and enclosure: Streets and spaces should be overlooked with well-defined street frontage
- Public realm: Public realm should be high quality, place specific, attractive and safe
- Variety and diversity: Development should provide a mix of uses and variety and choice of properties and places
- Adaptability: Buildings and spaces should be designed so that they are flexible and adaptable and can serve a variety of use over time
- Resource efficiency: Buildings and landscape should source materials responsibly

5.1 CHARACTER CONTEXT AREAS

The South Maldon and North Heybridge SMF’s set out the overarching vision for the development of new Garden Suburb communities in Maldon District. The Garden Suburb vision sets out to use the idea as a differentiator to start to build a new image of what living in Maldon and Heybridge might be like. The Garden Suburb vision puts forward the idea that different areas of the site could be designed to have varying character but based on the garden theme; of using the landscape setting to define the spirit, character and form of the new garden suburb communities; and responding to different parts of the sites which have particular qualities.

In order to achieve this, the North Heybridge SMF establishes the principle of defining three character context areas which reflect the qualities of the landscape in which the site sits, namely:

- Garden Suburb
- Village Edge
- Woodland Edge

The Character Context Areas are explained in section 2.3, which emphasises that these are broad brush and differ from the Character Areas identified within the DAS for Heybridge North (page 83). The broad Character Context Areas of the Garden Suburbs are over arching areas which will require a predominant approach, but the design response should be sensitive to local distinctions in specific parts of the sites which have particular qualities. For example Heybridge Wood provides a clear opportunity to reflect woodland character within the design. Another example is the difference in landscape character between the centre and edges of the site where there are other opportunities to define the spirit, character and form of the Garden Suburb with site specific responses to those parts of the site.

As well as responding to the existing character and landscape, there are a number of elements that will be introduced to the Garden Suburb sites that should enhance and reflect the character context areas. In particular a careful consideration of existing watercourses, hedge lines, SUDs and tree planting would be expected to reflect the character context areas and draw attention to the distinctions across the site. For example, the formality or informality of water attenuation features and their treatment within green spaces.

The dominance of street tree planting on Primary Streets has been referred to. Similarly planting beyond these key streets should be considered strategically, considering the effect that is sought in their use across the garden suburb and how it can enhance the garden suburb vision.

GATEWAYS AND ENTRANCES

The entrances into the garden suburb will have design treatments to reflect the Village Edge character location, but they should include subtle variations in response to context. For example it will be appropriate for the entrance from the Relief Road to have a traditional rural feel with post and rail fencing and white five-bar gates either side of the road as commonly seen in surrounding villages.

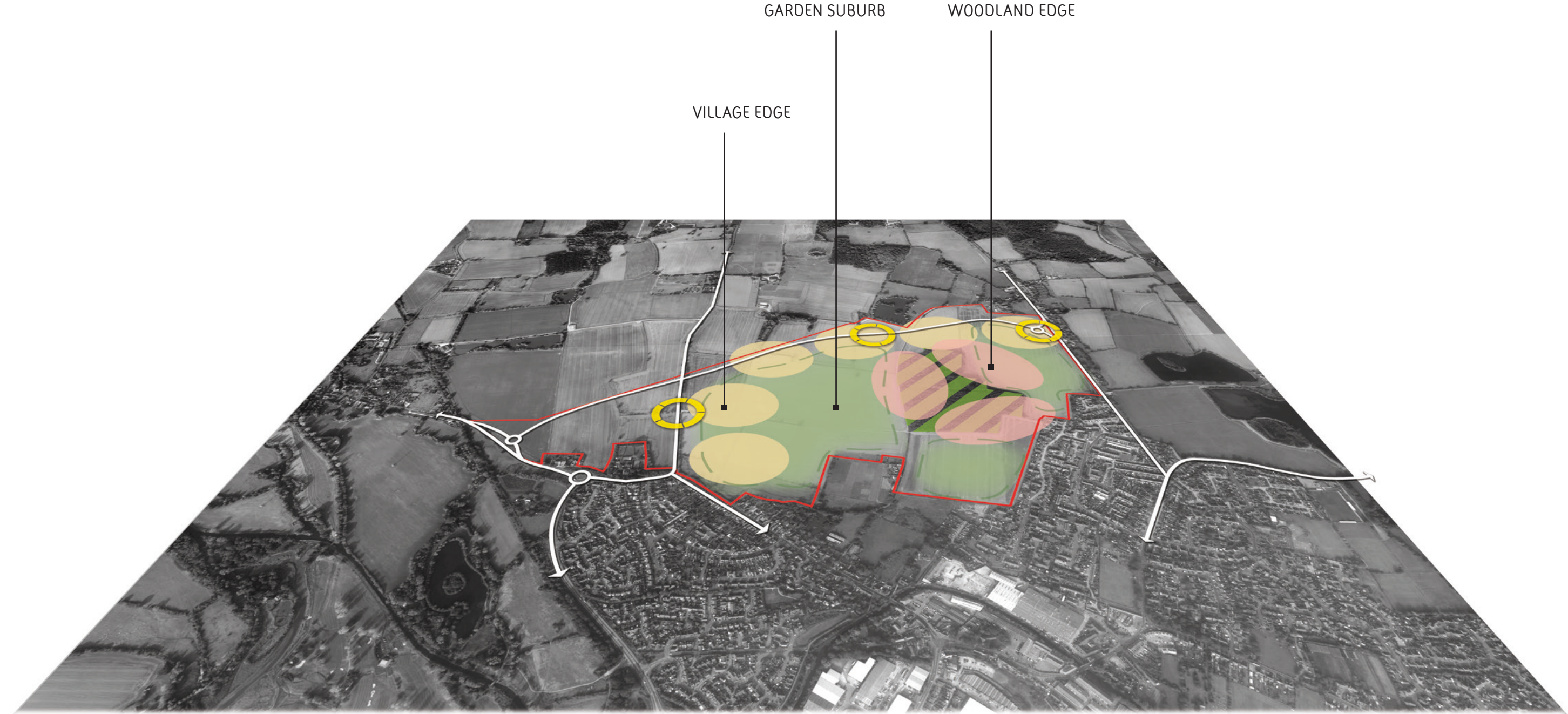
It should be clear from the design and layout of the entrances from Broad Street Green and Holloway Road that they are lesser routes, possibly by tighter urban form, which are not vehicular through routes to the wider Garden Suburb.

CONSISTENCY

There will inevitably be further local variations in character within each of these areas. However, the need to establish an overall consistency and common design language within each of these areas will be fundamental in establishing an image defining and memorable identity for North Heybridge. In order to achieve this it is expected that variations in public realm treatment, building line, height, orientation, boundaries, materials and architectural style should be carefully considered varying some but not all of these elements. The images in each of the following Character Context Area Guidance sections illustrate how a considerable measure of consistency is central to achieving a distinct and recognisable character. One of the consistent features of places which have successfully established a strong identity is a sense of rhythm in the built form where variations in the elements listed above vary on an urban block scale rather than individual plot scale. Design guidance documents that Maldon District Council has endorsed or adopted should be used as a reference and guide to develop appropriate detail design proposals.

The following section will outline the design principles which must be reflected in the design response for each of these character context areas as well as guidance on how these principles could be achieved. Imagery to convey how the potential character of each area might be realised is also included.

NORTH HEYBRIDGE CONCEPTUAL PLAN
CHARACTER CONTEXT AREAS



Aerial Photography - Bing Maps; Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

GARDEN SUBURB

The area closest to the urban edge of Heybridge most closely represents the formal planned layout of a traditional garden suburb. This area will be the core of the new community. It will be centred on the new school and local centre. It will have formal wide tree lined avenues, greenways leading to the most significant destinations and also giving designed views to focal points, landmarks and Heybridge Wood. Housing layouts respond to a formal linear structure with generally straight streets with a tighter urban form to achieve higher densities.

The street structure will be important to establishing the garden suburb character with careful consideration being given to the hierarchy and cross section of streets which are lined by avenues of a single species of tree (Acer Platanoides ‘Emerald Queen’, Tilia ‘Brabant’ or Tilia Cordata).

Although this character context area will have a generally harder and more formal nature the planting of front gardens and requirement to define front gardens with single species formal clipped hedges (Hornbeam or as required by Maldon District Council) will be a unifying element.

GUIDANCE DOWNLOAD
CORE OBJECTIVE
<p>To bring the core physical qualities of garden suburbs into the centre of the site, reflecting its proximity to Heybridge Centre with an urban character.</p> <p>To use the landscape setting to define the spirit, character and form of the place.</p>
URBAN FORM
<p>An urban form structured around a more formal linear structure for housing layouts and a regular street pattern.</p> <p>Generally straight streets, with a tighter urban form to achieve higher densities, well defined streets enclosed by consistent building lines and strong frontage to primary streets.</p> <p>Greenways with vistas to community and civic destinations as well as to designed open spaces.</p>
PLANTING STRATEGY
<p>Formal wide tree lined avenues - single species of trees. (Acer Platanoides ‘Emerald Queen’, Tilia ‘Brabant’ or Tilia Cordata) are some recommendations, with landscape strips to establish the character of this area.</p> <p>Areas of formal ornamental planting to reflect the more urban character of this area. Respects and enhances existing natural features, hedgerows, wildlife areas and water features.</p>
COLOUR AND MATERIALS
<p>Use of varied building materials - influence of local vernacular. Predominantly render, brick and timber.</p>
BOUNDARY AND TREATMENT
<p>Single species clipped hedges defining boundary between street and front garden. (Hornbeam or as required by Maldon District Council)</p> <p>Smaller garden sizes which reinforce a more urban character and enclose streets.</p>



Aerial Photography - Bing Maps: Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGF, swisstopo, and the GIS User Community



WOODLAND EDGE

The garden suburb areas surrounding the edge of Heybridge Wood will draw on the physical characteristics of the garden suburb but with an emphasis on the strong physical presence of the wood and it's protective buffer. It will have a looser, less formal arrangement of streets and will have a layout that works particularly well with development facing towards the woodland and attractive intervening treed spaces that provide a sense of the woodland permeating the layouts. It will combine with the best and most successful principles of Garden Suburbs to create a successful synergy between living and woodland.

The layout of buildings and landscape create a more informal character with areas for natural planting and green space. The street structure remains vital with tree lined streets and universal presence of clipped hedges defining the boundary between street and front garden. The layout is more organic and lower density than other parts of the development and includes areas of incidental green space.

The woodland will be surrounded by a protective green buffer forming part of an extensive network of green infrastructure with amenity space along cycling routes overlooked by surrounding development.

GUIDANCE DOWNLOAD
CORE OBJECTIVE
To bring the core physical qualities of garden suburbs into the Woodland Edge, creating a successful synergy between town and open countryside.
URBAN FORM
A natural and permeable arrangement of less formal streets with a rural influence and informal character. The layout will provide either front or side elevations facing the edge of the wood to create overlooking and to provide views to the woodland, soft edge and gradual transition to the wood itself.
PLANTING STRATEGY
Natural planting and green space to reflect woodland. Tree lined streets - single species of trees. (Acer Platanoides ‘Emerald Queen’, Tilia ‘Brabant’ or Tilia Cordata are some recommendations).
COLOUR AND MATERIALS
Varied natural material palette - influence of local vernacular. Neutral to warm coloured and natural materials including stone, metal, slate, brick and timber can be combined to ensure building complement the surrounding areas of countryside. Open space and public realm within this area should build on the woodland character incorporating informal footpaths and timber furniture.
BOUNDARY AND TREATMENT
Single species clipped hedges defining boundary between street and front garden. (Hornbeam or as required by Maldon District Council)



Aerial Photography - Bing Maps: Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGF, swisstopo, and the GIS User Community



VILLAGE EDGE

The village edge references its location adjacent to open countryside where it is appropriate to make a gentle transition from town to country. It will be formed from the development areas lying around the northern and western rural edge of the site and close to the Relief Road corridor.

The principle is to create a looser-grained rural edge with development arranged around informal and irregular shaped green spaces, as can be found at villages such as Little and Great Totham and punctuated by a designed landscape providing green infrastructure and wildlife corridors.

GUIDANCE DOWNLOAD
CORE OBJECTIVE
To bring the core physical qualities of garden suburbs into the Village Edge areas, creating a softer edge to the development through a sequence of open spaces, planting and the retention of existing landscape elements.
URBAN FORM
A softer edge around the northern and western rural edge of the site and close to the green corridor formed by the Relief Road. Built form should be planned to overlook open space positioned in peripheral areas of the site to strengthen the visual relationship between the housing and open space. Street layout should have a less consistent building line than other areas.
PLANTING STRATEGY
Additional planting to enhance existing planting, with new tree and shrub planting contribute to green space, particularly along pedestrian routes and within parks. Street and open spaces to be characterised by informal and naturalistic design and planting, with visible linear SUDs features and wetland planting.
COLOUR AND MATERIALS
Natural material palette - influence of local rural vernacular. Neutral to warm coloured and natural materials including stone, metal, slate, brick and timber can be combined to ensure building complement the surrounding areas of countryside. Open space and public realm within this area should build on the informal character incorporating gravel footpaths and timber furniture.
BOUNDARY AND TREATMENT
Predominant use of hedge boundaries, with some post and rail fencing and timber gates, to reinforce the rural character common to surrounding villages of the area (Goldhanger and Purleigh) to define the edge of relatively large front gardens.



Aerial Photography - Bing Maps: Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community



5.2 PARKING GUIDANCE

PRINCIPLES

The accommodation of parking into the scheme is one of the most difficult design problems of the development. People have a tendency to park wherever they can, as close to their houses as possible, regardless of the design intent of the scheme. If it is not flexible and naturally policed it will fail. In the past there has been too much reliance on parking courts as a mechanism to push cars off the street to achieve uncluttered frontages. This over reliance on parking courts has led to security issues, but has also created urban patterns which are expensive to implement and maintain with large areas of hard surfacing. Parking court solutions are often highly inefficient and unless they are well designed residents often chose not to use them.



The Maldon District Garden Suburb schemes should adopt a more flexible and traditional approach to parking and allow for on street and on plot parking as well as a small amount of court-based parking. Key principles to underpin any parking solution are:

- Provide sufficient parking in accordance with Maldon District Vehicle Parking Standard (or sucesor document) and Essex Design Guide Road Type Table (refer to Appendix A).
- Rely on natural policing: If a parking solution relies on a large amount of management and enforcement it is probably wrong. Solutions implemented in the past which force people not to park in the street to encourage the use of parking courts can prove particularly difficult to manage. Any parking solution should be capable of being naturally policed. Generally this will be achieved through the provision of adequate spaces in convenient and safe locations whilst ensuring that parking is difficult or unachievable in all other locations. Traditionally on-street parking has achieved this effectively by providing a set number of spaces but leaving little scope for abuse.
- Ensure natural surveillance: Residents like good surveillance of their vehicles. Ideally this will be by themselves so the provision of car spaces within sight of a dwelling is always preferred. However residents are also happy to park a vehicle where it is has good surveillance from properties other than their own. All parking areas should be overlooked or otherwise secure. Small dedicated access-controlled parking courts can be more effective than large public parking courts which are difficult to naturally survey.
- Allow flexibility: Parking solutions need to be flexible rather than prescriptive. Whilst dedicated on plot parking works well, other forms of dedicated parking is less effective. Parking off-plot should therefore be flexible and provided in the form of on-street parking and parking areas with good surveillance with an overall quantum of spaces adequate to meet demand. Parking in other areas should be made difficult or impossible through the design of streets and spaces.
- Parking should be achieved predominantly with on-street and on-plot parking with courtyard parking used occasionally and in certain situations including where there are flats.. Where courtyard parking is used it should generally be at the front of dwellings or at the end/side of short terraces. Side parking courts should be kept small and size and generally be a maximum of 6-8 spaces and should always be visible from the street and well overlooked by fronts of surrounding properties.
- Allocated parking is not acceptable in adoptable areas.



ON STREET PARALLEL PARKING

On-street parking is encouraged on all streets, parallel parking is the most common way of achieving this goal. These spaces can be set out to relate to specific houses as allocated spaces or for general visitor parking or for additional spaces in tandem with on-plot parking which is described further under Combination parking. Parallel on-street parking can be adjacent to footways or separated from footways by verges, they can also be used in shared surface streets to create homezone arrangements. On street parking should be provided in clearly marked bays and 'designed in' to the streetscene.

ON STREET PERPENDICULAR PARKING

An alternative arrangement for on-street parking is perpendicular or 90 degree parking which can achieve all the same advantages as parallel parking. Perpendicular parking can be used to vary the townscape by creating wider streets or varying the width of streets to become more like spaces than streets, often utilising trees within the parking areas to add to the townscape and make use of the space/street width created. They can also be used to alter the nature of streets. Perpendicular parking can effect the way the street is used with slower speeds, breaks in traffic etc. as cars access spaces.

Perpendicular parking can be used to increase numbers along streets and is often a useful way of achieving visitor parking at the end of streets or in small front parking courts. Care should be taken to ensure a minimum of 6m space beyond the parking spaces for manoeuvring and access.

As shown in the above photo landscaping or other interventions work well to break up perpendicular parking bays, to reduce their dominance on the street. A maximum of six spaces should be provided in any group before being broken up by a landscape bay or street tree planting.

ON STREET FLEXIBLE PARKING

Combinations of these types of parking along the same street or from street to street can be used to vary the character throughout the development. This approach to on-street parking is achieved by designing street cross sections that can utilise both parallel and perpendicular parking. This can be used to accommodate different housing typologies along a street and to improve and vary townscape by altering the width of streets public realm. This approach can be used to increase numbers of parking and identify allocated and non-allocated parking.



ON PLOT PARKING

On-plot parking can be achieved in many ways within the private curtilage but to achieve good and varying townscape we suggest three key approaches, on-plot to the front, on-plot to the side, and integral garaging.

On-plot to the side is to be utilised for detached, semi-detached and end of terraces. This will be accommodated through sufficient width of plot to allow parking at the side of the dwelling, potentially allowing for two spaces one in front of the other or even garage parking.

On-plot to the front of the dwelling is often utilised for terrace housing allowing for off-street parking. Where this arrangement is utilised it is essential that alongside the parking space, space is always retained for planting/garden, pedestrian access to dwelling, and cycle and refuse storage if the strategy for these requires it.

Integral garaging is often utilised for 'mews' housing or narrow urban streets. Care should be taken to ensure that the street elevations are not dominated by garage fronts, so integral garaging should either be alternated with other arrangements or used with wider dwelling frontages that incorporate entrances and openings onto habitable rooms to ensure active frontages.

COMBINATION ON STREET & ON PLOT PARKING

As already suggested, the use of on-street and on-plot parking is preferred and certain streets will benefit from the repetition of a certain parking type, however to achieve a truly varied and interesting townscape particularly on tertiary streets and shared surface streets it is important to allow for a combination of these parking arrangements. This combination can also help to accommodate a range of house typologies. Street sections can provide for on-street parking opposite on-plot parking as well as allowing for alternating arrangements along the length of a street.

COURTYARD PARKING

As discussed under 'principles' the guidance suggests that parking should be achieved predominantly with on-street and on-plot parking with courtyard parking used occasionally and in certain situations. Where courtyard parking is used it is generally at the front of dwellings or at the end/side of short terraces. There are certain circumstances where rear parking can be used successfully, such as on the frontage to open spaces where it is preferable to avoid parking on the green infrastructure edge and gateway or other particularly prominent locations. Where courtyard parking occurs it must be dealt with as a mews with at least two properties overlooking.

In this case a well-designed rear court could be used which should have good pedestrian access through the terrace to the front with surveillance and security considered. A maximum group of 10 spaces per courtyard (preferably 6-8 spaces), broken up with planting to reduce the impact that large numbers of cars can have in dominating the development. Proposals for courtyard parking and the number of spaces to be provided will be assessed taking into account site layout, amenity and landscaping proposals.

Front courtyard parking can be used to improve townscape and to increase parking numbers. This can be achieved through widening streets at key locations or by altering the street alignment. These spaces often provide the opportunity to combine parking and tree planting.



5.3 MATERIALS & PUBLIC REALM DESIGN PRINCIPLES

PRINCIPLES

The materials utilised both on buildings and within the public realm design will be important in helping to define the quality and character of the streets within the scheme. It is recognised that the project will be built over a number of years and it is likely that there will be changes in the availability of materials and furniture over the period. For this reason the guidance sets out a performance specification for key elements of the buildings and public realm across the development. The final specification and details for the external works finish in different development areas will be a matter for the developer and the local authority to resolve together. Considerations will include effects on commercial viability in responding to the key objectives of the Strategic Masterplan Framework. Regular discussion and agreement between Local Authorities and developers will clarify where the scope for innovation is at any point in development of the overall scheme.

The materials shown are typical examples only and are not proposed as a specification palette to be rolled out across the whole site. These materials are only indicative of the quality thresholds for specification of external works items and building elements in any part of the development. The Developer is responsible for the design development of a coordinated works design package that is appropriate to achieving the place making objectives and principles for character of the development plot they are delivering.

The illustrated layouts for the respective character context areas demonstrate a simple efficiency to support the delivery of the overall objectives of the Structuring Plan. It is possible to reinforce this approach through to detailed design of the public realm with a focus on the coordinated design and execution of a consistent quality of workmanship from a refined pallet of appropriate materials that are fit for purpose and reflect the hierarchy and added value the public space offers. For example; bitumen macadam on the carriageway of Primary streets and consistent surface dressing in shared surface areas tied together with a consistent kerb unit material throughout. Opportunities for adding trees, appropriate permeable surfaces, such as stabilised gravel, grass and/or paving blocks as part of the shared surface street would reinforce the legibility of the place. A simple contemporary design for street furniture and lighting would fit in with a street scene characterised by buildings that are equally refined and of their time.

With regard to public realm key principles to be taken into account include:

- Unify the scheme: the choice of dominant public realm colours, materials and finishes should help to visually link and unify all parts of the development and help it read as a cohesive whole. In practice this means ensuring that a small number of dominant elements of primary streets are consistently treated across each phase, these key elements are therefore specified in the primary street (PS) strategic design codes. They include:
 - Carriageway
 - Footway
 - Create Places: The location and positioning of public realm elements can help to define places and points of interest within the scheme.

- Robustness and durability: Ensure materials specified are robust, durable and will stand the test of time. This generally means the use of good quality natural materials and other materials that have a proven performance in an urban environment. However allow for the use of innovative local produced, sustainable or aesthetic materials supported by technical data where this will enhance the overall quality or sustainability of the scheme.
- Ease of maintenance: specify standards of materials and workmanship that allow easy maintenance and replacement. Avoid complicated and fussy detailing except where part of a feature with a specific design intent.
- Carbon footprint: consider the environmental implications of materials. Specify materials in accordance with the BRE Green Guide to minimise carbon footprint.

Public realm treatments within development blocks and along tertiary streets can help to support the character of each area. The use and arrangement of planting will have a particularly strong impact and guidance is given in Section 5.1 Character Context Areas and Performance Specification tables in this document. The consistency in the design of the space between the building elevation and the street, the design of street itself and the implementation of a consistent and coordinated design of the threshold level of buildings with the back of the pavement will all affect the perceived quality of the scheme.

Access and servicing arrangements, such as bin storage and the location of services and utilities infrastructure should all be coordinated as part of an integrated design of the streets and spaces. A consistent soft boundary treatment to properties, together with mature street trees of a uniform habit and species (Acer Platanoides 'Emerald Queen', Tilia 'Brabant' or Tilia Cordata) in the public space is an opportunity to soften the environment and reflect the formal arrangement of the buildings that characterises this area. The finer level of planting such as the treatment of soft verges, existing watercourses, swales and other landscape elements that extend throughout the area are also opportunities for a creative and distinctive approach that strengthens the identity of the character context area. For example, hedgerows adjoining the countryside and to the linear park can strengthen this identity with selection of appropriate native species.

Overall public realm treatments will serve to unify public areas rather than differentiate them and the main character variations will come from variations in density, formality of layout, garden and boundary treatments, the impact of use on character, scale of buildings and use of material in relation to building form and grouping.

Guidance is given below on acceptable materials for key public realm elements likely to feature within the scheme.

FOOTPATHS AND CYCLE PATHS

- All shared footpath surfaces should be designed to be fit for purpose according to the access requirements and traffic overrun anticipated
- Products using recycled material and local and/ or UK provenance are preferred
- Medium specification surface finish: Resin-bonded surface dressing
- Lower specification surface finish in ‘rural’ areas (for example through greenways, parks, green fingers) : Self-binding aggregate or other porous surface dressing
- Lower specification surface finish in urban areas: Blacktop

Typical examples of materials suitable for use in footpaths and cycle ways:

- | | |
|---|-------------------------|
| 1 | Resin bonded aggregate. |
| 2 | Self-binding aggregate. |
| 3 | Black top. |



CAR PARKING SURFACE TREATMENT

- Finishes in car parking areas shall be suitable for use in a permeable paving system to improve the SUDs design
- All paving systems will need to be able to tolerate forecasted trafficking, attain infiltration targets and are to be detailed to manufacturer’s requirements
- Where possible, products using recycled material of local and/or UK provenance should be specified
- Where possible, recycled material should be used through the full depth of construction
- A subtle method of marking parking bays is preferred to line marking and should be considered as part of the overall car park finish design

Typical examples of car park finishes:

- | | |
|---|---|
| 1 | Block pavers. |
| 2 | Stabilised gravel and turf systems |
| 3 | Metal studs, contrasting colour or tone of material used to demarcate parking |



CARRIAGEWAYS

- It is recommended that all primary street carriageways are to be finished in black top macadam or a clear bound natural aggregate.
- Carriageways in tertiary streets and shared spaces should be designed to respond to the relevant character context area description and achieve the design requirements set out by the relevant street code. Shared spaces and dedicated parking bays should also be finished in materials that support the character objectives of the area in which they are situated. Opportunities to use porous pavement design solutions should be considered wherever possible, particularly in tertiary streets and spaces.
- Carriageway design must meet relevant Highways Authority standards with materials in adoptable areas to be agreed with Essex County Council.
- Raised tabletop crossings and any other shared surfaces should be designed to complement the finishes in other parts of the scheme

Typical examples of materials suitable for use in carriageways:

- 1 Block pavers
- 2 Blacktop
- 3 Reconstituted stone kerb
- 4 Clear bound natural aggregate

- Note that resin bonded material and permeable paving is not acceptable within adopted areas.

KERB STONES AND EDGING

- A uniform kerb type should be used throughout the scheme on primary streets. To ensure this the selected kerb material or product should be available in a range of dimensions so it can be laid in a number of scenarios i.e. with upstand, flush etc.
- Kerbs on shared and tertiary streets should be laid flush/ minimal upstand where drainage requirements permit
- On tertiary streets and shared space kerbs constructed from recycled and/ or reconstituted stone should be used
- The use of 'integrated' or 'drainage' type kerbs can be considered but the material, design and finish should match other kerbs used on site and be coordinated with the other surface finishes used
- Additional specialist kerbs may be required for bus stops and controlled road crossings

Typical examples of kerbs:

- 1 Reconstructed stone kerb
- 2 Conservation kerbs
- 3 Granite kerb
- 4 Other natural stone materials



BIN STORAGE

Reference should be made to Maldon District Council’s current Recycling, Rubbish and Waste arrangements to understand the provision of containers for household waste and recycling. From early Summer 2016 residents within the District were provided with a black wheeled bin for refuse, a caddy for food waste, a pink recycling sack and a blue box for glass. Additionally consideration should be given to accommodating a green wheeled bin for garden waste.

Allocated space should be provided within the boundary of each housing plot for the storage of the necessary number of bins in a manner which does not detract from the street scene. The design must demonstrate that the containers can be stored and collected.

The following principles will be used to determine whether or not the proposed storage solution meets these requirements:

- Convenient: storage must be positioned in the most convenient location within the plot otherwise it will not be used.
- Visibility: The storage facility should be capable of minimising the visual intrusion of bins on the street scene when not out for collection, this will include ensuring sufficient height to screen bins, maximising the number of sides of the bin which are screened and minimising the number and frequency of visible bins from the street.
- Integrated: form and materials should be considered so that there is continuity with either the front elevation of the buildings or the boundary treatment or both and that the bin store itself does not end up dominating the street scene
- Consistency: A consistent approach should be taken to minimise the visual interference of the facilities in the street scene

The following examples illustrate a number of arrangements which could, if well designed, achieve these requirements of the Council guidance and the principles outlined above:

- Stored to the side or the rear of the property, suitable for detached, semi-detached and end terraced where this can be provided conveniently enough that bins do not end up stored in front gardens.
- Integrated into the front boundary treatment of the property, suitable where a hedge or a wall can be extended to provide a structure which is capable of limiting the visibility of bins. This could be either a bespoke bin store well integrated into the boundary or use of the boundary itself to provide the screen. This would require a boundary height of 1.2m so consideration should be given to visibility adjacent to the highway. It may be possible to raise the boundary for a small section of the boundary to provide the screen.
- Integrated in to the plot boundary between properties in similar arrangement described above
- Integrated into front elevation of the building, potentially combined with a porch area or integral garage, this approach must not result in a facility which dominates the front elevation of the building
- Placed in integral garages; where provided and of sufficient size.

Consideration should be given to combining bin storage with cycle storage and potentially on plot parking arrangements to ensure all these functions are catered for, their effects on the street scene are considered and do not result in a proliferation of structures over time.





APPENDIX A

ESSEX DESIGN GUIDE

ROAD TYPE TABLE

Due to be adopted April 2017

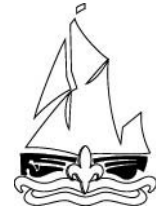
The draft standards are being applied by Maldon DC and they will be kept under review.

Reference to the road types highlighted in the Essex Design Guide Road Type Table are made in the Performance Specification Tables in this Strategic Design Code. ECC Highways standards may be subject to review and application of the Design Codes will need to take into account any future modifications. Early consultation with ECC is recommended to confirm standards applicable at the time of Reserved Matters applications.

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Road Type	Road Description	Guide to No Dwellings	Min Carriageway Width, Cycleway/Footway Requirements	Design Speed	Visibility Splays	Max Gradient	Min Centre Line Radius	Kerb Radii	Comments
A	Local Distributor	n/a	7.3m & 1 x 2m footway, 1 x 3.5m cycleway/footway. Buses to use full laybys. Pedestrian and cycle crossings to be provided on identified desire lines.		Junction and forward visibility splays will comply with current policy standards; please refer to DMRB or Manual for Street	5%	DMRB	10m for non-industrial, 15m for industrial	<ul style="list-style-type: none">Multi-purpose through route, required. Classification of County route required.Minimum 3m vergesClearway.Frontage access prohibitedA straight section of carriageway will be provided from the entrance junction for 30 metres.Lighting will be provided in accordance with ECC Operational plan
B	Link Road	n/a	6.75m & 1 x 2m footway, 1 x 3.5m cycleway/footway. Buses to use half laybys or on carriageway. Pedestrian and cycle crossings to be provided on identified desire lines.	50kph 30mph	Junction and forward visibility splays will comply with current policy standards; please refer to DMRB or Manual for Street	6%	44m	10m	<ul style="list-style-type: none">The road forms more than 1 access to the development. Links neighbourhoods and also serves non-residential or industrial uses.Public transport route.Minimum 3m verges required.No parking unless off carriageway provision is made.No frontage access within 15m from junctionsEgress in forward gear only within 15 - 30m from a junctionA straight section of carriageway will be provided from the entrance junction for 22 metres.Lighting will be provided in accordance with ECC Operational plan
C	Feeder Road	n/a	6m or 6.75m if a current bus route now or one is expected in the future. 2 x 2m footway	30kph 20mph	Junction and forward visibility splays will comply with current policy standards; please refer to DMRB or Manual for Street	8% 6% on a bus route	20m	6m 10m on a bus route	<ul style="list-style-type: none">May serve residential and non-residential uses.A 50kph (30 mph) speed limit may be considered on a public transport route where it is not possible to provide appropriate traffic calming for a 30kph (20mph) speed limit.No parking unless where off carriageway provision is made.No frontage access within 15m from junctionsEgress in forward gear only within 15 - 30m from a junctionA straight section of carriageway will be provided from the entrance junction for 22 metres.Lighting will be provided in accordance with ECC Operational plan
D	Access Road	400 units on a loop, or 200 for a cul de sac	5.5m with 2 x 2m footways or 1 x 2m footway if fewer than 25 dwellings are served.	30kph 20mph	Junction and forward visibility splays will comply with current policy standards; please refer to DMRB or Manual for Street	8%	13.6m	6m	<ul style="list-style-type: none">Provide direct access to dwellingsA section of carriageway will be provided from the entrance junction for 15 metres.Lighting will be provided in accordance with ECC Operational plan
E	Minor Access	100 units on a loop, or 50 for a cul de sac	Combined pedestrian/vehicular surface 6m. Maximum length 125m for a cul-de-sac, or 250m for a through-route. Localised narrowing where appropriate.	30kph 20mph	Junction and forward visibility splays will comply with current policy standards; please refer to DMRB or Manual for Street	8%	13.6m		<ul style="list-style-type: none">Tabled entrance approach and priority for pedestrians and cyclists across junctionsJunctions a straight section of carriageway will be provided from the entrance junction for 15 metres.Street lighting not required
F	Mews Court	20 units on a cul-de-sac	Combined pedestrian/vehicular surface 6m. Maximum length 50m (cul de sac). Localised narrowing where appropriate.	30kph 20mph	Junction and forward visibility splays will comply with current policy standards; please refer to DMRB or Manual for Street	8%	13.6m		<ul style="list-style-type: none">Special junction detail.Priority for pedestrians and cyclists across junctions.A constricted entrance enclosed by buildings or walls to a minimum height of 1.8m for the first 8m back from the footway of the major road (except for the 1.5x1.5m pedestrian visibility splays).No doors, gates or other entrances may open on to the mews within this first 8m.No projections over the net adoptable area of the mews court.No windows or doors should open outwards over the highway.A straight section of carriageway will be provided from the entrance junction for 12 metres.Street lighting not required
G	Shared Private Drive	5 units	5.5m for first 6m tapering down to a lesser width. Desirable max length 18m, longer requires a turning head of size 5 and passing bays.		Junction and forward visibility splays will comply with current policy standards; please refer to Manual for Street	8%			<ul style="list-style-type: none">Where private drive joins a 30kph (20mph) network, width may be reduced.At junctions a straight section of carriageway will be provided from the entrance junction for 6 metres.Street lighting not required.
H	Mixed Use Street	n/a	6.75m carriageway comprising of two 3m running lanes with generally a 0.75m central over run-able strip & 2 x 2m footways. Bus route.	30kph 20mph	Junction and forward visibility splays will comply with current policy standards; please refer to DMRB or Manual for Street	5%	20m	10m on a bus route otherwise 6m	<ul style="list-style-type: none">Serves residential and non-residential uses; for example High Streets.On-street parking in bays.Street trees required. Details in The Urban Place Supplement.Over run-able strip should be wider where appropriate e.g. at crossing points.A straight section of carriageway will be provided from the entrance junction for 22 metres.Lighting will be provided in accordance with ECC Operational plan

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REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

DRAFT MALDON DISTRICT DESIGN GUIDE AND PROPOSED PUBLIC CONSULTATION

1. PURPOSE OF THE REPORT

- 1.1 To seek the Committee's approval for the Draft Maldon District Design Guide (**APPENDIX A**) to go out to public consultation.

2. RECOMMENDATION

That the Draft Maldon District Design Guide is approved for public consultation.

3. SUMMARY OF KEY ISSUES

- 3.1 Policy D1 Design Quality and Built Environment of the submitted Local Development Plan 2014 – 2029 (LDP) sets out ten criterion that all development must have regard to reflective of the unique character and distinguishing features of the District. Policy D1 LDP states that: *In addition, all developments must demonstrate that they are in general conformity with the design principles set out in the emerging 'Maldon District Design Guide SPD' and any other relevant local development documents, neighbourhood plans or Village Design Statements (VDSs) adopted or endorsed by the Council.*
- 3.2 The Committee approved the Project Brief (**APPENDIX B**) to appoint consultants to prepare the draft Maldon District Design Guide on 14 January 2016. That Project Brief took full account of the objectives set out in LDP Policy D1 and evidence base documents to ensure that all development is of a high quality design wherever it is located to enhance environmental quality whether natural built or historic; is inclusive to ensure that all development can be used by all people and to deliver green infrastructure benefits; to reduce social exclusion and seek to improve people's access to housing, employment and services and facilities and to connects people and places by sustainable modes of transport particularly through the provision of safe and attractive pedestrian and cycle routes. BDP were appointed as our consultants in April 2016.
- 3.3 The Draft Maldon District Design Guide (MDDG) incorporates supporting evidence base documents focussed on Policy D1 requirements, for example, the Maldon District Characterisation Assessment and Maldon District Landscape Character Assessment and references existing Neighbourhood Plans, Village Design Statements

and Conservation Area Reviews and Appraisals to inform the design process from a planning and visual context analysis. It is intended that future plans, studies and statements will be referenced and linked to in the MDDG as and when prepared and endorsed. It is intended that the MDDG will be a digitally interactive document whereby documents are accessed via direct web links.

- 3.4 An officer working group made up of cross-cutting service areas has ensured a coordinated and collaborative approach to produce the draft MDDG. A special Development Management workshop tested the draft MDDG's effectiveness as a planning tool and its usability for both decision makers and consultees on real applications. In this regard a number of technical documents and appendices have been prepared to support the draft MDDG and to expand on particular design elements within the document.
- 3.5 These are:
- Appendix 1 – Technical Documents:
 - Vehicle Car Parking Standards Supplementary Planning Document (SPD), Maldon District Council (MDC) 2006, (or successor document)
 - Essex Design Guide Road Type Table, Essex County Council (ECC), 2017
 - National Space Standards, the Department for Communities and Local Government (DCLG), 2015
 - Technical Document 1 - Noise, MDC, 2017
 - Technical Document 2 - Air Quality, MDC, 2017
 - Technical Document 3 - Older Persons Housing, MDC, 2017
 - Technical Document 4 - Landscape and Arboriculture, MDC, 2017
 - Appendix 2 – How to Measure Density
 - Appendix 3 – How to write a Design and Access Statement
 - Appendix 4 – How to write a Heritage Statement
- 3.6 The MDDG is intended to be a Supplementary Planning Document (SPD) and will be used by:
- Applicants and developers when preparing their development schemes;
 - Planning officers to assess the design quality of development proposals when determining applications and offering pre-application advice; and
 - Members when assessing development proposals.
- 3.7 It is proposed to present the draft MDDG to Members on 1 March 2017 by the Urban Design Officer to show how the draft MDDG works in practice.

4. CONCLUSION

- 4.1 The Draft Maldon District Design Guide sets out the design considerations and design quality expected in the District for all development proposals and aligns with Policy D1 LDP and supporting the LDP as a whole.
- 4.2 The public consultation will be for a minimum of six weeks and outcomes will be reported back to this Committee in due course. Our consultants will assist with the public consultation detailed in paragraph 4.6 of the approved Project Brief (**APPENDIX B**) and will include direct consultation with existing agents, architects and planning consultancies, relevant stakeholder and statutory consultees by web-based consultation, exhibition based consultation and community workshops.
- 4.3 Public consultation is a necessary element of the Council's processes to give weight to Policy D1 and an element of soundness to the LDP.
- 4.4 It is intended that the Maldon District Design Guide will become a Supplementary Planning Document (SPD) as a Tier 3 document to the LDP and henceforth will become a material consideration in the planning process.

5. IMPACT ON CORPORATE GOALS

- 5.1 Planning supports corporate goals which underpin the Council's vision for the District and in particular protecting and shaping the District and balancing the future needs of the community.

6. IMPLICATIONS

- (i) **Impact on Customers** – Planning applications for all development will be determined in accordance with the Maldon District Design Guide ensuring delivery of high quality design.
- (ii) **Impact on Equalities** – The Maldon District Design Guide will inform inclusive development and social cohesion through good design.
- (iii) **Impact on Risk** – The draft Maldon District Design Guide has been prepared in accordance with the submitted Local Development Plan.
- (iv) **Impact on Resources (financial)** – The costs related to the public consultation will be met through the community engagement resource for the LDP.
- (v) **Impact on Resources (human)** – Project Management of the Maldon District Design Guide is in-house by the Urban Design Officer.
- (vi) **Impact on the Environment** – The Maldon District Design Guide will assist the Council in promoting high quality and inclusive design and sustainable development to support new and existing communities and safeguard the character and distinctiveness of the District.

Background Papers:

Local Development Plan 2014 – 2019 as submitted for examination

Maldon District Vehicle Parking Standards SPD, MDC, 2006

Technical Housing Standards - nationally described space standard, DCLG, 2015

Enquiries to: Jackie Longman, Urban Design Officer, (Tel: 01621 875731).

Maldon District Design Guide

CONSULTATION DRAFT

March 2017

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FOREWORD

One of the best things about living in the Maldon District is its unique character and diversity in locations across town and village and in rural, coastal and estuarine environments set against the backdrop of a beautiful natural landscape with a rich history and heritage.

As the Local Planning Authority, an important role of ours is to protect or enhance our valued natural, built and historic environments. One of the ways we will do this is by making sure that all development supports the principles of inclusive design and high quality design and that homes, communities and neighbourhoods are designed well for those that will live, work and visit there.

The design policies in the Local Development Plan together with this new Design Guide will be the key mechanisms to deliver design quality in the District and is intended to assist landowners, developers, applicants, agents, designers and planners in the process of developing and assessing design quality.

This Design Guide promotes best practice in the initial stages of the design process to understand and analyse the context in which development is proposed and to work up a considered design.

As a Supplementary Planning Document [subject to LDP adoption], this Design Guide will be a material consideration in determining planning applications and is assisted by a suite of technical documents. The technical documents focus in more detail on the content of this Design Guide including landscape character, open space and play space, air quality and noise, car parking and internal space standards. Where area specific Strategic Masterplan Frameworks and Design Codes are in place, this Design Guide should be read alongside their design principles and technical specifications.

I sincerely trust this Design Guide will inspire those who use it and will promote the design quality expected in the Maldon District.



Cllr Mrs P A Channer
CC Chairman, Planning and Licensing Committee
Maldon District Council

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A1 Appendix 1 - Technical Documents

National Space Standards, DCLG, 2015

Noise

Air Quality

Older Persons Housing

Landscape and Green Infrastructure

Vehicle Parking Standards, MDC, 2006 (or successor document)

Essex Design Guide - Road Type Table

A2 Appendix 2 - How to Measure Net Density

A3 Appendix 3 - How to Write a Design and Access Statement

A4 Appendix 4 - How to Write a Heritage Statement

A 01 THE OPPORTUNITY

Maldon District is at an important point in its history, one where the decisions it makes today will shape and inform the quality of the District as a place to live, work and visit.

The Local Development Plan (LDP) sets out the spatial vision for growth by ensuring that the District's unique identity is protected by maintaining high design standards and the principles of sustainable design in new development.

A key component is the rise in the number of new households in the District over the next 15 years and beyond. The LDP aims to address the needs of an ageing population, inward migration and reduction of household sizes by increasing the delivery of housing to respond to the projected need to sustain the economic and population growth over this period.

In order to provide the design guidance to supplement the Policies in the LDP, the Maldon District Design Guide (The Guide) provides a clear direction on what is required to deliver the high design standards expected in the District.



Coastal Farmland



Wooded Farmland



Countryside



Riverside



Estuarine



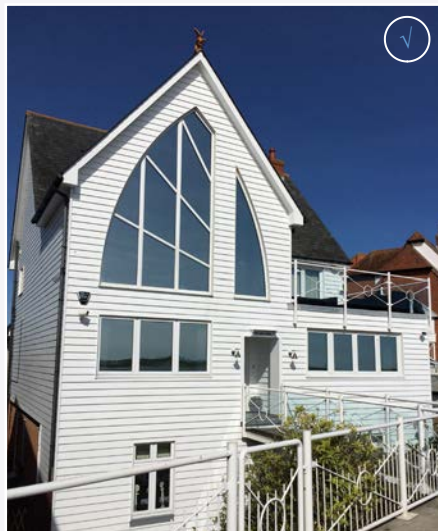
A 02 PURPOSE OF THE GUIDE

This Guide has been prepared to set out clear design principles to guide future development in the District. It aims to provide general guidance to encourage a designed approach to all development from large residential schemes to modest residential extensions and small infill developments.

The design principles have been prepared for all development to respond to. The principles are accompanied by illustrations and good practice examples to gain a better understanding of how to deliver good design and clearly signposts where more detailed guidance can be accessed and when these should be taken into account.

This Guide is intended to be a user-friendly tool and does not seek to replicate existing policy and regulations that will continue to apply to all development. The Guide should be read by:

- Developers and builders in considering potential development proposals.
- Householders considering residential extensions.
- Designers in drawing up schemes.
- Development management officers in assessing the suitability and determination of planning applications.
- Statutory and non-statutory consultees and members of the public in commenting on planning applications.



The Quay, Burnham on Crouch



The Salt House, St Lawrence

A 03 THE VALUE OF GOOD DESIGN

Good design has an impact on all aspects of the built and natural environment, be it a simple extension to an existing house or a large residential or commercial development.

Good design translates into more than the appearance of buildings. It is important in both small residential extensions and large scale developments where form and materials are introduced and new streets and spaces are created. Functionality and practicality are embedded in the design and are as important as the visual quality of a building, town centre or rural intervention.

Perhaps the greatest benefits of good design are felt in our own homes and the spaces around them. Well-designed neighbourhoods help to build communities, give them a sense of belonging and make residents feel safe. Often this can be through simple approaches such as natural surveillance, an easy technique created when new streets and public open spaces are overlooked by windows and doors.

Carefully positioned car parking and cycle storage, as well as integrated refuse and recycling bins also help to create a sense of order and reduce litter and vandalism.

The quality of open space and the way in which new streets and spaces are designed has a direct effect on how people feel about a place and the whole community benefits from a commitment to usable green space. Access to open space is also shown to have a direct impact on the health and wellbeing of those able to take advantage of it.

For commercial development well designed buildings are good for business. Flexibility to respond to changing social and economic circumstances is important, as are design solutions which encourage creativity and innovation. Everywhere, investment in good quality design is shown to provide a higher return on the investment made.

Good design in all development is inclusive and accessible for everyone, has a positive impact on the environment, is integrated into its immediate and wider surroundings, provides flexibility for future change, is easily maintained and delivers a return on investment.



Commercial Example



Residential Example

A 04 KEY DESIGN OBJECTIVES

Design quality is a result of an ambition and creativity brought together in a robust design process. The ambition often relates to key factors like character, safety, diversity, movement, legibility, adaptability and sustainability. It is critical that even before the design process commences the right set of design objectives are established. Listed below are some of the key objectives that set the scene for design quality. Throughout the design process they remain the focus of the creative thinking with some being more important than others depending on the project.

Character	Enhance identity and sense of place. All design proposals should be informed by a contextual analysis of the area. Respond to the scale, height, density, urban grain, massing, type, and landscape details of the surrounding area.
Safety and inclusion	Ensure places are safe, secure and welcoming for all, including the elderly and disabled. High quality design with no differentiation between market and affordable housing. Understand and address the needs of all potential users to ensure inclusive design. Create safe communities and reduce the likelihood of crime and antisocial behaviour.
Diversity	Provide variety, choice and sensory richness in the design. Incorporate a mix of uses and facilities as appropriate with good access to public transport and a wide range of house types and tenures.
Ease of movement	Ensure places are easy to get to and move through. Allow for access to local services, facilities and open spaces, and where needed, provide new facilities, services and open space. Ensure a sufficient level of well-integrated and imaginative solutions for car and bicycle parking and external storage including bins.
Legibility	Ensure places can be easily understood. Ensure that streets and spaces are overlooked creating a positive relationship between fronts and backs of buildings, with clearly defined public and private spaces

Adaptability and Quality	Anticipate the need for changes in buildings and outdoor spaces. Design places that function well today, last for the future and are easy to adapt to changing requirements of occupants and other circumstances at any time. Construct buildings that are flexible to accommodate changing needs.
Sustainability	Minimise the impact on our environment. Use land efficiently whilst respecting the existing landscape character and green infrastructure. Enhance biodiversity and as a minimum, deliver schemes that lead to no net loss of habitat. New development should be sustainable and resilient to climate change by taking into account landform, layout, building orientation, massing and landscape to minimise energy consumption and mitigate water run-off and flood risks.
Designing for future	Design buildings and spaces and use materials that can be maintained over time and will age well. Take account of possible future development in the local area. Consider potential for future expansion of the development.
Good streets and spaces	Provide a clear and permeable hierarchy of streets, routes and spaces to create safe and convenient ease of movement by all users. Create places with attractive outdoor spaces. Incorporate or link to a well-defined network of green spaces and water. Establish a high quality public realm with well managed and maintained public areas.

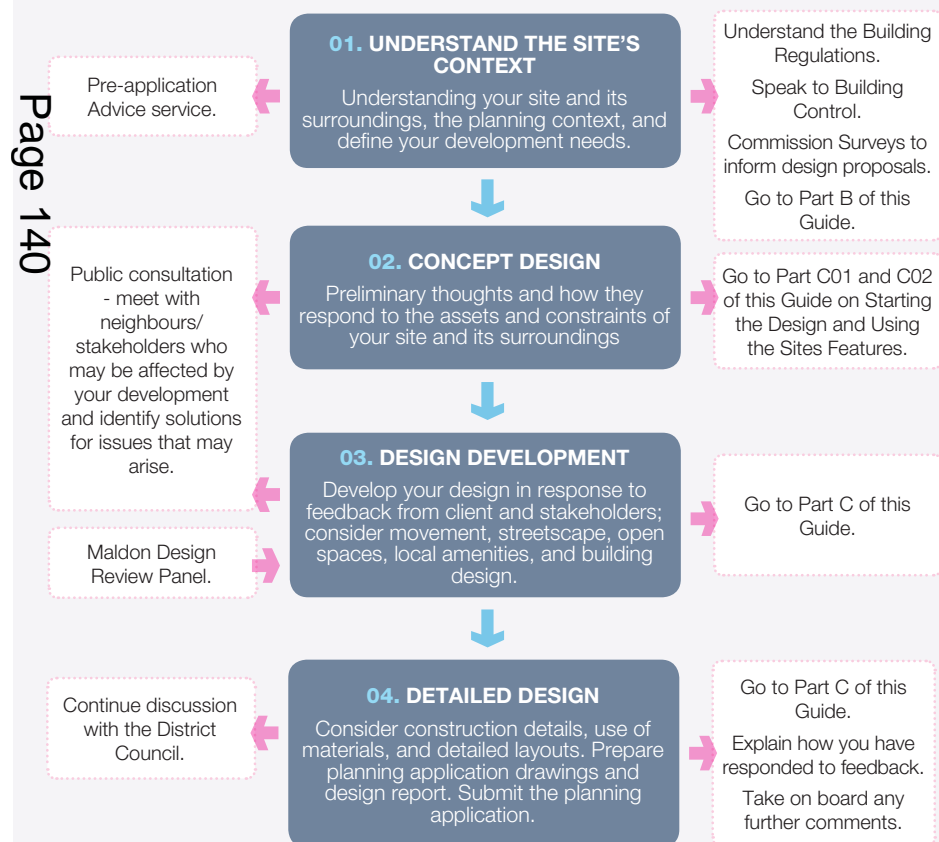
+ Additional useful guidance:

- By Design, Urban Design in the Planning System: Towards better practice (2000)
- Urban Design Compendium 1 (3rd edition, 2013)

A 05 WHAT IS THE PROCESS?

Having defined the objectives we need to understand the next steps. The RIBA (Royal Institute of British Architects) has created a plan of work which helps everyone involved in the design, planning and construction process in a logical manner. This process considers all relevant issues, constraints and opportunities, engages with key stakeholders and the public (where appropriate) and then through an iterative process it applies creative thinking to these opportunities into development propositions.

The diagram below provides an overview of the issues to be addressed at different stages in the design process. These requirements will be dependent on the scale and complexity of the application.



Flow chart indicating design process

A 06 WHO TO TALK TO?

Depending on the scale and nature of the scheme, it may be appropriate to engage with relevant statutory authorities and organisations and officers within the District Council. This should be done as early in the design process as possible.

In addition, it may be appropriate to talk to neighbours and carry out public consultation with the existing community as proposals can affect a wide range of people. The Government promotes a proactive approach to planning where community engagement and effective consultation are carried out prior to the submission of a planning application.

This can be used to gather views on key aspects of your site, and its context, which are considered important to the local community. Their views on your initial options and ideas will help gain an understanding of any concerns that the community may have in relation to your application.

There are a number of ways to consult or engage with the town and parish council, the community and other interested parties.



Public consultation and exhibitions

+ Statutory authorities and organisations that might be relevant to consult to provide initial advice:

- Natural England: Landscape, Green Infrastructure and Biodiversity.
- Historic England: Heritage Assets.
- Environment Agency: Flooding, Rivers and Pollution.
- Utility Companies, including Anglian Water.
- Highways: Essex County Council Highways.
- Town and Parish Councils.
- Urban Design, Landscape, Highways, Ecology, Trees, Education, Environmental Health and Conservation officers.
- Conservation Advisory Groups.
- Neighbourhood Planning Forums.

Refer to the Council's Statement of Community Involvement for further information and see page 10 of this guide.

PART B MAKE IT MALDON DISTRICT

B 01 MALDON DISTRICT CONTEXT



Steeple Village Hall, a rural village context

B 01A. LOCATION AND PLANNING DESIGNATIONS

- Identify statutory or non-statutory designation.
- Refer to B 02 Helpful policy and guidance to consider



B 01B. PREPARE A CONTEXTUAL ANALYSIS

- Understand surrounding landscape, biodiversity, heritage and built form
- Refer to B 03 Landscape settlement and character and B 04 Locational characteristics



B 01C. IDENTIFY ANY CONSTRAINTS AND OPPORTUNITIES

- Identify key constraints and define opportunities
- Refer to B 05 Site appraisal



RESPOND

- Concept design (stage 02)
- Design development (stage 03)
- Detailed design (stage 04)
- Refer to PART C

The previous section outlined the purpose and value of the Guide and the design process applicants should follow in developing their proposals. This Part of the Guide, outlines the District Council's requirements on how applicants should assess the context and character of their site to ensure that new development will respect, respond to and enhance the unique characteristics of the settlement at Stage C01.

The aim of this Part of the Guide is to help the applicant understand the context, and character of the area and establish the constraints and opportunities that will guide their proposals.

The applicant must have an understanding and respond to the specific context of their site and appreciate that the application of design principles within this Guide will depend on the location within the District. This chapter provides an overview of the various parts of the District.

The first step is to demonstrate a clear link between the appraisal of the context, any applicable planning designations, the character of the site, physical constraints and opportunities and the development proposals. This link or rationale will need to be explained through the Design and Access Statement that will accompany the planning application.

The steps required in this process are set in the flow chart below.

The environment of the District is protected by a number of local, national and international designations, including: SSSI, Special Areas of Conservation, Conservation Areas, Listed Buildings, Registered Parks and Gardens, Registered Battlefield Sites and Scheduled Ancient Monuments, which seek to preserve the area's natural and built environment for future generations. The applicant should check the LDP Proposals Map, and carry out their own desktop analysis referring to the Council's website for further details.

Flow Chart indicating the process that applicants should follow to respond to the site and setting

B 02 HELPFUL POLICY AND GUIDANCE TO CONSIDER

All development is likely to be subject to local planning policy and national planning guidance. This can be in the form of helpful documents or more prescriptive 'acts' set within a legal framework.



National and local planning policies will influence whether a site is suitable for development and the form and nature of development. The applicant should carry out a planning review of relevant the planning policy documents. In addition, there are a series of other documents, including Neighbourhood Plans, Village Design Statements (VDS), Conservation Area Reviews and Appraisals, Masterplans and Design Codes, which have been adopted or endorsed by the Council - these are material planning considerations in planning decisions and should be considered in the design of new development. These documents provide a key source of local policy interpretation to supplement the policies in the LDP. Details of the documents endorsed or adopted are available on the Council's website - <https://www.maldon.gov.uk>.

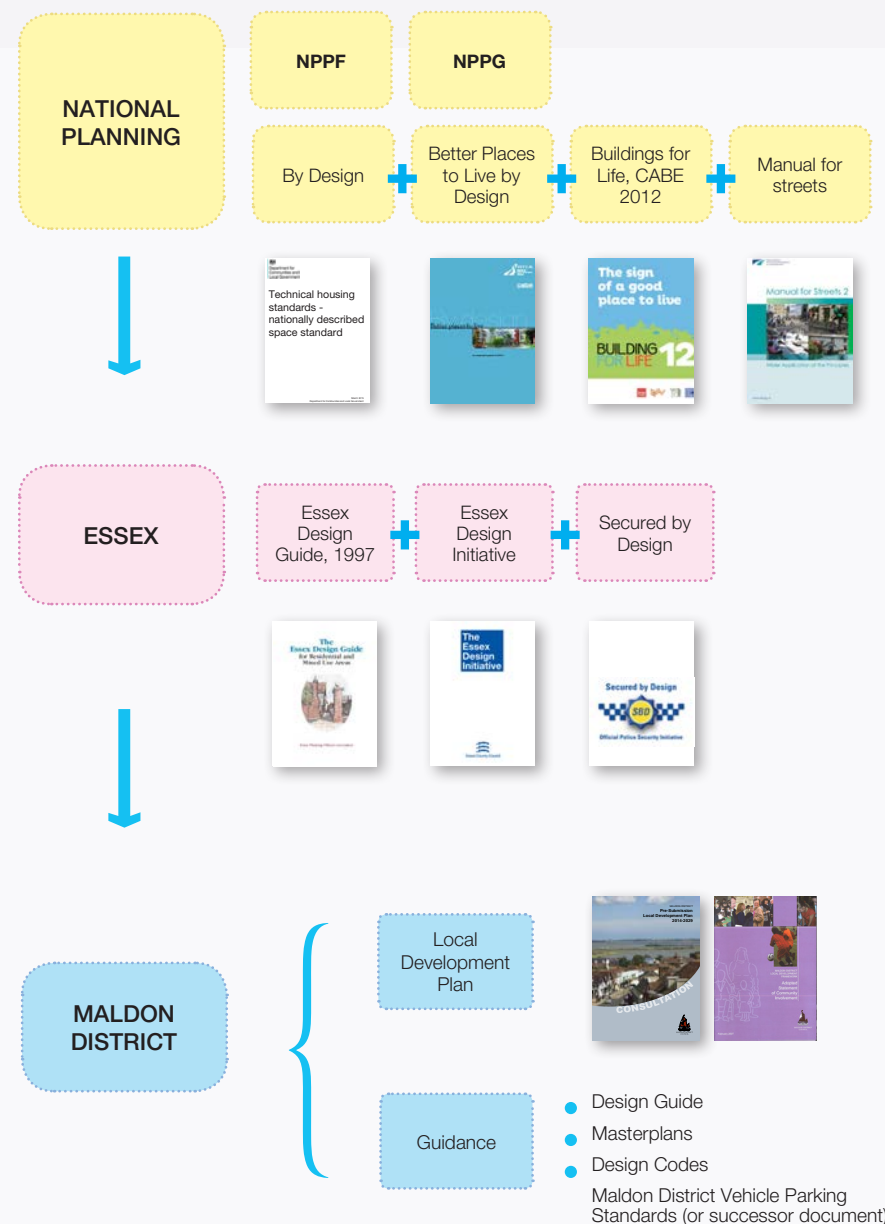
Permitted Development

In some instances, construction may be able to proceed without the need for a formal planning application / approval. This is known as 'Permitted Development' (PD) rights. They derive from general planning permission granted not by the local planning authority, but by Parliament. Details are available from the Department for Communities and Local Government website.

Even if you do not need to make a planning application, you should follow good design principles, with materials, forms and architectural detailing.

Consider Best Practice

In addition to planning policy, applicants should consider best practice in terms of sustainable design, creating better environments and the quality of the built form. Further advice is available from the Homes and Communities Agency (HCA), the Commission for Architecture and the Built Environment (CABE), English Heritage/Historic England, Landscape Institute publications.



B 02 HELPFUL POLICY AND GUIDANCE TO CONSIDER

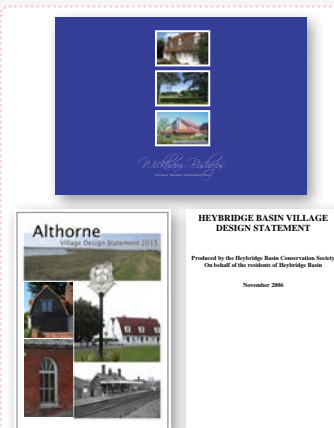
Pre-application advice and service

Regardless of the scale of development proposed, the Council is committed to provide an effective planning service which will add value to the design quality of your scheme. Early discussion between the applicant and the planning authority will help reduce delays and potential uncertainties by identifying any issues at the earliest stage.

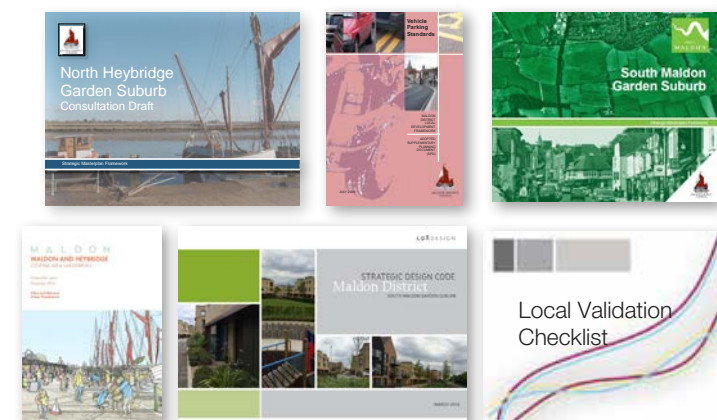
BUILT, HISTORIC AND NATURAL ENVIRONMENT



NEIGHBOURHOOD PLANS AND VILLAGE DESIGN STATEMENTS



SPDS, MASTERPLANS AND DESIGN CODES



+ Additional Useful Guidance:

If you require any guidance in respect of what may or may not be permitted development you should contact the Council's planning department or refer to the publication 'Permitted Development for Householders: Technical Guidance' (Department for Communities and Local Government, updated in April 2014, http://www.planningportal.gov.uk/uploads/100806_PDFforhouseholders_TechnicalGuidance.pdf).

+ Further Information

Homes and Communities Agency www.homesandcommunities.co.uk

The Commission for Architecture and the Built Environment (CABE) (<http://www.designcouncil.org.uk/our-services/built-environment>) and English Heritage/Historic England publications; particularly Understanding Place series and Constructive Conservation series (<http://www.helm.org.uk/guidance-library/>).

Landscape Institute - <https://www.landscapeinstitute.org>

B 03 LANDSCAPE SETTLEMENT AND CHARACTER

The natural landscape of the District is largely shaped by the estuaries of the Blackwater and The Crouch, and the Chelmer and Blackwater rivers and valley sides together with their extensive flat and undulating alluvial plains. These have created a subtle range of landscapes which have and continue to influence the pattern of development.

The nature of the landscape is set out in detail within the Council's Landscape Character Assessment and each Landscape Character Area (LCA) represents an important consideration when preparing new development proposals. All landscape features need to be acknowledged within any new development and present opportunities to influence it in a manner unique to the location. Whilst some features may initially appear to limit new building, with careful consideration they will help shape development in a way that strengthens local character and creates high quality solutions.

Six differing landscape types are identified within the Landscape Character Assessment i.e. River Valley Landscapes, Estuarine Marsh; Drained Estuarine Marsh; Coastal Farmland Landscape; and Wooded Farmland Landscapes. Each has its own distinct characteristics which affect the patterns of built settlement. A summary of the six landscape types is set out below.

LCA A: River Valley Landscapes are found around the Lower Chelmer and Blackwater river valleys. Generally they have flat or gently undulating valley floors which in part have a wooded character. In the Lower Chelmer Valley small settlements are dispersed along the valley sides or clustered around bridging points, e.g Langford.

In the Blackwater River Valley, isolated farmsteads are found on the valley slopes with linear villages centred on the roads. All the settlements are screened by deciduous trees.



Drained Estuarine Marsh Landscape



Estuarine Marsh Landscape

LCA C: The Estuarine Marsh is composed of mudflats and marsh which combine to create a flat, open landscape with a sense of remoteness and tranquillity. Generally there is an absence of trees and hedgerows and whilst there are no settlements within the area, North Farnbridge overlooks it.

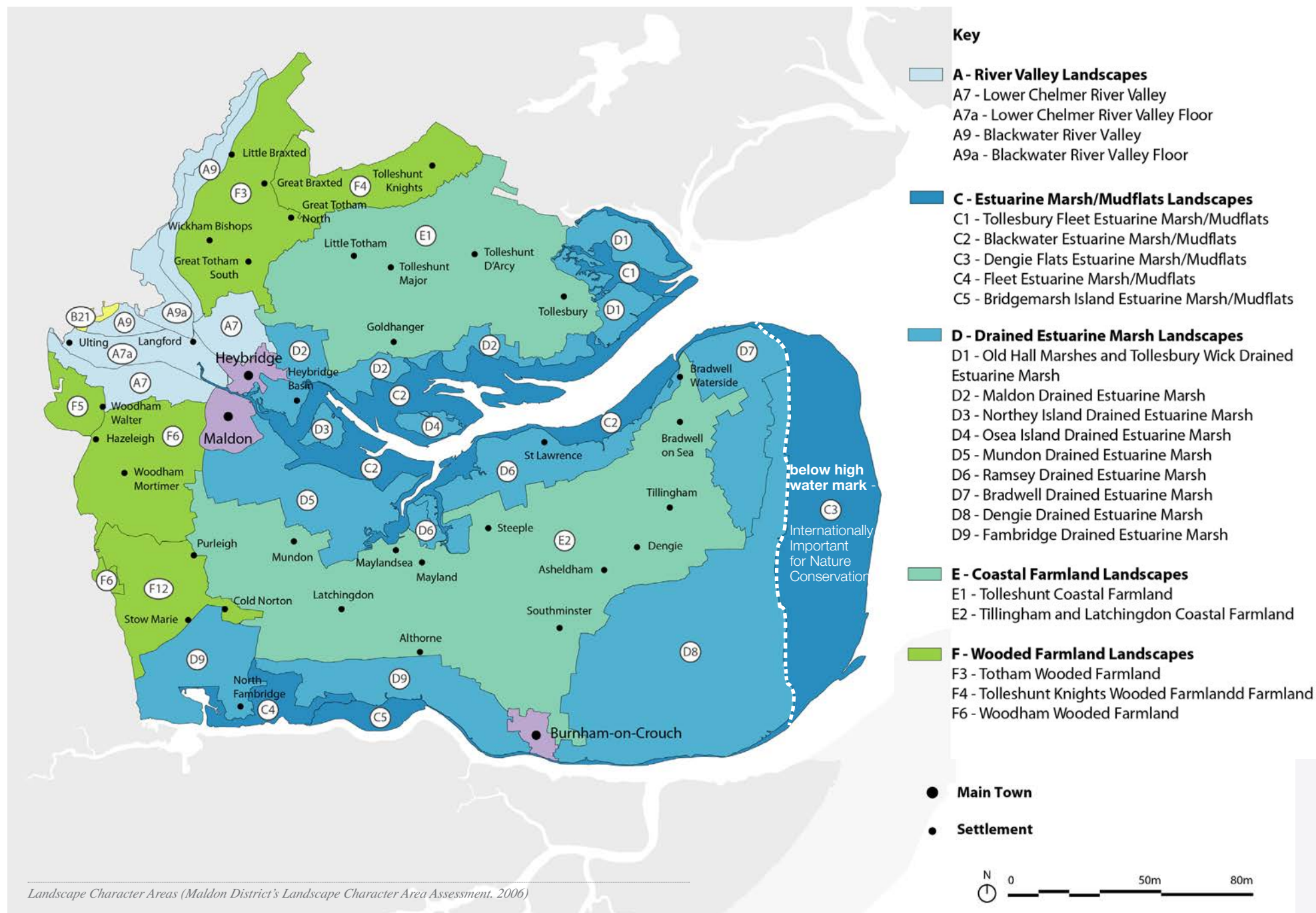
LCA D: The Drained Estuarine Marsh Landscape is one of flat former salt marsh now devoted to grassland and cultivated fields. Whilst there is an absence of trees and woodland, the drainage ditches create a distinctive pattern within the landscape. Historically, settlement has been dispersed across the area in the form of farms and agricultural buildings. The urban fringe of Maldon and the tourist attractions of Heybridge Basin create a visually more intrusive presence. As with the Estuarine Landscape, the character of the place is one of isolation and tranquillity.

LCA E: The Coastal Farmland Landscape is substantially flat and artificially drained to create agricultural land with distinctive ancient rectilinear field pattern. There is an absence of woodland and the settlement pattern is sparse. The network of rural lanes which serves the area is small in scale and sensitive to change.

LCA F: The Wooded Farmland Landscape is predominantly elevated undulating hills or ridges and slopes. It includes a mixture of arable and pasture farmland. It includes blocks of mature mixed and deciduous woodland (including areas of ancient and semi-natural woodland); copses, hedges and mature single trees, and mature field boundaries. It provides framed views to adjacent character areas. In places it has an enclosed character with network of quiet, often tree-lined narrow lanes.

+ Further information can be obtained from Maldon District's Landscape Character Area Assessment. 2006 (or successor document).

Further advice on assessing the landscape character and the visual impact of larger development can be obtained from 'An Approach to Landscape Character Assessment' by Nature England and 'Guidelines for Landscape and Visual Impact Assessment' by the Landscape Institute.



B 04 LOCATIONAL CHARACTERISTICS

Preparing a Character Study

The level of information in the study should be related to the scale of the development proposal. For example a proposal for large-scale urban extension, should be supported by an extensive study to consider the extension in the context of the settlement, and its movement and green infrastructure network. It should carefully consider how the development would integrate with and enhance the settlement. However, an application for infill development or a single dwelling may consider the character of the street and the neighbouring properties to inform how the development can successfully complement the streetscene.

Consider the Context

The starting point for the character study is a consideration of the wider context of the applicant's site. The application of the principles within the Guide may vary depending on the location of the site within the District. English Heritage's Place Check tool kit may assist applicants when a character study is undertaken (see Appendix 4 - How to Write a Heritage Statement).

For most major applications in the rural area or urban fringe / countryside edge a professional landscape appraisal or landscape and visual impact assessment (LVIA) will be also required.

Within the District, there are four main settlement contexts:

1. **Main Towns** - Burnham on Crouch, Maldon and Heybridge. Predominantly an urban context where the buildings, hierarchy and mix of uses and the public realm are the dominant features and is defined by a series of streets, squares and courts. Within the core of the town centres, the landscape takes a complementary role within this context, but adds significantly to their setting.
2. **Agricultural Settlements** - Althorne, Asheldham, Bradwell-on-Sea, Dengie, Hazeleigh, Langford, Latchington, Mundon, Purleigh, Southminster, Steeple, Stow Maries, Tillingham, Tolleshunt D'Arcy, Tolleshunt Knights, Tolleshunt Major, Ulting, Little Braxted and Great Braxted. Buildings are loosely clustered to define space in key locations such as around nodes, main streets and defining important spaces for example Village Green. The landscape features define the space. The Parish Church is the focal point of villages in these areas.
3. **Riverside and Maritime Settlements** - Bradwell Waterside, Goldhanger, Heybridge Basin, The Maylands, North Fambridge, St Lawrence and Tollesbury. In most of these villages the waterfront provides a strong feature of character and are closely linked to the open nature of the surrounding landscape.
4. **Arcadian Settlements** - Cold Norton, Great Totham (North and South), Little Totham, Wickam Bishops, Woodham Mortimer and Woodham Walter. Houses are generally in large plots and partially hidden from the public view nestling in mature tree cover, hedgerows and landscaping and structural planting.



Main town and setting



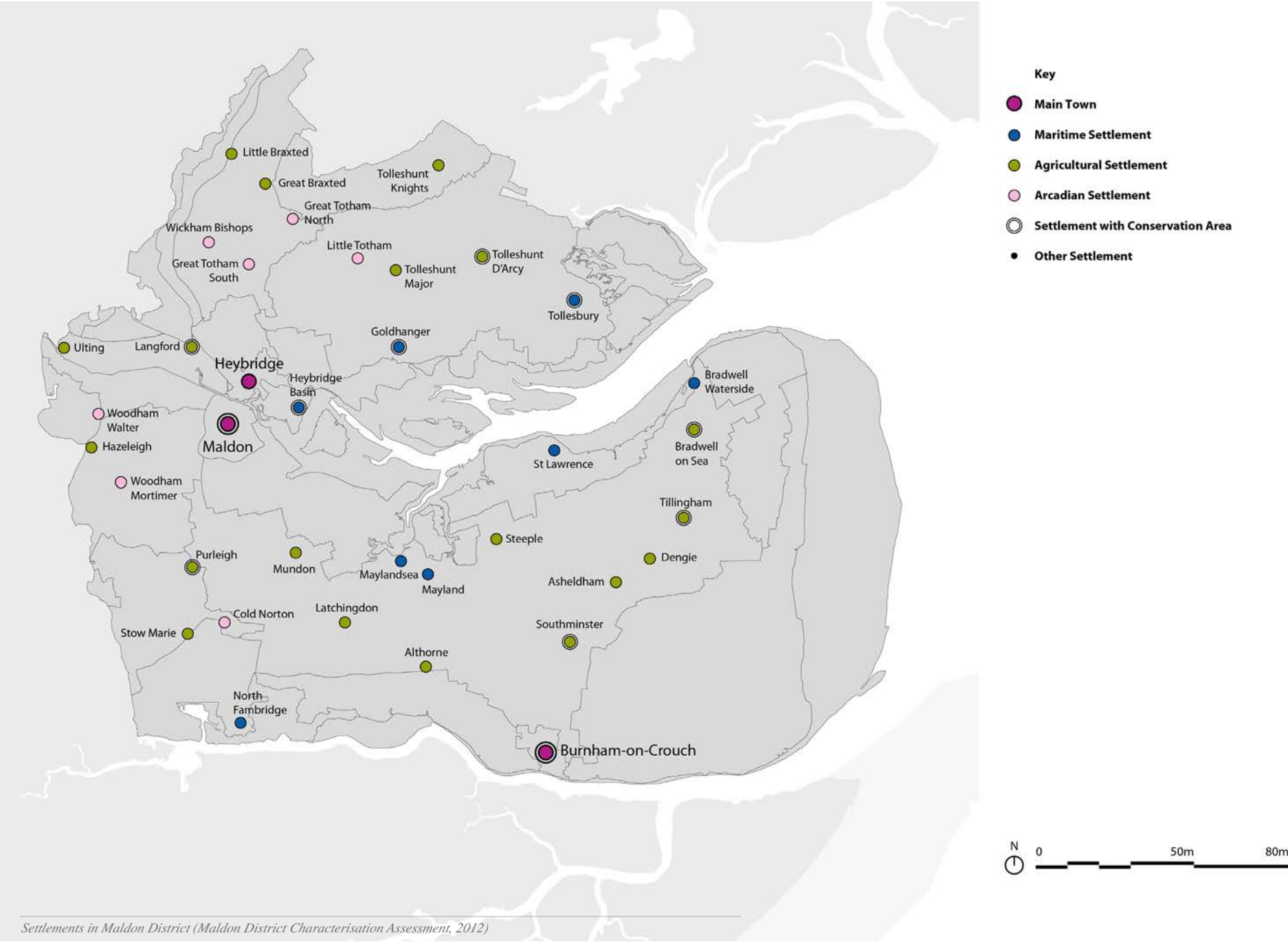
Riverside/Maritime



Rural Edge



Arcadia



B 04_01 MAIN TOWNS

Unique qualities that may apply to your development

The main towns in the District are **Burnham on Crouch, Heybridge and Maldon**. These have different characteristics but are all most prominent within their established historic centres and relate to their landscape settings. The main towns are located at river / estuarine locations that have a strong influence on character and riverside setting.

A number of key factors define the quality of the built and natural environment and should be considered carefully at the start of the design process. These relate to layout and townscape, building form and massing, public realm and landscape, materials and details. Having an understanding and appreciation of the characteristics of the town and its landscape context and linking this to the exact location in the settlement, provides a good base to start the design process, regardless of how small or large the development is. Whilst town centre interventions may be smaller scale and be focused on details and materials, establishing a positive relationship with the building form and massing that defines the character of the place will be equally important.

Layout and Townscape

The character in different parts of the town is largely defined by the size of individual land ownerships. In the historic centre, the urban fabric is often a finer grain than in the rest of the town because of smaller plot sizes and fragmented land ownership. These characteristics are important, especially when planning larger developments out of the main town or on the edge of town as are topography and distant views.

The manner in which buildings front streets contributes to specific townscape qualities. Whilst most streets in the main towns have continuous frontages with breaks to accommodate only other streets and lanes, in other parts of the towns streets feel open as they accommodate more detached buildings. The townscape qualities can be defined by the ratio of building height to street width and relate to street hierarchy.

Varied rooflines in all three main towns contribute to creating a sense of scale and character. It is important in the design process to carefully consider and respond to the context and mix of existing pitches, volumes, materials and details.

Building Form and Massing

Building heights vary from 1 to 4 storeys, with variations on floor-to-floor height, depending on the use, location and age of the building.

Form and massing is most often related to topography, orientation, use and location within the town. Buildings along main streets are generally of larger scale and vertical proportions, whilst buildings on the edge of towns respond to the landscape with more fragmented and horizontal proportions and two storeys.

Roofs types relate to building footprint and use. Roofs in main towns are steeper than those on the edge of town. Roof volumes often vary within one building and make a major contribution to the character of an area.

The existing streetscape in all three main towns is characterised by variations in building height, massing, and colour. Where a larger plot is developed, the design needs to be

sufficiently varied to relate to the surrounding streetscape. This can be achieved through breaking up the overall block mass into smaller buildings. Façades can also provide appropriate vertical emphasis through the fenestration and/or use of colour and material.

Public Realm and Landscape

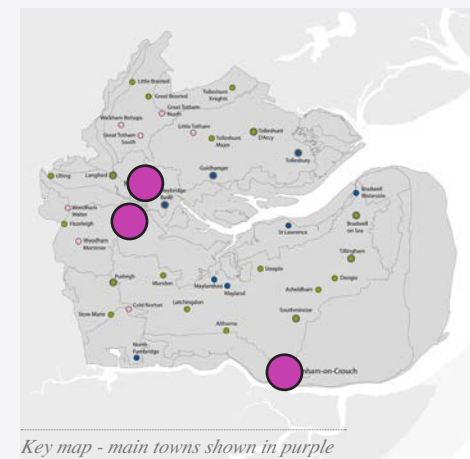
The character of the main towns is defined by the network of spaces and routes and their treatment. For example, buildings in the town centre are often built against the street edge and frequently have no privacy strip or defensible space. Whilst the public/private boundary is defined by soft landscape in developments on the edge of town. The provision of good quality public spaces and direct routes to facilities and services play a key role in integrating new development into the existing urban fabric.

The provision of appropriate amenity space, car parking and bin storage affects not only the quality of the development but has a direct impact on the character of the area.

Trees, planting and SuDs are also important for the successful integration of developments into the existing urban and landscape fabric in both the main towns and on the edge of settlements.

Materials and Details

Materials and colours provide the visual link to the surrounding context and the careful selection of these is very important. Having in mind the historic context of most of the towns, the texture and weathering qualities of materials used in new developments should ideally reflect those of the traditional ones. Details highlighting verticality e.g chimneys, doors and window designs, play a key role in defining character.



Key map - main towns shown in purple

left: proportions and vertical emphasis illustration (c 1877 and 2003)

B 04_02 AGRICULTURAL SETTLEMENTS

Unique qualities that may apply to your development

The following settlements have been defined as agricultural settlements in the Maldon District Characterisation Assessment: Althorne, Asheldham, Bradwell Village, Dengie, Great Braxted, Hazeleigh, Langford, Latchington, Little Braxted, Mundon, Purleigh, Southminster, Steeple, Stow Maries, Tillingham, Tolleshunt D'Arcy, Tolleshunt Knights, Tolleshunt Major and Ulting.

All Agricultural Settlements are located above the reclaimed marshes, on clay lands in the central core of the Dengie peninsula and also grouped to the north of the Blackwater Estuary. The villages are linear or clustered in form with the majority dominated by a landmark parish church located at the heart of the settlement. At their edges landscape character influences context.

The village centre where the church is located is usually compact with buildings developed close to each other and fronting directly onto the street. Gardens to the rear of these houses are narrow and long, with some opening into farmlands. The larger settlements include more than one centre as the edges were developed in the 20th century. At the village edge the overall pattern of development is open with buildings set back from the street. Local landscape character strongly influences character at the settlement edges.

Layout and Townscape

The change between the centre and edge of village is often pronounced and identified by the point where buildings begin to dominate and enclose views, as opposed to them just fronting a street that accommodates front gardens, trees and hedges. To retain this characteristic, infill and new development should enclose and front the street in a similar manner. Where larger sites are developed on the edge or outside the village boundaries the main consideration is the sensitive rural edge where new development relates to landscape character.

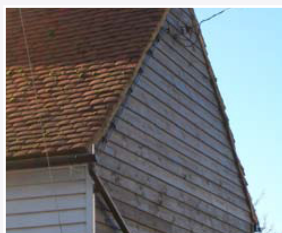
Building Form and Massing

Form and massing is varied and related to topography, orientation and use. Public buildings are usually of larger scale than domestic buildings.

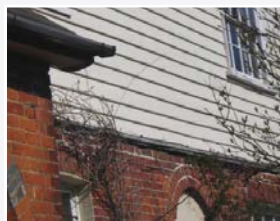
The variety of house sizes located in the village centre is reflected in the height with



Typical details: weather-board, tiled gambrel roof, Bradwell-on-Sea



Typical details: steep plain tile roof, with weatherboard painted white. Tillingham



Red brick and weatherboard, Eveleigh House, Purleigh

buildings of 1, 1.5, 2 and 3 storeys. Where 2 storey houses dominate the townscape they have a different floor-to-floor height adopted at the time of construction. The predominant height at the edge of village is 2 floors and floor-to-floor height is more consistent.

Wide span roofs with shallower pitches dominate the edges and gabled roofs with some half hipped examples are common in the village centre. Gambrel roofs provide attic space on first or second floor and are a local characteristic.

The facades are dominated by solid walls with vertical emphasis on openings such as doors and windows. In village cores, occasional shop windows provide a good contrast.

Public Realm and Landscape

The character of the village centre is defined by a central space and narrow routes. The space relates to the church and is enclosed by continuous building frontages. Buildings in the village centre are constructed against the street edge with no front gardens.

The private space in front of houses on the edge is important as it defines the public/private boundary. Planting around the front gardens is mature and makes a significant contribution to the character as it starts to blur the edge between village and countryside.

Trees, structural planting and SuDs form important green infrastructure that successfully integrates developments on the edge and outside the village boundary into the existing urban and landscape fabric.

Native hedgerows and hedgerow trees are distinctive features at the settlement edges.

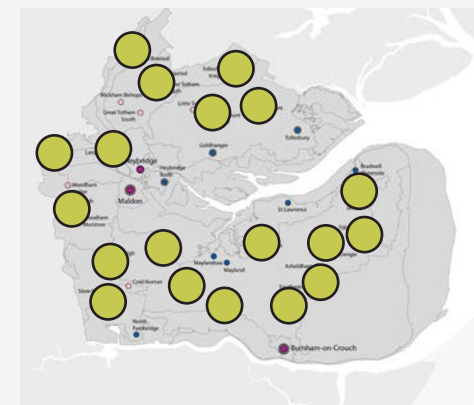
Materials and Details

Materials and colours provide the immediate visual link to the surrounding context and the careful selection of these is important. Most of the agricultural villages include painted timber, red or yellow brick and render.

Windows are flush with the wall. Dormer windows set apart from each other are seen on high pitched and gambrel roofs. Details that highlight verticality such as chimneys, doors and windows, play a key role and are important for defining character.



Brick and window detail, Bradwell on Sea



Key map - agricultural settlements shown in yellow

B04_03 RIVERSIDE AND MARITIME SETTLEMENTS

Unique qualities that may apply to your development

The settlements defined as maritime and riverside by the Characterisation Assessment are: Bradwell Waterside, Goldhanger, Heybridge Basin, The Maylands, North Fambridge, St Lawrence and Tollesbury. These villages are located in low lands adjacent to the Blackwater Estuary and the River Crouch. The estuaries themselves are of International Importance for Nature Conservation. These settlements have developed either as ports or recreation areas and have less defined urban character but, are closely linked to the open nature of the surrounding landscapes.

Aspects of some port and riverside settlements are found on the waterfronts of both Maldon and Burnham on Crouch.

Layout and Townscape

The maritime and riverside settlements are characterised by concentrations of development along the waterfront which act as the centre of gravity. The relatively flat topography defines the regular layout of narrow streets and blocks. Plots are small and accommodate a mix of houses and boat sheds.

Houses are usually set back from the street and modest in scale with pitched roofs that are shallow in comparison to other parts of the District. Some streets are lined with trees and hedges that dominate the townscape.

Building Form and Massing

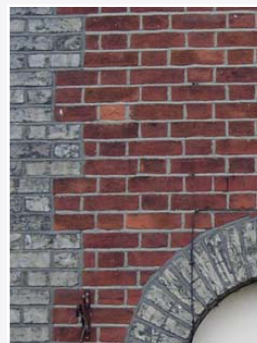
Building height is predominantly 1 or 2 storeys. Form and massing relate to topography and the open feel of the waterfront. Views and vistas influence form and massing of buildings and the domination of horizontal proportions of the facades.



The Sail Lofts, Tollesbury



Typical details: vertical sliding timber sash windows, Heybridge Basin and weatherboarded cottage, Goldhanger



Typical details: red brick with quoins of gault brick, Heybridge Basin

Rooflines and chimneys are less dominating elements of the townscape. Whilst solid walls dominate in older buildings, windows and openings are larger and more irregular in newer developments. These architectural features play an important role to reflect light and create shadows.

Public Realm and Landscape

The relationship of public space with the waterfront and how it is accessed and framed is of critical importance to the maritime settlements. The integration of SuDs and the use of open space to alleviate flood risk are important considerations for developments in these areas.

Materials and Details

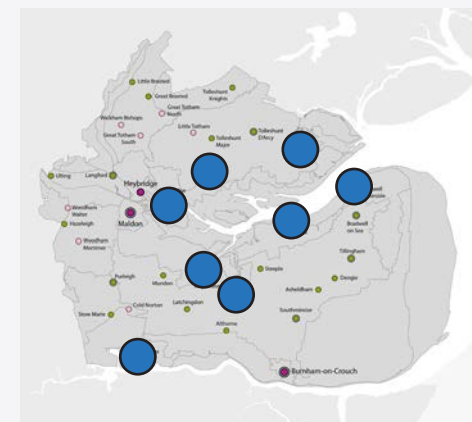
Traditionally buildings in this area were built of timber and timber remains the material that characterises the area. The facades are often finished in horizontal timber boarding which is in its natural weathered colour or painted white or black.

House Boats

The District has small groups of houseboats along the rivers and estuaries which provide a unique type of housing framing part of the District's varied character. House boats are a form of residential development similar to that of land based housing, requiring similar infrastructure such as car parking provision, access roads, refuse disposal points and where possible access to main infrastructure works. Development will be required to take into account nature conservation and the character and appearance of the area.



Typical details: white and black weather-board, Heybridge Basin



Key map - riverside and maritime settlements shown in blue

B 04_04 ARCADIAN SETTLEMENTS

Unique qualities that may apply to your development

The following settlements have been defined as Arcadian by the Maldon District Characterisation Assessment: Cold Norton, Great Totham (North & South), Little Totham, and Wickham Bishops, Woodham Mortimer, Woodham Walter.

Although these settlements have different characteristics, they do have much in common such as their rural environment integrated by the picturesque approach to landscape design and structural planting. This approach is characteristic of the layout of parks of great country houses in the eighteenth century.

Recent years have seen an increase of infilling of spaces between houses, rebuilding of houses with a larger footprint than their relatively modest predecessors and the addition of large groups or small cul-de-sac estates to the edges of the villages. This has gradually eroded much of the original Arcadian character. Where there has been the retention of trees or structural planting, the need to create new vehicular accesses, footways and parking areas has caused the removal of soft verges and hedges resulting in the disappearance of a sense of soft enclosure and in some cases seclusion.

A number of key factors define the character of development in the settlements and should be considered carefully at the start of the design process.

Layout and Townscape

The layout of the Arcadian villages is characterised by small farmsteads, with cottages and modern houses present along rural lanes, especially in the east of the area. There are various dispersed groups of hamlets, each with its own specific but small historic core.

The size of dwellings varies with larger houses and small bungalows placed centrally in large gardens. The earlier houses still follow traditional forms and roof spans with additive compositions of wings and ranges sometimes at right angles to the main house. Plans can be varied, with no predominant arrangement of entrances or main windows.

The building of some larger houses and villas for residents emerged in the 19th century. More recently, development has seen areas of higher density housing evolve as incremental small scale developments have crept along and between the existing lanes, with new cul-de-sacs and the loss of the overall dominance of trees and soft landscape.

Building form and massing

Traditional storey heights and roof forms were present until the mid twentieth century following the precedents of pitched roofs with chimneys.

The existing streetscape is characterised by an original network of lanes, where properties are often set back behind substantial front gardens. The roadside hedges, trees, green verges and ditches were retained. Deep gardens to the side and rear were retained with planted boundaries. The density can be as low as six dwellings per hectare.

Public realm and landscape

The Arcadian character is derived mostly from the qualities of the surrounding landscape. Buildings of varied architectural style can fit into this character if they are integrated into landscape and relate to the unique environment.

The existing tree cover and vegetation, including grass verges, of a site must be retained and enhanced by new planting of native and appropriate species. Hedges or other appropriate natural boundary treatments should be used to provide enclosure to front gardens. Open plan lawns are not appropriate in the Arcadian context.

Materials and detail

The earlier vernacular houses generally have smaller windows that reflect the lower height of the internal spaces. The emphasis on openings is usually vertical, with vertical subdivisions. The Arts and Crafts movement led to a greater focus on horizontal openings.

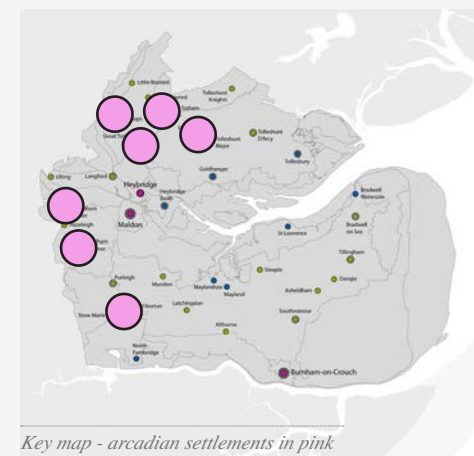
A range of local materials is found which includes: timber frame with render or weatherboard, soft red and gault bricks with imported yellow stock. The use of render with mock applied timbers is used on some of the larger individually designed houses and reinforces the Arts and Crafts and other external influences.



Woodham Mortimer



Barn Conversion, Wickham Bishop



Key map - arcadian settlements in pink

B 05 SITE APPRAISAL

Having identified planning designations relevant to the site and prepared a character study, applicants should then carry out a detailed site appraisal to consider the physical aspects of their site. This could include topography, existing drainage, natural features and access points in order to identify the key constraints and opportunities that may impact on future development.


The aim of the site appraisal is to identify, in spatial terms, those constraints that will impact on the design and the opportunities present to the site. This will inform the understanding and design approach to the site including:

- The context of the site - the building lines of adjoining properties, the size, the height and materials of any adjacent buildings should be noted. Where there are variations, these should be considered as key determinants of the form, massing and layout of the new development.
- The relationship of the site to the wider street scene and the settlement as a whole including (where appropriate) views of the site from open countryside.
- The character and significance of any existing buildings, walls and other structures already on the site. Their potential for reuse as part of the new living accommodation, for storage or garaging or as a means of tying the new buildings into their surroundings should be fully investigated.
- The contribution made by trees or other structural planting or landscape features within or adjoining the site.
- Sustainable development principles – reducing energy use and using renewable sources; choosing 'low impact' and local building materials from sustainable sources; minimising levels of waste arising from development; and prioritising the use of brownfield land.
- The impact and requirements of the Building Regulations, in particular Approved Document B (Fire Safety), Approved Document L - Conservation of Fuel and Power, and Approved Document M - Access to and Use of Buildings, upon the design of new building(s).

It is also recognised that most settlements in all the village categories and particularly within the main towns contain mid-late twentieth century developments that have a weak character. If these developments were to be redeveloped as a whole, then their redevelopment should be guided by the key characteristics and policies of the appropriate section of this Design Guide.

B 06 SITE AND SETTING SUMMARY

Make sure that a site appraisal plan is prepared that considers the wider and local context within which the development is set considering: *What are the features of the site and its surroundings? Can we use them to shape the design? How does it connect with the surroundings and integrate with the settlement?*

 In particular, have you:	Tick when reviewed
<ul style="list-style-type: none"> • Identified all planning designations. 	<input type="checkbox"/>
<ul style="list-style-type: none"> • Considered the character of the site within its settlement and outlined how your design can respond to this character. 	<input type="checkbox"/>
<ul style="list-style-type: none"> • Prepared a detailed site appraisal and established the constraints and opportunities that apply to the site, in particular to: 	<input type="checkbox"/>
Existing networks of natural features including trees, hedgerows, watercourses, ponds, green space, meadows, arable land, habitats and Public Rights of Way (footpaths, bridleways etc).	<input type="checkbox"/>
Any conservation or 'special' areas.	<input type="checkbox"/>
Areas, buildings and/or structures of historical importance including man-made landmarks.	<input type="checkbox"/>
Views into and out of the development site.	<input type="checkbox"/>
Topography and landform.	<input type="checkbox"/>
Land uses adjacent to the site and the effects these may have on the design or treatment of the edges of the development.	<input type="checkbox"/>
Meets all site and setting appraisal requirements of the MDC Local Validation Check List.	<input type="checkbox"/>

At this stage, the applicant should have a full understanding of their site and its settings. This work should inform the design proposals in terms of street layout and connectivity, land use, landscape and townscape character, use of materials, height and massing, and details in a holistic manner.

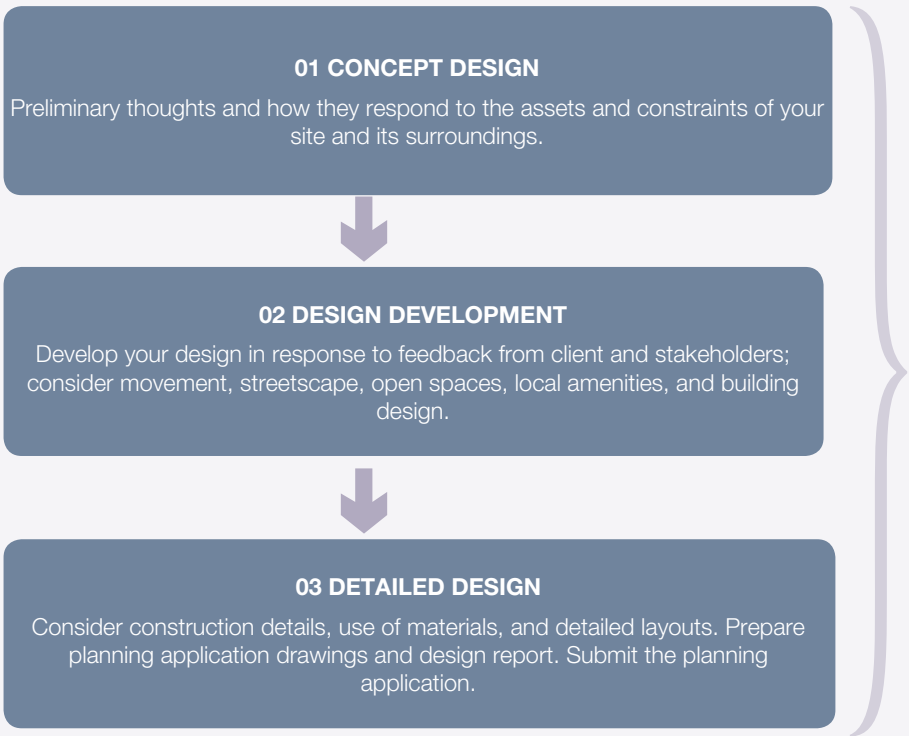
C 01 STARTING THE DESIGN

How to use Part C of the Guide

Once the unique qualities that may apply to your development have been established, this part of the Guide sets out the design principles to follow. Illustrated by the diagram below, Part C is subdivided into 3 sections:

- Landscape and Public Realm,
- Building Design,
- Delivery and Maintenance.

The design principles are illustrated using indicative sketches, which capture some of the most important characteristics of the settlements across the District. The sketches are not meant to be prescriptive but illustrate how development could provide a design response.



How to use this part of the Guide

LANDSCAPE AND PUBLIC REALM

- C02 Using the site's features.
- C03 Creating a network of streets, cycleways, footpaths and access arrangements.
- C04 Creating enclosure and definition.
- C05 Streets where everyone can meet, gather and relax.
- C06 Open spaces.
- C07 Residential outdoor amenity.
- C08 Street furniture, lighting and public art.
- C09 Waste management and utilities.
- C10 Car parking.
- C11 Cycle parking.



BUILDING DESIGN

- C12 Layout and plot size.
- C13 Density and uses.
- C14 Form and massing.
- C15 Corner buildings.
- C16 Inclusive and accessible design.
- C17 Noise and overshadowing.
- C18 Facades and elevations.
- C19 The building edge.
- C20 Materials and details.



DELIVERY AND MAINTENANCE

- C21 Future proofing.
- C22 Delivering high quality development.
- C23 Sense of Place Toolkit.

C 01 STARTING THE DESIGN

Concept Plan

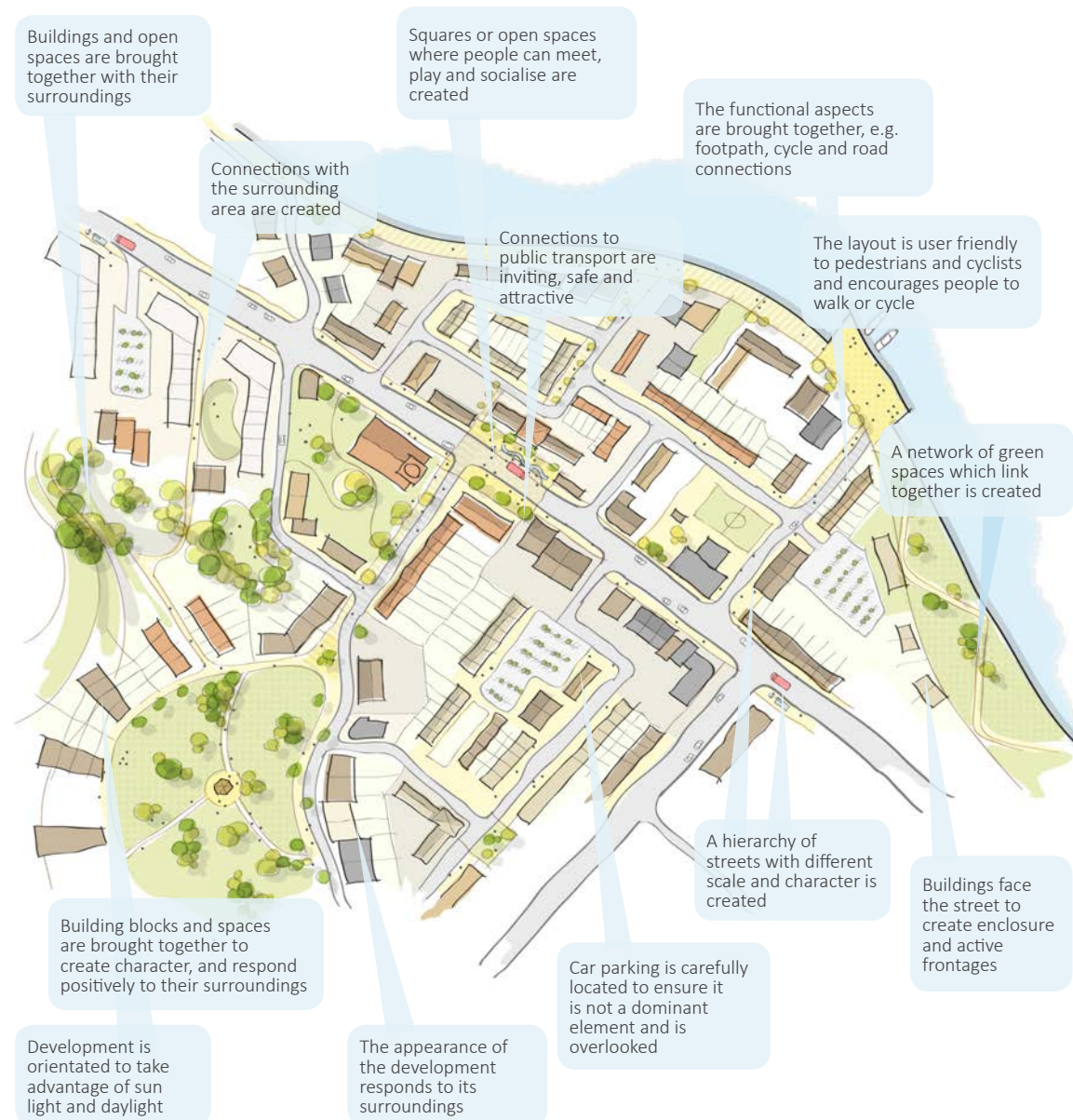
The information gathered should be drawn upon to create a concept plan which responds to any significant constraints on or adjacent to the site and exploiting its opportunities.



Make sure that:

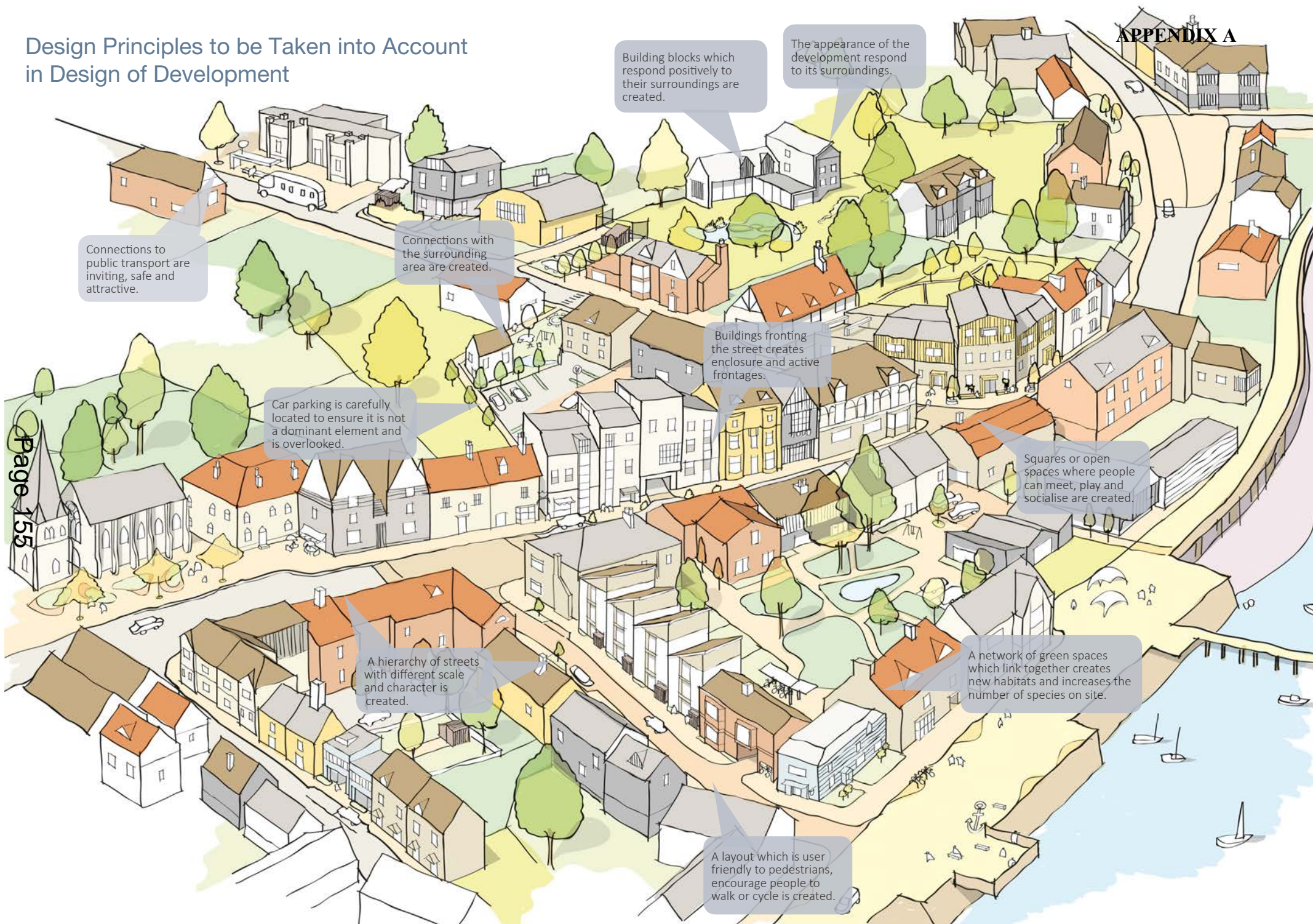
Tick when reviewed

- | | |
|---|--------------------------|
| • A vision or 'picture' of the development is established which brings buildings and open spaces together with their surroundings. | <input type="checkbox"/> |
| • Environmental issues such as air quality, water consumption and quality, drainage, sewerage, energy, noise, light, waste, contamination, design and building materials - are taken into account | <input type="checkbox"/> |
| • The appearance of the development responds to its surroundings, reflects the scale and character of the local area and helps strengthen the character. | <input type="checkbox"/> |
| • The functional aspects are brought together, e.g. footpath, cycle and road connections. | <input type="checkbox"/> |
| • Connections with the surrounding area are created and the development is integrated with its surroundings. | <input type="checkbox"/> |
| • Building blocks and spaces are brought together to create character, and respond positively to their surroundings. | <input type="checkbox"/> |
| • Development is orientated to take advantage of sun light and daylight. | <input type="checkbox"/> |
| • The layout is user friendly to pedestrians and cyclists and encourages people to walk or cycle. | <input type="checkbox"/> |
| • Buildings face the street to create enclosure and active frontages. | <input type="checkbox"/> |
| • A hierarchy of streets with different scale and character is created. | <input type="checkbox"/> |
| • Squares or open spaces where people can meet, play and socialise are created. | <input type="checkbox"/> |
| • A network of green spaces which link together to provide green infrastructure for wildlife and people is created. | <input type="checkbox"/> |
| • Car parking is carefully located to ensure it is not a dominant element and is overlooked, and various solutions are considered. | <input type="checkbox"/> |
| • Connections to public transport are inviting, safe and attractive. | <input type="checkbox"/> |



Design Principles to be Taken into Account in Design of Development


APPENDIX A



C 02 USING THE SITE'S FEATURES

Landscape or Natural Features

Natural assets and physical characteristics such as watercourses, orientation, wind direction, topography, landform, geology, drainage patterns, field patterns, boundaries and vegetation cover, have had a significant influence in shaping the District's settlements. Working with these features can contribute to sustainable development and help to enhance the distinctive local character.

 Make sure that the scheme:	Tick when reviewed
• Is integrated with the local landscape character	<input type="checkbox"/>
• Uses the physical features and topography of the site to best advantage.	<input type="checkbox"/>
• Is designed to maximise the benefits of the site's natural resources.	<input type="checkbox"/>
• Strengthens and retains existing features of biodiversity and ecological value such as hedgerows, ditches and watercourses.	<input type="checkbox"/>
• Has a network of green spaces which connect to the wider landscape and create new habitats to increase biodiversity value.	<input type="checkbox"/>
• Has a joined-up network of open spaces and is located where existing and new residents are able to have easy access.	<input type="checkbox"/>
• Incorporates SuDs as an integral part of the development.	<input type="checkbox"/>
• Reflects natural or cultivated elements e.g. incorporating tree species/ planting characteristics of the area.	<input type="checkbox"/>
• Uses the challenges of air quality, noise and contaminated land sources to inform orientation and location of development.	<input type="checkbox"/>




Further guidance:

- TCA/ Wildlife Trust
- Building for Life 12
- Maldon District's Vehicle Parking Standards SPD (or successor)
- TCPA/Wildlife Trust (2012) Planning for a healthy environment - good practice guidance for green infrastructure

Townscape

Street layout, building, scale and massing, rooflines, windows and door proportions, chimneys, orientation, layout of gardens and land use, all define the townscape.

 Make sure that the scheme relates to:	Tick when reviewed
• The scale, character and pattern of surrounding buildings.	<input type="checkbox"/>
• Conservation Area and Listed Buildings requirements.	<input type="checkbox"/>
• The existing building materials, textures and colour palettes.	<input type="checkbox"/>
• Key views, focal points or landmarks.	<input type="checkbox"/>
• Existing features and amenities.	<input type="checkbox"/>
• The current highway, cycle and footpath network, including public rights of way (PROW) and bridleways.	<input type="checkbox"/>



Building framing the view to waterfront



Building set back to provide distant views

C 03 CREATING A NETWORK OF STREETS, CYCLEWAYS, FOOTPATHS AND ACCESS ARRANGEMENTS

Streets should be designed to feel part of the local area. It should serve many functions, not just to the circulation of traffic, but also walking, cycling, play and places for social interaction and direct connections to public transport and local facilities and services.



Make sure that:

Tick when reviewed

- | | |
|---|--------------------------|
| <ul style="list-style-type: none"> The scheme has a network of streets, cycle ways and footpaths, which connect to each other and where possible provide alternative routes for all users and all modes of transport. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> New streets have a logical order and a street hierarchy which incorporates multi-functional routes. These will be expressed by their width, built enclosure, frontage, parking arrangements, materials and street planting. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> New streets and footways can connect to neighbouring land, which may be developed in the future. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Cul-de-sacs and dead ends are avoided where possible. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Streets, cycle ways and footpaths follow desire lines and use natural features identified at the start to create an inviting, interesting, direct and safe routes for all users. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Streets, cycle ways and footpaths include proposals for hard and soft landscape that reflect the characteristics of the local area. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> All routes provide direct pedestrian and cycle links to local amenities such as shops, schools and health centres. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Routes are attractive to encourage cycling, walking and use of public transport. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Where appropriate, to consider access to PRow and bridleways and potential for multi-user tracks and links | <input type="checkbox"/> |



Further guidance:

- Manual for Streets (DfT)
- Building for Life 12
- Maldon District Vehicle Parking Standards SPD, 2006 (or successor document)



Examples of public realm design providing easy access and movement for all users that encourages walking and cycling

C 04 CREATING ENCLOSURE AND DEFINITION

The definition and enclosure of streets and spaces relates to the height and width of buildings. The distance between buildings should be relative to the width of the street or space in front of them and the buildings on the other side.

This relationship will affect how comfortable or safe a person feels when using the street or space.

The boundaries between public and private spaces can take many forms including planting, hedges, walls and fencing. Sometimes it is appropriate to have a less physical barrier, for example the change from public space to private space could be shown by a change in materials. The choice of boundary treatment should reflect high quality treatments used in the local area.



Make sure that streets and spaces:

Tick when reviewed

- Are defined and enclosed by buildings with a human scale. ☐
- Are fronted by buildings that have ground floor windows from habitable rooms overlooking the street or space - known as natural surveillance. ☐
- Are fronted by main entrances/front doors which provide direct access to the street or space - known as an 'active frontage'. ☐
- Are defined by boundaries that do not limit the amount of overlooking from the adjacent buildings but clearly indicate and/or divide public space from private space. ☐
- Provide residents of adjacent buildings with privacy by providing a sufficient amount of private space between public and/or communal spaces and the adjacent buildings - known as 'defensible space'. ☐

Planting adds to the character of the streetscape and provides an efficient boundary between private and public spaces

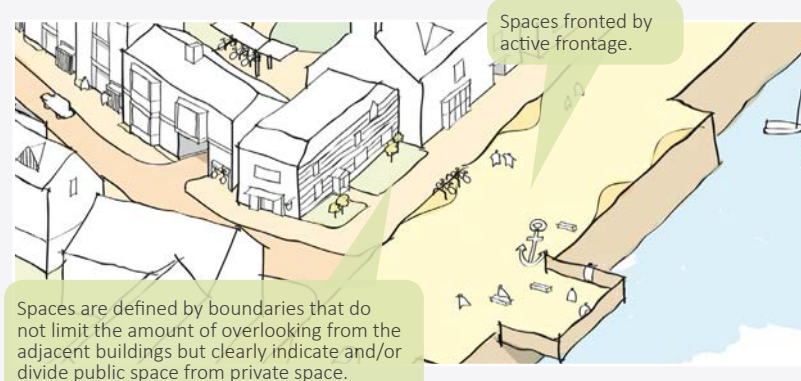


Further guidance:

Local Development Plan - Policy S3 Placemaking

Urban Design Compendium

Building for Life 12 - Creating well defined streets and spaces



Design Principles for Creating Enclosure and Definition

C 05 STREETS WHERE EVERYONE CAN REST, GATHER AND MEET

Streets and spaces are where people rest, gather and meet. They are also often the most permanent features of the built environment. Frequently, an attractive public realm enhances people's quality of life and the perception of an attractive place.



Make sure that streets and spaces:

Tick when reviewed

- | | |
|--|--------------------------|
| • Have been designed as social places providing places for people to rest, gather and meet. | <input type="checkbox"/> |
| • Encourage pedestrian movement by prioritising the needs of pedestrians, cyclists and public transport users over those of motorists. Provide continuous pavements of a sufficient size to meet the needs of all users. | <input type="checkbox"/> |
| • Integrate natural methods of traffic calming within the street design. | <input type="checkbox"/> |
| • Shared surfaces are used on lower order streets and/or local centres, next to public spaces, or other appropriate locations. The use of shared surfaces does not confuse the order of streets. | <input type="checkbox"/> |
| • Are convenient, safe and easy for all to use, including wheelchair users. The amount of street furniture has been kept to a minimum. | <input type="checkbox"/> |
| • Include trees and soft landscaping (on all street types) that reflect the order of the street. The species provided are appropriate to the environment and their location, both at the time of planting and maturity - applicants should seek advice from appropriate specialists. | <input type="checkbox"/> |



Further guidance:

- Appendix 1 - Technical Document; Landscape and Green Infrastructure
- Local Development Plan - Policy S3 Placemaking
- Manual for Streets (DfT)
- Building for Life 12 - Streets for all



Design Principles for Streets and Spaces

C 06 OPEN SPACES

All development must contribute towards improving the provision, quality and/or accessibility of local and strategic open space, sports, community and leisure facilities. This could be achieved through appropriate contribution or direct provision. Where direct provision is made open spaces should form part of an overall green infrastructure network and must make a positive contribution towards the townscape. Open spaces should be high quality and have a primary role or function to prevent them becoming unused or neglected. The local context should be reflected in the design of local open spaces, which could be achieved through the use of materials, trees, planting, lighting and street furniture.

✓ Make sure that open space:

Tick when reviewed

- | | |
|---|--------------------------|
| • Uses the natural features identified at the start of the design process as focal points. | <input type="checkbox"/> |
| • Integrates into the wider landscape of the scheme and is located so that residents can access it easily and directly. | <input type="checkbox"/> |
| • Links with existing spaces to form green routes/networks. | <input type="checkbox"/> |
| • Is not pushed to the edge of the scheme. | <input type="checkbox"/> |
| • Is of an appropriate size, shape and layout to meet the needs of the scheme's users. | <input type="checkbox"/> |
| • Is appropriately defined and enclosed by buildings with windows on the ground floor from habitable rooms overlooking it where appropriate | <input type="checkbox"/> |



Example of community space with water feature integrated with SuDss

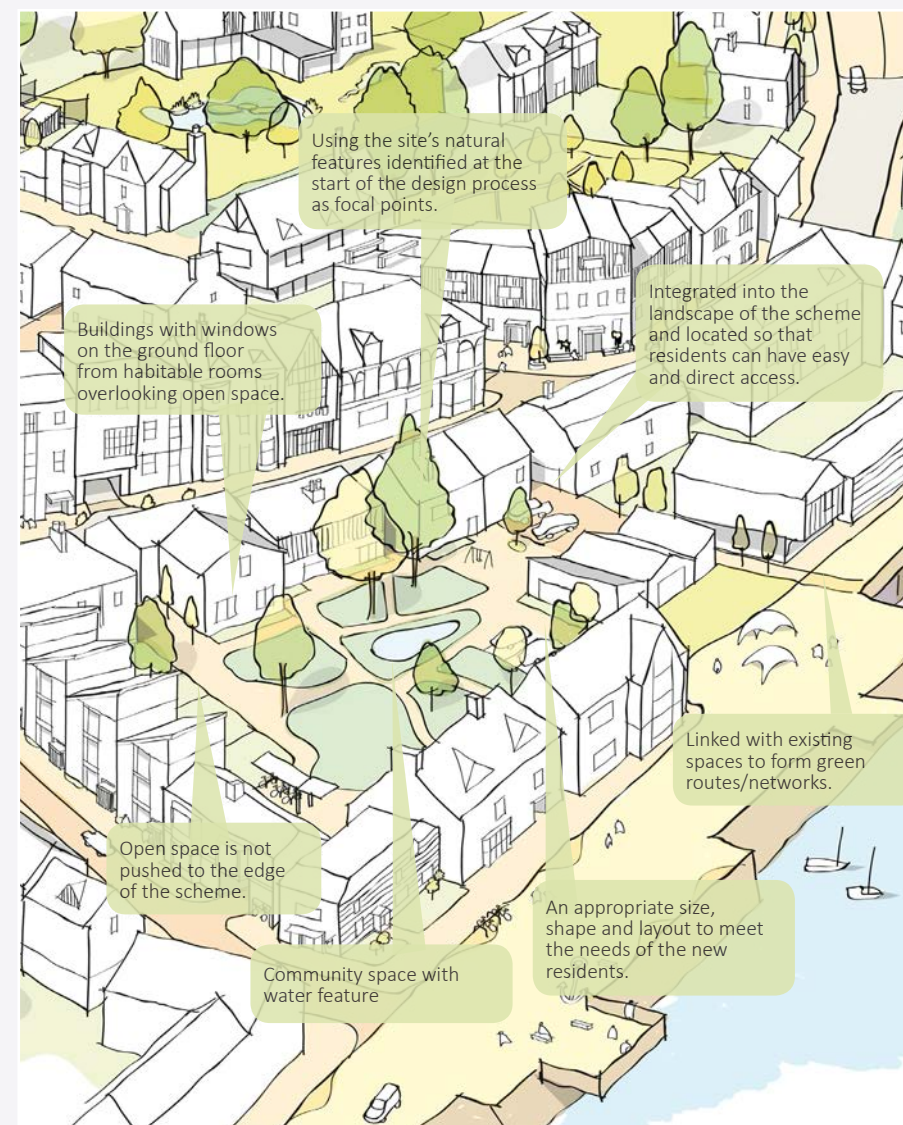


Example of pocket green space with playground



Further guidance:

- Local Development Plan - Policy S3 Placemaking
- Urban Design Compendium Landscape and Thriving Public Realm
- Building for Life 12 - Public and Private Spaces
- Landscape Institute (2014) Profitable Places
- Design Council (2009) - Design and Planning for Play
- Appendix 1 - Technical Documents- Landscape and Green Infrastructure



Design Principles for Open Space

C 07 RESIDENTIAL OUTDOOR AMENITY

Whether residential development is private or affordable, they should be indivisible, and should be planned around safe and usable spaces. Usable outdoor amenity could be in the form of private or communal gardens, balconies, children's play areas, allotments, or public spaces.

Depending on the scale of the development and the quality and quantity of existing provision in the wider area, it may be necessary to incorporate an element of outdoor amenity for residents into your development. Generally, provision should be made that fulfils a number of functions: hanging out washing, sitting out, having a kick around, providing habitats for wildlife and allowing water to drain naturally.

Private Amenity Space i.e. Gardens and Balconies - Private garden land is the enclosed area within a dwelling curtilage from which the public is excluded. Private gardens should contribute towards the leafy, green character of the local area. They provide a function that may not be interchangeable with the offer of public open space. Additionally, they may provide residents with the opportunity to play and grow food. Balconies must be usable, functional and practical in the same way as private gardens.

Communal Gardens and Amenity Space - In contemporary flat developments, it is essential to provide an element of communal outdoor amenity space to complement the lower levels of private outdoor space. These spaces should be safe, usable, designed to a high standard and well managed so that the space remains high quality.



Make sure that:

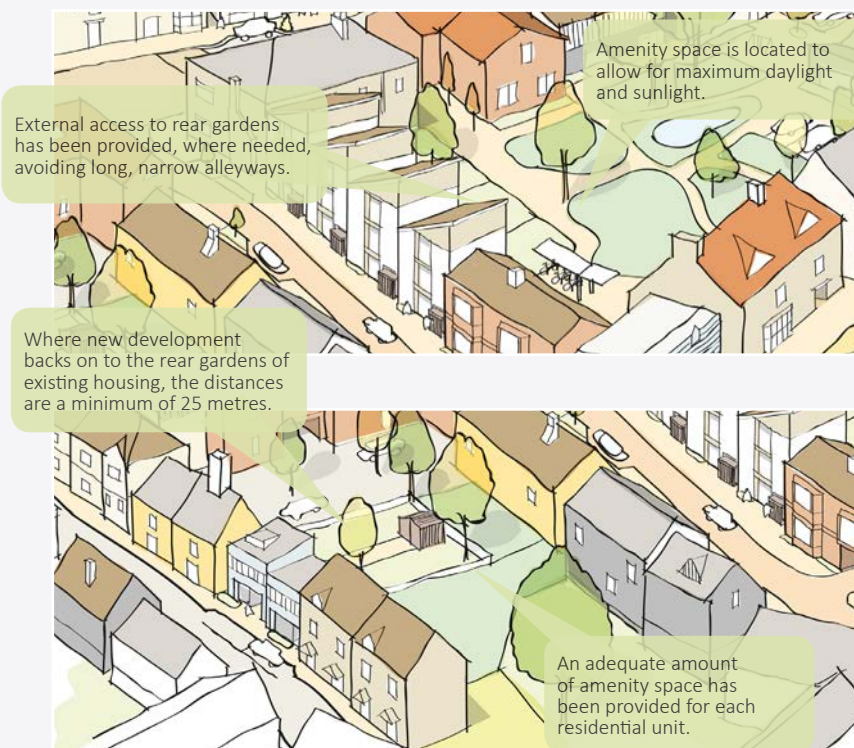
- An adequate amount of amenity space has been provided for each dwelling. The overall size of the garden is in proportion with the type of dwelling and the character of the area. The minimum standards set out in the Essex Design Guide are achieved: for houses 3+ bedroom = 100 m² per unit; 1-2 bedroom = 50 m² per unit; for flats = 25 m² per unit.
- External access to rear gardens has been provided avoiding long, narrow alleyways.
- Amenity spaces have not been compromised by the location of parking areas, garages and refuse storage areas.
- Amenity space is located to allow for maximum daylight and sunlight.
- Topography is taken into account and imaginative solutions are used to respond to it.
- Where new development backs on to the rear gardens of existing housing, the distances between buildings are a minimum of 25 metres.



Further guidance:

- Appendix 1 - Technical Documents- Noise and Air Quality

Tick when reviewed

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Design Principles for Residential Outdoor Amenity

C 08 STREET FURNITURE, LIGHTING AND PUBLIC ART

The design and location of street furniture should be simple, high quality, well designed, robust, and responsive to its setting. They should be considered in an integrated way into the design of the landscape. Street furniture should be restricted to essential items, and where possible functions should be combined, for example attaching signs to lamp posts, mounting street signs, or lighting on buildings.

The changes in level should be considered when designing for planting and street trees to ensure that they are integrated into the public realm to minimise the need for bollards.

Schemes with lighting that are well considered and reflective of the area are essential to the creation of safe, high quality streets and spaces.

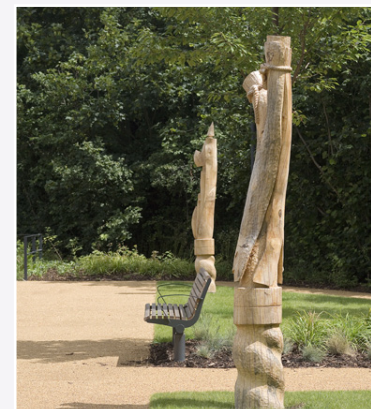
Public art can play a significant part in the character of the public realm. It helps to create distinctive places as well as forming legible features. It can be delivered in varied forms, but should be designed for a specific location in the landscape or public realm.

Make sure that:

	Tick when reviewed
• The number of elements such as light columns, sign poles, seating, is kept to a minimum. This helps to avoid clutter.	<input type="checkbox"/>
• There is a relationship between the individual items of street furniture.	<input type="checkbox"/>
• Where possible their functions are combined.	<input type="checkbox"/>
• The use of bollards is kept to a minimum.	<input type="checkbox"/>
• Lighting is used to create safe, inviting routes and spaces, but avoids over-lighting particularly in sensitive and dark rural areas.	<input type="checkbox"/>
• Where public art is used, it is carefully integrated into the public realm and provision is made for its maintenance.	<input type="checkbox"/>
• All elements are attractive, robust, durable and easy to maintain.	<input type="checkbox"/>

Further guidance:

- Local Development Plan - Policy S3 Placemaking
- Manual for Streets
- Guidance for the reduction of obtrusive light (ILE)



Benches, trees and art features help guide direction and avoid the use of too many bollards




Carefully integrated lighting will create safe, attractive and usable public spaces in conjunction with the location of other furniture and planting. Light fittings should minimise light spill and excess light distribution into adjacent properties and the landscape

C 09 WASTE MANAGEMENT AND UTILITIES

The provision of waste management facilities within developments is fundamental to provide and maintain an attractive and healthy environment. In some instances, it may be more practical to provide these facilities where it would result in a better visual appearance to the streetscene rather than as a cluster.

For utilities, whilst they generally run under ground, they have an impact on where trees can be planted and above ground supply boxes can be unsightly. The provision and location of utility requirements should be considered at an early stage to minimise potential conflict and reduce their impact.

 Make sure that:	Tick when reviewed
<ul style="list-style-type: none">Sufficient space has been provided for store bins and containers. The storage areas are convenient for residents and are attractive where they can be seen from streets and spaces.	<input type="checkbox"/>
<ul style="list-style-type: none">Access has been provided between bin storage areas and collection vehicle access. Long path/alley ways between rear gardens and the street have been avoided	<input type="checkbox"/>
<ul style="list-style-type: none">Convenient access has been provided for service vehicles that avoids the need for them to frequently turn around and gives priority to through routes.	<input type="checkbox"/>
<ul style="list-style-type: none">Utility boxes, cable runs and maintenance access points have been integrated positively into the scheme and do not conflict with landscape features, tree planting and/or the design of the public realm.	<input type="checkbox"/>

Examples of the successful integration of covers, manholes, and other utilities within the design of public spaces will contribute to the overall impression of the quality of the public realm.



Design Principles for Waste Management and Utilities

C 10 CAR PARKING

The provision of parking is a significant design challenge in the built environment. If poorly designed it can have a significant negative impact on the appearance of streets and spaces. Car parks should include tree planting at regular intervals

Designing streets so that they can accommodate on-street visitor parking can really benefit residents by reducing the likelihood of anti-social parking. Visitors' parking spaces are needed most where residents have a limited number of allocated parking spaces, especially where these are located in-front of properties or in rear parking courts. This could be locating visitors' parking spaces next to open spaces and local facilities, for example.

The suitability of parking solutions will vary depending on the location and nature of the proposal. For example, parking on driveways in larger developments or developments in urban locations should be avoided. However, in rural locations, parking on driveways could be an acceptable solution provided accesses are kept clear.

Page 164

**Make sure that:**

Tick when reviewed

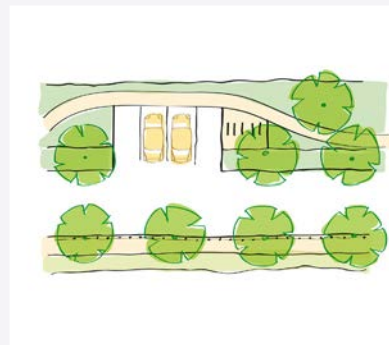
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|---|--------------------------|
| • All parking solutions and accommodation for trees are thoroughly considered early in the design process. | <input type="checkbox"/> |
| • A variety of parking solutions are used to form part of the overall street design. | <input type="checkbox"/> |
| • Adequate visitor parking spaces are located throughout the development, and are easy to recognise. | <input type="checkbox"/> |
| • Garages and car ports are carefully integrated within building frontages and do not break up the enclosure or the definition of the street. | <input type="checkbox"/> |
| • Rear parking courts are provided when all on-street options have been exhausted, ensuring they are small in scale and overlooked and avoid large expanse of tarmac. | <input type="checkbox"/> |
| • Where practical, housing is designed to enable the installation of a domestic electric vehicle charging point to the approved industry standard. | <input type="checkbox"/> |
| • Parking in front of dwellings are minimised if possible, or do not use up all front gardens, and avoid extensive areas of hard surfacing. | <input type="checkbox"/> |

+ Further guidance:

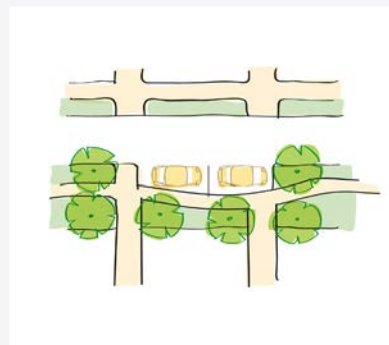
- Maldon District Vehicle Parking Standards SPD, 2006 (or successor document)
- Local Development Plan - Policy S3 Placemaking
- Trees in Hard Landscapes: A Guide for Delivery, Trees and Design Action Group, 2014



Dedicated parking within a central space



Perpendicular parking



Parallel parking



Electric charging point



Examples of carefully integrated car parking



Examples of carefully integrated car parking

C 11 CYCLE PARKING

For cycling to become an alternative to the car, bicycles must be readily accessible with secure parking.

The type of storage will depend on the nature and scale of development. If cycle storage is conveniently located i.e. close to entrances, cyclists are more encouraged to use them.



Make sure that:

- Cycle storage is integral to the design of the scheme and easily accessible and secure to encourage its use.
- Cycle storage is provided externally, sited sensitively, well screened and is made from durable materials appropriate to their setting.
- Cycle storage is not projected forward of the established building line.
- Dedicated visitor cycle spaces are provided close to the main entrances and located in areas that are well overlooked by habitable rooms. The Council will require at least 1 cycle space per 8 units for visitors.

Tick when reviewed

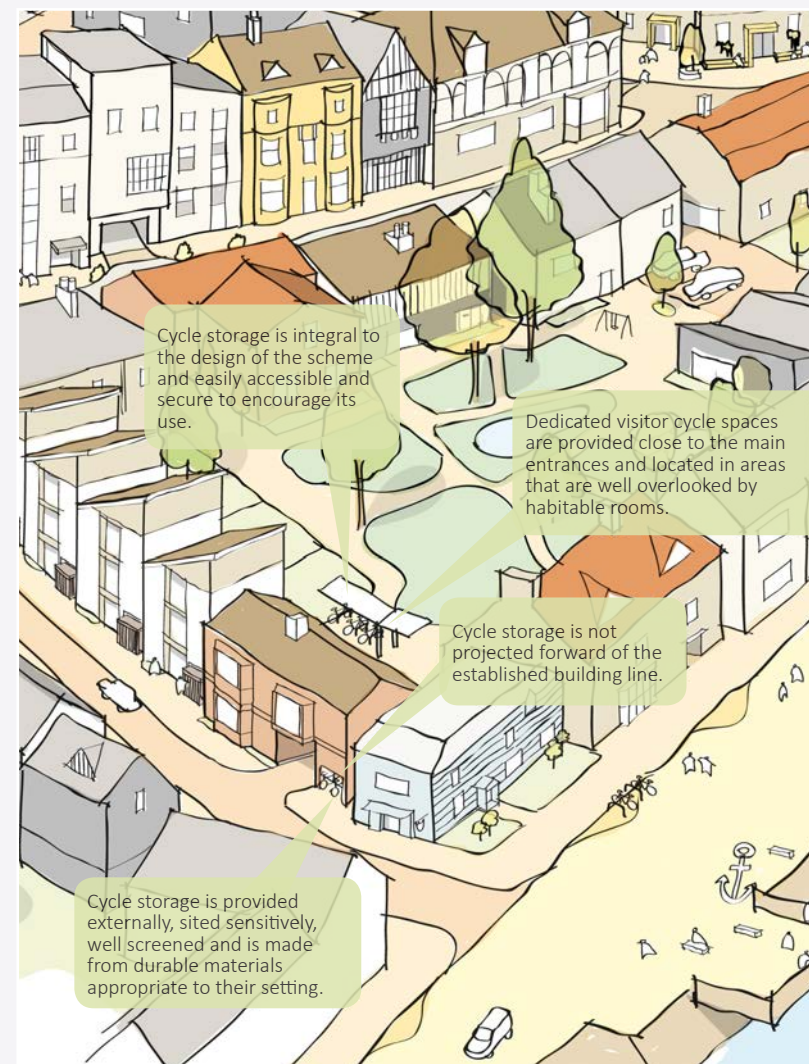
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Examples of carefully integrated cycle parking



Further guidance:

- Maldon District Vehicle Parking Standards SPD, 2006 (or successor document)
- Local Development Plan - Policy S3 Placemaking



Design Principles for Cycle Parking

C 12 LAYOUT AND PLOT SIZE

New development should respond to the existing pattern of development within a settlement, taking cues from existing block sizes, patterns of plot subdivision, and relationship between the built and non-built private space. This approach will help to integrate new development within existing settlements as a natural extension. These cues should be drawn from the Character Study carried out as part of 'Responding to the Site and Setting' and responding to planning policies in the Local Plan.

Full account of context should be taken and use of standard designs should be avoided.



Make sure that:

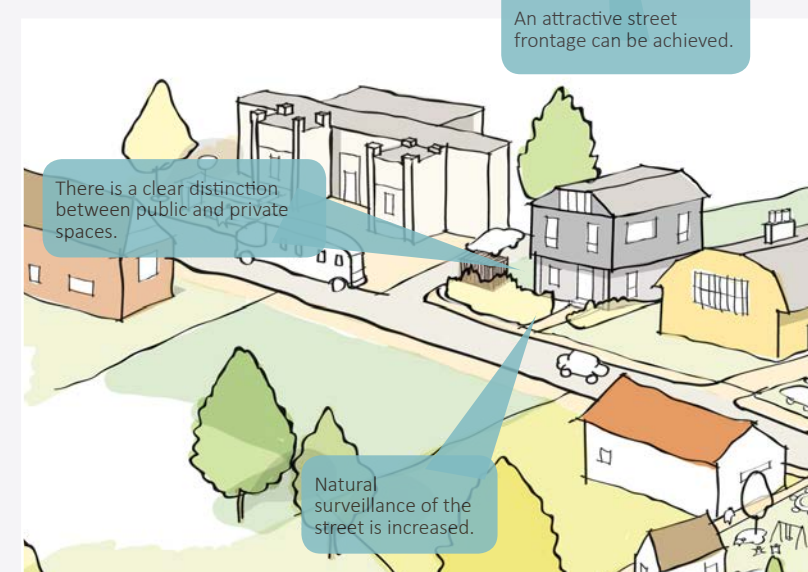
- Development ensures the efficient use of land.
- Connections to surrounding areas are made.
- There is a clear distinction between public and private spaces.
- Natural surveillance of the street is increased.
- An attractive and active street frontage can be achieved.
- Building frontage/plot width responds to context with narrower frontages located in town and neighbourhood centres and along the waterfront.
- Block sizes are flexible and suitable for a range of uses

Tick when reviewed

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Further guidance:

- Local Development Plan - Policy S3 Placemaking
- Manual for Streets (DfT)
- Building for Life 12 - Streets for all
- Appendix 1 - Technical Documents - National Space Standards




Design Principles for Layout and Plot Size

C 13 DENSITY AND USES

While it is important to ensure best use of land in an efficient and cost effective manner, density should be appropriate to the location, respond to and/or enhance the character of the existing settlement and context.

Typically, densities decreases the further from the centre of a settlement. Lower densities may be more appropriate in Agricultural or Arcadian settlements, and edge of settlement sites. However, a higher density may be more appropriate for the Main Towns, or areas where they have good access to public transport, services and facilities, in the interest of creating sustainable development.

 Make sure that:	Tick when reviewed
<ul style="list-style-type: none">Density is appropriate to the location.	<input type="checkbox"/>
<ul style="list-style-type: none">It responds to and/or enhances the character of the existing settlement.	<input type="checkbox"/>
<ul style="list-style-type: none">A range of densities in large developments is provided.	<input type="checkbox"/>
<ul style="list-style-type: none">Higher densities are focused around key movement nodes, along strategic routes, and within neighbourhood, local and village centres.	<input type="checkbox"/>

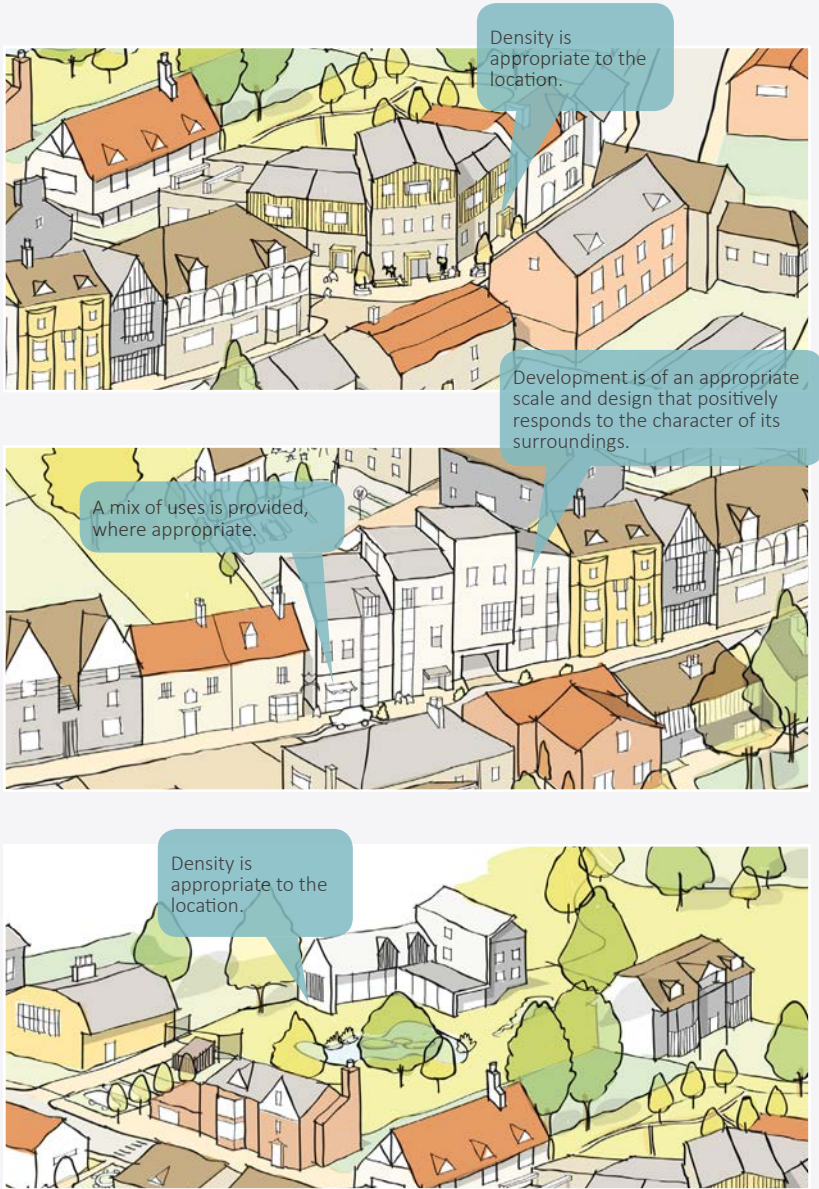


Examples of densities appropriate to the location



Further guidance:
Local Development Plan - Policy H4: Effective Use of Land, Landscape and Visual Impact Assessment (2010), Maldon District Characterisation Assessment (2006), Maldon District Historic Environment Characterisation Project (2008) and Maldon Landscape Character Assessment (2006).

Appendix 1 - Technical Documents - How to Measure Net Density.



Design Principles for Density and Uses

C 14 FORM AND MASSING

The form and massing of development can have a significant contribution to the character of an area. The majority of traditional buildings in the District either in urban or rural locations adopt a very consistent, simple form, with regular floor plans, and pitched roofs. New development should create a positive character, with its own identity that relates to the characteristics of the settlement and the opportunities or constraints for innovative design.



Make sure that:

Tick when reviewed

- | | |
|---|--------------------------|
| • Development has an identity that respects or responds to the characteristics of the settlement and opportunities/constraints identified. | <input type="checkbox"/> |
| • The design of the buildings relate to the form, height and proportions of buildings in the local area. | <input type="checkbox"/> |
| • The buildings adopt a simple form, including the form of the roof, using proportions that are relevant to the order or hierarchy of the street. | <input type="checkbox"/> |
| • Building elevations relate to and take cues from existing well designed buildings in the local area. The arrangement of windows and openings is simple and aligned. | <input type="checkbox"/> |
| • The scheme incorporates features, such as chimneys and dormer windows where they are predominant in the local area. Where included, they must form an integral part of the building function. | <input type="checkbox"/> |
| • The design emphasises the character of the local area. | <input type="checkbox"/> |
| • The design provides variation in form, scale and massing. | <input type="checkbox"/> |



Traditional, Chandlers Quay, Maldon

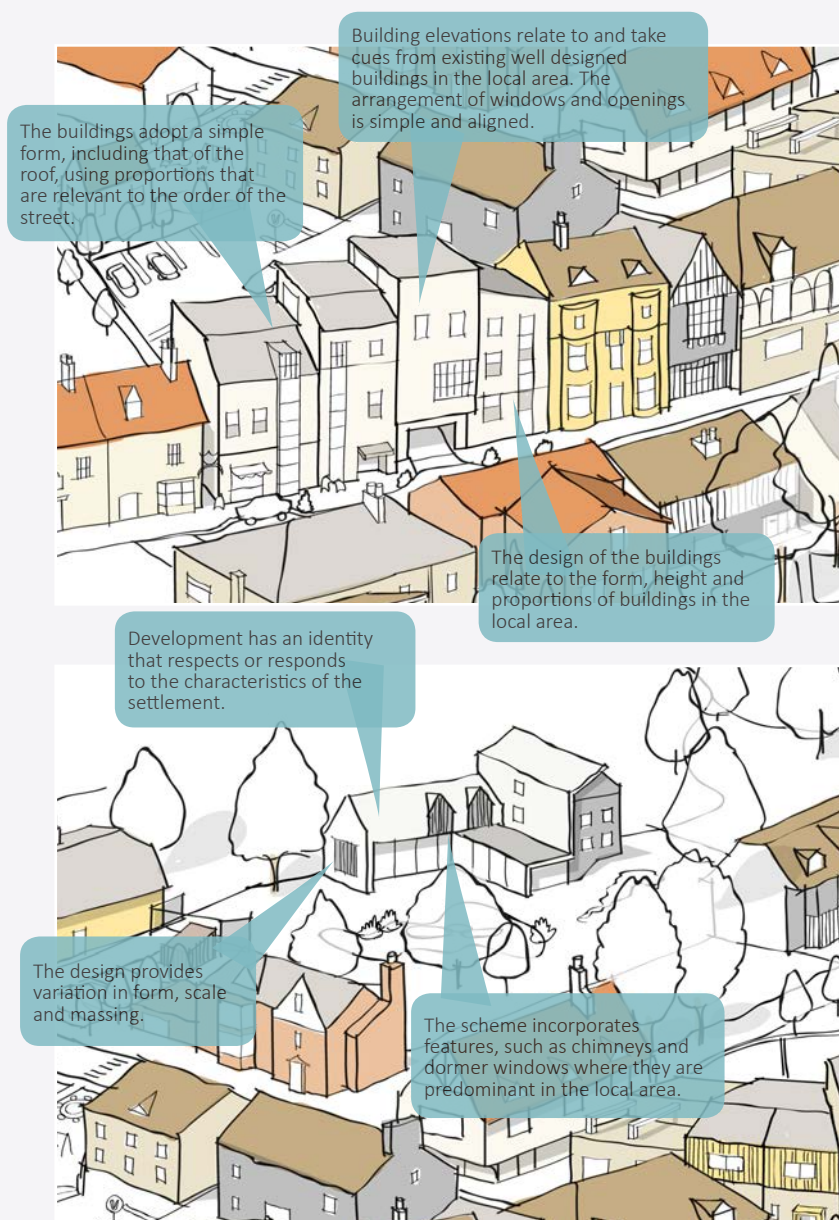


Contemporary, Salt House, St Lawrence



Further guidance:

- Building for Life - Character



Design Principles for Form and Massing

C 15 CORNER BUILDINGS

Corner sites are visually prominent and may provide an opportunity to accommodate non-residential uses to aid legibility of a place, or to contribute to its character through distinctive designs or increased building height.

Standard house types are unlikely to work on corner locations and local centres.



Make sure that:

- Buildings have been designed to define the corner space of a block and ensure the continuity of the street/space and building frontage.
- Corner buildings provide activity and overlooking onto both streets/spaces by providing natural surveillance from ground floor windows on both of the facades facing the street and/or space.
- Where garages and carports have been integrated within the frontage of the building, they do not create blank frontages to streets and spaces.

Tick when reviewed

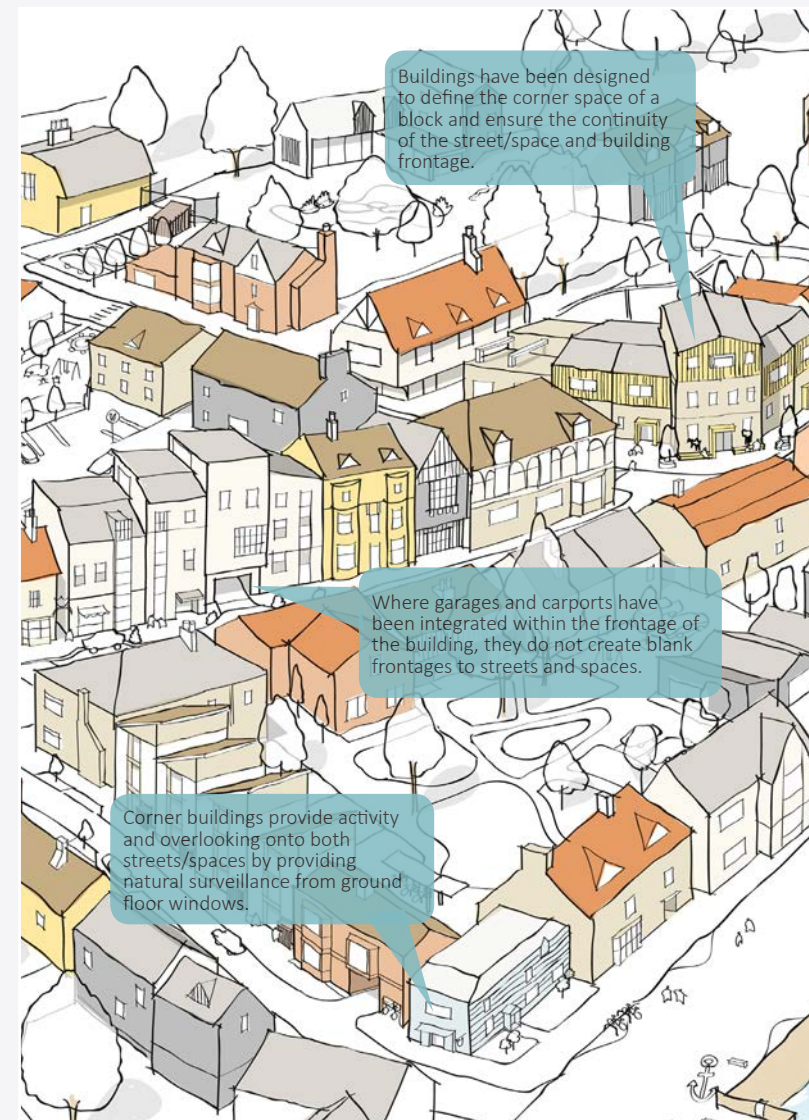
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Corner treatment



Further guidance:

- Building for Life - Creating well defined streets and spaces



Design Principles for Corner Buildings

C 16 INCLUSIVE AND ACCESSIBLE DESIGN

Inclusive design is about making places everyone can use.

Residential development must address the needs of everyone regardless of age, gender, mobility, ethnicity or circumstances especially the most vulnerable, i.e. those with pushchairs, people with disabilities and the elderly.

Inclusive design also means promoting high levels of social inclusion. A residential development that is not only functional but also aesthetically pleasing will offer good value to residents and reassure those who may be reluctant to leave their homes, i.e. the elderly.



Make sure that:

Tick when reviewed

- | | |
|---|--------------------------|
| <ul style="list-style-type: none"> The needs of everyone are taken into account when designing access to and from dwellings; open spaces, and other landscape features; bin and cycle stores; parking spaces. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Current building regulations with respect to accessibility is complied with. These include: means of access to and into a dwelling; circulation within the entrance storey of a dwelling; accessible switches and sockets in the dwelling; WC provision on the entrance storey of a dwelling; and passenger lifts and common stairs in blocks of flats. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Individual dwellings are designed to be flexible, capable of adaptation to meet the changing needs of residents in the future - such as needing to adapt a home to reflect a loss of mobility, or adapting a home to allow home working. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Where appropriate all new dwellings are designed and built to Lifetime Home Standards and are wheelchair accessible or easily adaptable for residents who are wheelchair users. | <input type="checkbox"/> |
| <ul style="list-style-type: none"> Affordable housing is provided where developments provide 5 or more homes, or comprise an area of 0.5 ha or larger. The quality of affordable should be as good, if not better, than that of market housing, including how it looks aesthetically to encourage social inclusion and community cohesion. | <input type="checkbox"/> |

Further guidance:



- Building for Life - Character
- Appendix 1 - Technical Documents - National Space Standards
- Appendix 1 - Technical Documents - Designing for Older People Housing



Friary Fields, Maldon, a scheme of 32 homes for social housing was sensitively designed to sit comfortably in its historic context



Addressing the needs of everyone



Spaces everyone can use



Means of access to and into a dwelling - level access or DDA compliant ramps

C 17 NOISE AND OVERSHADOWING

Noise can be a significant source of aggravation for residents. Issues associated with noise are prevalent in locations close to external sources of noise such as railway lines and busy main roads. Busy roads can also affect air quality and the usability of outdoor areas. Careful design can help to reduce the impact of noise.

Buildings close to the boundary of neighbouring properties can increase overshadowing or loss of daylight to neighbouring properties. Habitable room windows should normally be at least 12 metres away from the flank wall of the neighbouring property.

Care should be taken to avoid areas which are permanently in shade, overshadowed by adjacent buildings.



Make sure that:

- Buildings are orientated to that habitable rooms and amenity space do not face noise sources.
- Design features such as recessed balconies are introduced.
- Barriers such as garages or walls are located between noise sources and dwellings.
- Noisy external activities such as play areas are located close to properties they serve, but far enough away to avoid noise disturbance.
- The relationship of buildings does not cause overshadowing.

Tick when reviewed



Garages are sensitively located



Recessed balconies



Roof overhangs providing protection to glazed facade



Further guidance:

- Local Development Plan and SPDs
- Building for Life - Character
- Appendix 1 - Technical Documents - Noise and Air Quality

C 18 FACADES AND ELEVATIONS

Existing buildings within the locality should be the starting point for the consideration of facade design and elevational treatment for new buildings. Generally, this should interpret key aspects of their facade and elevations through their layout, window to wall ratio and proportions, and placement of windows and doors.

The District has a wide range of architectural styles and the arrangement of facades varies from settlement to settlement. However, building facades are generally organised with windows and doors aligned horizontally and vertically.



Make sure that:

- An architectural approach is established and an identity has been informed by the surrounding area of building facades and elevations.
- The relationship between the existing and the new facades and elevations are demonstrated in the Design and Access Statement.
- Keep it simple! Align windows and doors horizontally and vertically.

Tick when reviewed



Examples of different approaches to facade design

C 19 THE BUILDING EDGE

The edge of a development and its external appearance are critical aspects for urban extensions and new development at the edge of existing settlements. At times, the edge is poorly defined by wooden fences abutting the countryside or buildings presenting hard built edges to open countryside.

Development should provide an edge which has a clear and well defined external identity. The nature of this edge will depend on the location. However, development abutting countryside should have soft edges, created with appropriate green infrastructure or structural landscaping.



Make sure that:

Tick when reviewed

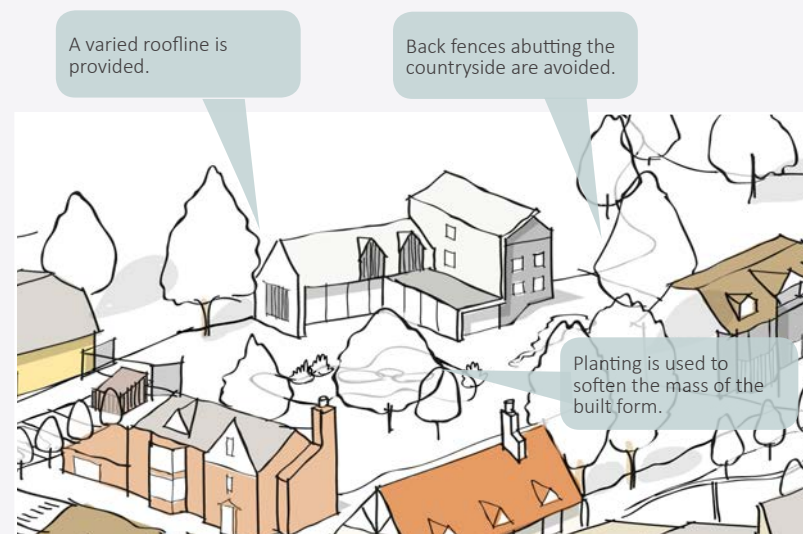
- | | |
|---|--------------------------|
| • A strong building frontage is created. | <input type="checkbox"/> |
| • Structural planting and boundary treatment such as: stone walls are combined. | <input type="checkbox"/> |
| • Planting is used to soften the mass of the built form. | <input type="checkbox"/> |
| • Entrances are clearly defined through the use of pinch points, corner or feature buildings. | <input type="checkbox"/> |
| • Views to important landmarks and/or key buildings are maintained. | <input type="checkbox"/> |
| • A varied skyline and roovescape is provided. | <input type="checkbox"/> |
| • Back fences abutting the countryside are avoided and green infrastructure and structural landscaping is used to create soft edge. | <input type="checkbox"/> |



Defining edges

+ Further guidance:

- Appendix 1 - Technical Documents - Landscape and Green Infrastructure



Design Principles for the Building Edge

C 20 MATERIALS AND DETAILS

Whilst the architectural style varies across the District, a common characteristic of buildings is a simple palette of materials, detailing and architecture features to the design.

New development should take cues from the choice of materials and architectural features from the local context or reinterpret it, in a contemporary manner using high quality materials and detailing.

Depending where the development is, the Council has a suite of adopted guidance to inform the design for character areas within Strategic Design Codes and Strategic Masterplan Frameworks.



Make sure that:

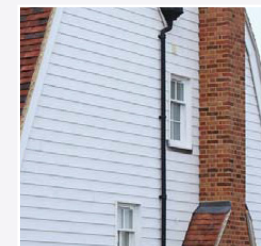
Tick when reviewed

- | | |
|---|--------------------------|
| • A simple palette of robust, and local building materials has been proposed that relates to the to the most commonly used materials in the local area. | <input type="checkbox"/> |
| • Natural and sustainable materials and reconstituted materials are considered (if reconstituted materials are of comparable quality of natural materials) | <input type="checkbox"/> |
| • Architectural detailing has been added to the buildings that relate to the detailing used on buildings in the local area - helping to add richness and visual interest. | <input type="checkbox"/> |
| • The paving and surfacing materials proposed for footpaths, cycle ways and vehicle highways are robust and durable for the street type proposed. | <input type="checkbox"/> |
| • The materials used for on-street parking spaces relate to the materials used on the adjacent highway. | <input type="checkbox"/> |
| • Shared surface areas are laid out in one consistent material used for both the highway and parking areas with parking spaces carefully defined. | <input type="checkbox"/> |
| • The type of boundary treatment and materials used reinforce character. | <input type="checkbox"/> |

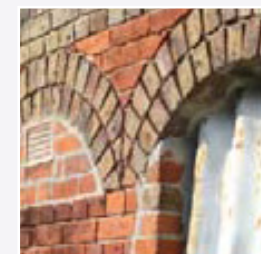
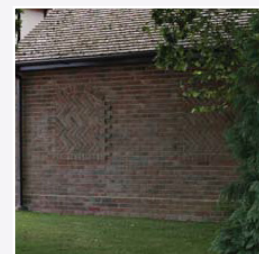


Further guidance:

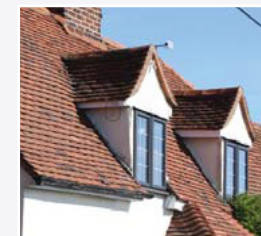
- Building for Life - Character
- Appendix 1 - Essex Design Guide - Road Type Table
- BRE Green Guide - How to Minimise Carbon Footprint



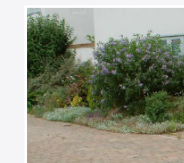
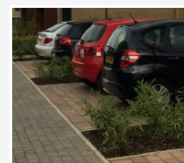
Weatherboard - a classic building material used throughout East Anglia in particular Essex. This style has been adopted for many new houses which helps them to blend in with older buildings in the area



Classic red and decorative brick used. Later buildings of the 60s and 70s use a mix of buff and red brick with little change in course of pattern or natural colour variation



Variety of roof tiles used throughout the District, including traditional Clay Peg tiles and variations, red clay pan tile and prefabricated concrete tiles, as well as natural slate tiles



Car parking surface materials and boundary treatments

C 21 FUTURE PROOFING

It is important to mitigate the effects of climate change in hotter and colder weather patterns but also to reduce energy consumption and consider recycling.



Make sure that:

Tick when reviewed

- Building design takes advantage of orientation.
- Building depths are limited wherever possible to maximise natural lighting levels and natural ventilation.
- Layouts avoid single-aspect dwellings which may cause homes to overheat if south-facing or create additional heating demands if north-facing.
- South-facing windows maximise natural daylight.
- North-facing facades seek to minimise large areas of glazing to prevent unnecessary heat loss in winter.
- Shading is provided to south-facing windows to prevent overheating in the summer months, such as a deciduous tree adjacent to the property.
- The use of green roofs are considered and are appropriate within the context of the site.
- The use of low-embedded energy or materials that can be recycled is used, where appropriate.
- The use of materials with high thermal mass are used where appropriate.
- Existing and proposed green infrastructure, landscape features and SuDs are incorporated to mitigate climate change



Further guidance:

- Building for Life - Character
- Building Regulations - Approved Document M; access to and use of buildings, Approved Document B (fire safety); Approved Document L Conservation of fuel and power.

Designing homes to reflect and adapt to changing climate represents a priority at national and local levels. The significant heritage context of Maldon District should not be in a conflict with good sustainable design and good urban design. The introduction of sustainable design and construction techniques in Conservation Areas and on/or close proximity to Listed Buildings can potentially enhance these heritage assets.



Make sure that:

Tick when reviewed

- You have checked the building is listed or in conservation area.
- Energy efficient solutions are integrated as part of the development.
- Any solar panels that are positioned on building surfaces are facing within 90 degrees south and not overshadowed. Ensure that the roofs are strong enough to hold the panels.
- The design allows for system maintenance.
- All water fittings i.e. taps and showers are specified and installed are recognised low flow technology.
- Low or dual flush WCs are used.
- The opportunity for rainwater harvesting or grey water recycling is maximised, where possible.



Solar panels and rainwater harvesting

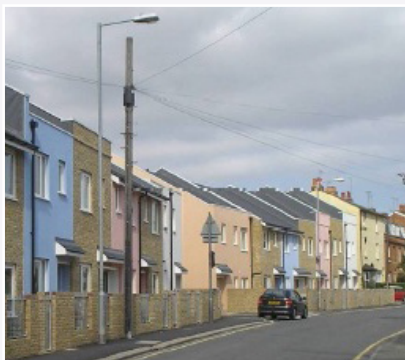
C 22 DELIVERING HIGH QUALITY DEVELOPMENT

From design through to on-site construction and maintenance, all development must be delivered to the same standard for which it was designed.

**Make sure that:**

Tick when reviewed

- | | |
|--|--------------------------|
| • New development considers long term maintenance and management from the outset. | <input type="checkbox"/> |
| • Buildings are constructed and completed as detailed on the drawings approved. | <input type="checkbox"/> |
| • A simple palette of materials is used that are durable and robust, and weather well over time. | <input type="checkbox"/> |
| • Where value engineering is used, it is to resolve detailing and seeks improvements to the construction of a building. The use of Non-Material Amendment and/or Minor Material Amendment are at the discretion of the Council and will not be accepted where it is clear that a reduction in quality or material deviation from the approved plans is proposed. | <input type="checkbox"/> |
| • Arrangements are in place for future management of amenity and open spaces. | <input type="checkbox"/> |



Charter House – Winner of Maldon District Conservation & Design Award 2009 for Design



Extension to historic building – Winner of Maldon District Conservation and Design Award 2011 for Design

C 23 SENSE OF PLACE TOOLKIT

Sense of Place

The Maldon District 'Sense of Place' is a vision which articulates the place and its attractiveness, and defines who the place is for, how it is special, where it has come from and where it is going in the future.

Sense of Place Toolkit

The Sense of Place Toolkit seeks to celebrate Maldon as a special and unique place and includes themes, photography, graphic languages and colour palette, all inspired and drawn from the District. The Sense of Place assets enable businesses and stakeholders to show they support and are part of the local area. The assets can be used across a range of different media, public realm materials and advertising and advertisements giving visitors and residents a sense of visual identity and influence their feeling about what it is like to be in Maldon District.



Maldon District Toolkit - Sense of Place Brand

**Further guidance:**

- Sense of Place Toolkit: <http://www.visitmaldondistrict.co.uk/Business.aspx>

Acronyms and Glossary

Accessible - When people are able to move around an area and reach places and facilities, including the elderly and disabled, those with young children and those encumbered with luggage or shopping.

Active Frontage – The interface between buildings and streets is characterised by multiple entrances and windows, which allows interaction between public realm and the premises facing the street.

Adaptability - The ability of a building to respond to changing social, technological, economic and market conditions.

Affordable Housing – Social rented, affordable rent and intermediate housing, provided to eligible households whose needs are not met by the market.

Agricultural (Settlements) – Located above the reclaimed marshes, on clay lands in the central core of the Dengie peninsular and also in a group to the north of the Blackwater estuary.

Aradian (Settlements) – Unusual for their dispersed pattern and without a core street or green at their heart. Group on the highest contours of the district.

Article 4 Direction – A legal mechanism which withdraws deemed planning permission granted by the General Permitted Development Order.

Backland and Infill Development – Backland development refers to the development of land to the rear of existing buildings including garden land, whilst infill development refers to sites on the street frontage between existing buildings.

Biodiversity - a diverse range of species and the complex ecosystems they make up.

Blocks - A form of development where the perimeter is defined by streets.

Building Blocks – The whole or any part of any structure or erection and bounded by a set of streets.

Building Line – The line defined by the frontages of buildings along a street or road.

Built Environment – Buildings, roads, parks, and all other improvements constructed by people that form the physical character of a community.

Built Form – Buildings and Structures.

Character – The appearance of any urban or rural location in terms of its landscape or the layout of streets and open space, often giving places their own distinct identity

Communal Gardens - private open space shared by a number of households.

Community – a general term referring to the people living in a locality or the locality itself.

Conservation Area – A Conservation Area is an area of special architectural or historic interest, with a character or appearance, which is considered to be desirable to preserve or enhance.

Context – The setting of a site or area, including factors such as land uses, built and natural environment, and social and physical characteristics.

Cul-de-sac – A street that does not connect to others; a dead-end.

Defensible Space – A space in front of a building which indicates a change from public to private ownership. This can be expressed in different ways and vary in size. e.g garden, porch, railings etc.

Density – The mass or floor space of a building or buildings in relation to an area of land. It can be expressed in terms of plot ratio (for commercial development); homes or habitable rooms per hectare (for residential development); site coverage plus the number of floors or a maximum building height.

Design and Access Statements – are short reports which accompany and support planning applications where required, to outline design principles and concepts that have been applied to a proposal in relation to layout, scale, landscaping, and overall appearance.

Design Principle – An expression of one of the basic design ideas at the heart of an urban design framework, design guide, development brief or a development.

Desire line – A line of movement linking facilities or places, which would form a convenient and direct route for pedestrians and cyclists.

Diversity – A place with variety and choice to respond local needs.

Dormer Window - a vertical window with a roof of its own, positioned, at least in part, within the slope of the roof.

Eaves - the point where the lowest point of a roof slope, or a flat roof meets the outside wall.

Ecological - relating to, or concerned with, the relation of living organisms to one another and to their physical surroundings.

Edge of Village – Fringes of the villages, a location within certain metres of the village core boundary. Local circumstances should be taken into account in determining whether a site falls within the definition of edge of village.

Elevation – An external face of a building, or the height of a site above sea level.

Emphasis - where building elements highlight the vertical or horizontal nature of buildings which makes them look taller or wider.



Vertical Emphasis



Horizontal Emphasis

Acronyms and Glossary

Environment – Consists of all, or any, of the following media, namely, the air, water and land.

Enclosure – The arrangement of buildings, walls, trees to provide different levels of space.

Façade – The external face of a building or group of buildings that face the public realm.

Fenestration – The placement of windows on the exterior of a building.

Form – The physical appearance of a development i.e. its 3 dimensional shape.

Formal Play Spaces – Area marked and laid out for formal active recreational activities. This includes sports pitches or athletic tracks.

Functional – Designed to be practical and useful rather than attractive.

Gable - the vertical part of the end wall of a building contained within the roof slope, usually triangular but can be any 'roof' shape.

Gated Developments - developments that are totally secured from non residents entering by secure controlled access gates.

Garden Suburbs - Large scale development planned in a holistic and comprehensive way, including extensions to existing settlements. Development of this nature is based on the 'garden city' principles which in effect aim to improve quality of life by providing high quality design; infrastructure appropriate for the needs of the society such as public transport, public services, education and health facilities as well as community facilities and provision of green spaces, gardens, open spaces and landscaped areas integral to their design.

Grain – the general shape and direction of building footprints.

Green Infrastructure Network - a network of high quality green spaces and other environmental features such as parks, public open spaces, playing fields, sports pitches, woodlands, and allotments. The provision of Green Infrastructure can provide social, economic and environmental benefits close to where people live and work.

Habitable Room - any room used or intended to be used for sleeping, living, or cooking and eating purposes. Enclosed spaces such as bath or toilet facilities, service rooms, corridors, laundries, hallways, utility rooms or similar spaces are excluded from this definition.

Hard Standing - an area of hard core surface, which is usually used for the parking or manoeuvring of vehicles.

Heritage Assets - A range of geographical components of the historic environment which have been positively identified as having a degree of significance meriting consideration in planning decisions. These include listed buildings; old buildings that are not listed but still have local historical importance; scheduled monuments; war memorials; historic wreck sites; parks; historic gardens; conservation areas, archaeological sites and so on.

Hierarchy – A logical sequence of spaces, streets or building forms, increasing or decreasing in size or density throughout a development.

Historic Environment - All aspects of the environment resulting from the interaction between people and places through time, including all surviving physical remains of past human activity, whether visible, buried or submerged, and landscaped and planted or managed flora.

Inclusive – ensuring that buildings and their surrounding spaces can be accessed and used by everyone.

Informal Play Spaces – area not specifically marked and laid out for formal active recreational activities. This includes casual or informal playing space within housing estates, safe shared space such as play streets, outdoor equipped play areas for children of all.

Interest – something that brings advantages to or affects someone or something.

Landform – May refer to layout, scale, appearance and landscape.

Landmark – A building or structure that stands out from surrounding buildings.

Landscape – The appearance of land, including its shape, form, colours and elements, the way these (including those of streets) components combine in a way that is distinctive to particular localities, and the way they are perceived.

Landscape Character Assessment - An assessment to identify different landscape areas which have a distinct character based on a recognisable pattern of elements, including combinations of geology, land-form, soils, vegetation, land use and human settlement.

Language – The system of communication. Sometimes it may be used to show how an object (building) presents itself in relation to its surroundings.

Layout – The arrangement of buildings, streets, and spaces in a development.

LCA - Landscape Character Area

Legibility - The degree to which a place can be easily comprehended by its users so that navigation through that space is easily achieved.

Lifetime Homes - new homes which conform to Lifetime Homes standards thereby catering for various occupants and their needs. Lifetime Homes standards apply to external and internal features of buildings.

Listed Buildings – A building of special architectural or historic interest. Listed buildings are graded I, II* or II with grade I being the highest. Listing includes the interior as well as the exterior of the building, and any buildings or permanent structures (e.g wells within its curtilage). Demolition, in whole or in part of a listed building or any works of alteration or extension that would affect the character of the building will require a Listed Building Consent.

Listed Building Consent - an approval required before any alteration or whole or partial demolition of a listed building is undertaken.

Local Authority - is a generic term for any level of local government in the UK.

Local Character – See 'Character'.

Local Development Plan (LDP) - The plan for the future development of the local area, drawn up by the local planning authority in consultation with the community and stakeholders. Once adopted the Local Development Plan will legally form part of the Development Plan for the District, superseding the Replacement Local Plan (2005).

Maritime / Riverside (Settlements) – Abut the Blackwater estuary and the River Crouch. Low-lying, usually protected by sea walls, and with waterfront as the heart.

Massing – The volume of a building or group of buildings.

Movement - The passage of people and vehicles through buildings, places and spaces.

National Planning Policy Framework (NPPF) - Sets out the Government's planning policies for England, and provides a framework within which local people and their accountable councils can produce their own distinctive local and neighbourhood plans, which reflects the needs and priorities of their communities.

Neighbourhood Plan – A plan prepared by a Parish Council or Neighbourhood Forum for a particular neighbourhood area.

NPPG - National Planning Practice Guidance

Open Space - All open space of public value, including not just land, but also areas of water (such as rivers, canals, lakes and reservoirs) which offer important opportunities for sport and recreation and can act as a visual amenity.

Order of streets – See 'Street Hierarchy'.

Passive (open space) – See 'Informal play space'.

Permeability - The degree to which a residential development can be penetrated by foot, cycle and vehicle and the connectivity of the development to adjacent developments.

Permitted Development - Development that is deemed to be permitted without the requirement to submit a formal planning application. Development is usually small scale.

Photovoltaic - The direct conversion of solar radiation into electricity by the interaction of light with electrons in a semiconductor device or cell

Place – An area, town or building.

Planning obligation - A legally enforceable obligation entered into under section 106 of the Town and Country Planning Act 1990 to mitigate the impacts of a development proposal.

Plot - the area contained within the boundary of one dwelling or a group of linked dwellings, such as a block of flats or a sheltered housing complex.

Proportion – See 'Scale'.

Public Art – Permanent or temporary physical works of art visible to the general public, whether part of a building or free-standing. For example, sculpture, lighting effects, street furniture, paving, railings and signs.

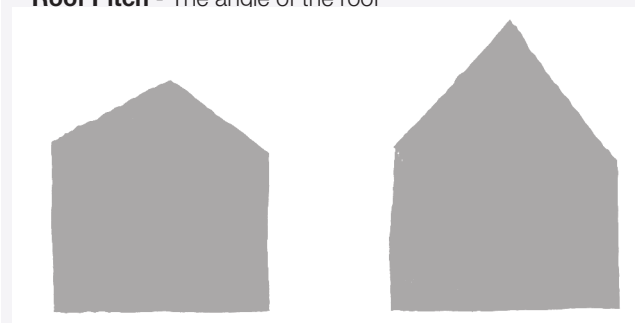
Public Realm - The spaces between buildings accessible to the public; including the highway, green areas, squares etc.

Robust - Functions well in a wide range of, often unanticipated, future scenarios by being able to accommodate modification and adaptation.

Private Amenity Space – are small spaces of land which can be found in and around residential areas. They provide opportunities for informal recreation and enhance the quality of residential areas.

PROW - Public Rights of Way

Roof Pitch - The angle of the roof



Slack Pitch

Steep Pitch

Rhythm – Repetition or alternation of elements or architectural features like columns, chimneys, windows and doors with defined intervals between them. It can create a sense of movement and establish a pattern and texture.

Scale – The impression of a building when seen in relation to its surroundings, or the size of parts of a building or its details, particularly as experienced in relation to the size of a person.

Set-back - the distance of a building alignment from the front property boundary or street frontage.

Sense of place - A property of space with strong identity and character that is deeply felt by local inhabitants and visitors.

Shared Surface - is where all users of a street share one uniform surface with virtually no delineation in height, surface materials, or road markings.

Spatial Context - The relationships or interactions that exist between uses, services and facilities within a specified area.

Social Exclusion - term for what can happen when people or areas suffer from a combination of linked problems, such as unemployment, poor skills, low incomes, poor housing, high crime environments, bad health and family breakdown.

Social Interaction – The connections between people and places.

Spaces – Includes not just land, but also areas of water.

‘Special’ Areas – Special Area of Conservation (SACS) are a selection of very important SSSIs that entered in the Register of European Sites as part of the Natura 2000 network. SACs are established to protect wild birds under the Birds Directive.

SSSI - Site of Special Scientific Interest

Street Furniture – Structure in a street or space, for example, bus shelters, light columns, signs, seating and litter bins.

Street Hierarchy – The structure of street or footways that connect in the local area and at wider scales.

Streetscape - The character and appearance of the street environment.

Structural Planting - evergreen and deciduous shrubs, trees or other planing that retains its form and shape throughout the year.

Supplementary Planning Documents (SPD) - Documents which add further detail to the policies in the Local Development Plan. They can be used to provide further guidance for development on specific sites, or on particular issues, such as design. Supplementary planning documents are capable of being a material consideration in planning decisions but are not part of the development plan.

Sustainable Development – Development that meets the economic, environmental and community needs of the present, without compromising the ability of future generations to meet their own needs.

Sustainable Transport - Efficient, safe and accessible means of transport with overall low impact on the environment, including walking and cycling, low and ultra-low emission vehicles, car sharing and public transport.

SuDs - Sustainable Urban Drainage Systems. Schemes for handling surface water by means other than pipes and storm drains, such as porous paving, swales, channels, reed beds and balancing ponds, to reduce the potential of flooding and improve water quality on new and existing urban development.

Tenure Blind - the delivery of housing whereby both market housing and affordable housing are designed to be as visually similar as possible as a way of reducing inequalities or the feeling of inequality that may exist between residents.

Topography – A description (or visual representation on a map) of the shape of the land, for example, contours or change in the height of land above sea level.

Townscape – The general appearance of a built-up area, for example a street, a town or city.

Urban Design - The process of making places, incorporating the design of buildings, spaces and details.

Urban Grain - The pattern of development in a settlement.

Vegetation – Plants in general or the plants that are found in a particular area.

Village core – Usually clusters around one or more road junctions with development spread along the road frontages.

Vision – the ideas for the future of an area. It can be developed through a local development plan and to refer to the aims, objectives and aspirations of stakeholders.

Wheelchair Housing - affordable and private housing that is designed to be wheelchair accessible.

A1 APPENDIX 1 - Technical Documents

National Space Standards, DCLG, 2015

Noise

Air Quality

Older Persons Housing

Landscape and Green Infrastructure

Vehicle Parking Standards, MDC, 2006 (or successor document)

Essex Design Guide - Road Type Table

A2 APPENDIX 2 - How to Measure Net Density

Residential Density Calculation

Residential density is the ratio of a population to residential land area. This measure can be further classified in terms of net and gross residential densities based on the definition of the reference area.

The measure of gross residential density considers the residential area in its integrity. In addition to the area allocated for residence, it also takes into account nonresidential spaces such as internal roads, parks, schools, community centres and so on which are meant to serve the local community.

Net residential area refers only to land covered by residential development, along with gardens and other spaces that are physically included in it; this usually takes into account half the width of adjacent roads.

+ Calculation Example

Figure A

no. of residential units: 54

residential area in its integrity: 2Ha

GROSS residential density: 27 units/Ha

Figure B

no. of residential units: 54

residential area: 1.2Ha

NET residential density: 45 units/Ha

Figure A: area taken into account to measure GROSS residential density

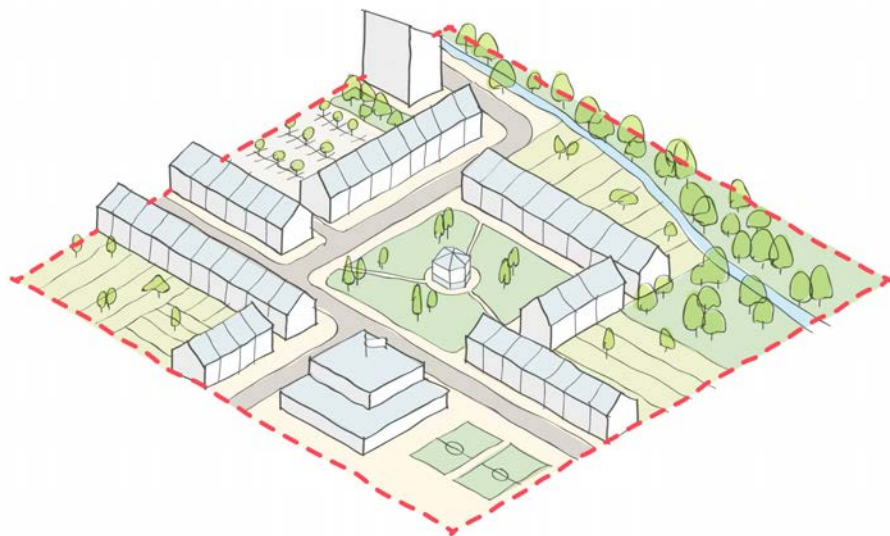
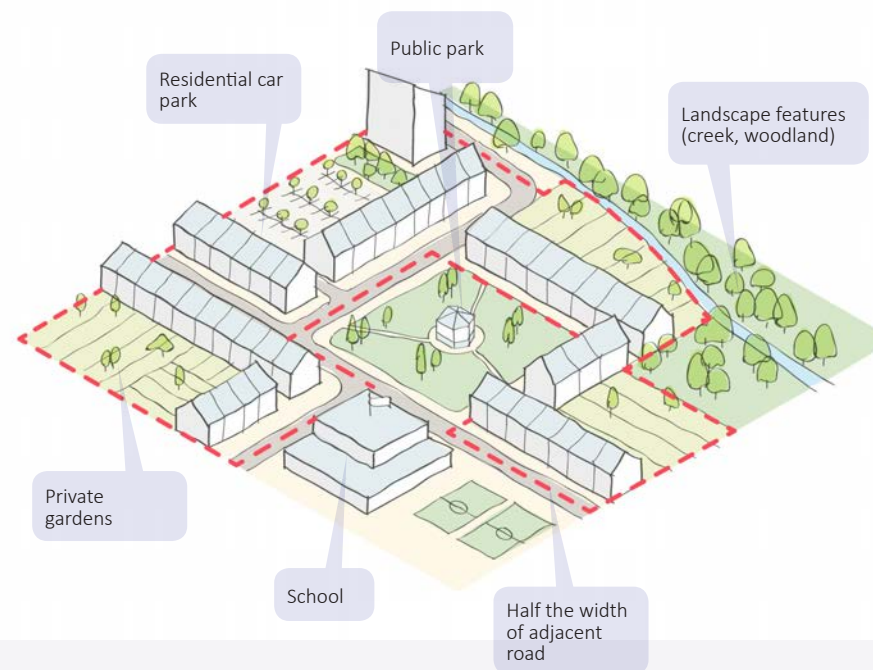


Figure B: area taken into account to measure NET residential density



A3 APPENDIX 3 - How to Write a Design and Access Statement

A4 APPENDIX 4 - How to Write a Heritage Statement

Maldon District Design Guide

Planning and Noise



MALDON DISTRICT
COUNCIL

**Technical Document to
Maldon District Design Guide
February 2017**



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- 1.2 The aims of this guidance
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- 3.3. Residential development proposals
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- 3.5. Wind turbines
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- 3.7. Schools, hospitals and other noise sensitive buildings
- 3.8. Licensed premises
- 3.9. Artificial grass pitch (AGP – all weather pitches)

APPENDIX: - Quick reference guide



1. Introduction

1.1. The purpose of this guidance

Noise can have a significant impact on the quality of life, health and wellbeing of communities. It comes from a many sources, including roads, railways, aircraft, industry, retail premises, sports, leisure and recreational activities.

These environmental noises can generally be described in two ways. Firstly there is anonymous/ambient noise which cannot be attributed to a single noise source such as road traffic, industrial estates (multiple occupants). Secondly there are specific noise sources such as mechanical ventilation or industrial processes which can be identified and considered separately.

It is recognised that exposure to noise can cause annoyance and sleep disturbance both of which impact on quality of life. Experts agree that there is growing evidence showing that annoyance and sleep disturbance can cause adverse health effects. The distinction that has been made between 'quality of life' effects and 'health' effects recognises that there is emerging evidence that long term exposure to some types of noise (research has focused on transport) can additionally cause an increased risk of direct health effects. Research on the health effects of long term exposure to noise continues in the UK and the Europe. BS8233: 2014 (Sound insulation and noise reduction for buildings) sets noise levels in section 7 based on levels known to affect health and is considered a minimum standard.

This guidance is to assist planning agents, developers and their consultants, to help ensure consistency in the approach to noise considerations when dealing with development projects in the Maldon District at the planning stage.

Noise can be a material consideration within the planning process with the potential to affect and influence the planning decisions. This guidance intends to ensure that noise considerations are dealt with at the earliest opportunity. To achieve this the necessary and appropriate information concerning both existing noise impacts on the proposed development as well as noise likely to be caused by the proposed development should be provided with planning applications at the validation stage.

This document clarifies when a noise assessment is likely to be required and provides guidance on the information required to undertake such an assessment. It is important that noise considerations can be taken into account early in the development management process to minimise the risk of delays caused by noise concerns during the consultation process.

1.2. Aims of this guidance

An important focus of this guidance is to ensure that residents living within the Maldon District are able to live in a property where noise does not unreasonably affect their quality of life or adversely affect their health and wellbeing.

Additionally, that businesses are able to operate effectively without being unduly restricted as a result of the noise they create or affecting neighbouring land uses.

In line with government policies on noise and sustainable development the main aim will be to ensure that development achieves a good quality of life rather than just ensuring that health will not be affected which is considered a minimum standard.

This guidance aims to ensure that new development proposals design out noise problems rather than provide mitigation and this will be the starting point.

With this in mind this guidance aims to:

- Sustain and where practical improve ambient noise levels in residential areas
- Ensure that new residential properties are not exposed to levels of noise that will impact on the future occupier's quality of life or health
- Ensure that new noise sources do not unreasonably affect existing noise sensitive land uses
- Ensure that new residential developments or other noise sensitive receptors are not introduced in areas where existing business or leisure facilities that emit noise are restricted or likely to have complaints made against them as a result of the new development

This document supports the Government's noise policy vision which is to:

'Promote good health and a good quality of life through the effective management of noise within the context of Government policy on sustainable development.'

NOISE POLICY STATEMENT FOR ENGLAND (NPSE) AS PRODUCED BY DEFRA IN MARCH 2010.

1.3 The soundscape in the Maldon District

The Maldon District is a predominately rural district with villages and towns located amongst arable farmland. The soundscape is therefore predominately quiet rural in and around the villages. Night time background noise levels are commonly below 30dBLA90 and often as low as 20dBLA90.

Even in the largest towns of Maldon, Heybridge and Burnham-on-Crouch night time background levels do not rise much above 30dBLA90 in most areas.

The exception to this is around some of the busier roads in the towns and the busy A and B roads around the District as well as some of the larger industrial estates.

The vast majority of the District has ambient noise significantly below the levels recommended as a minimum standard in gardens by BS8233:2014 and the World Health Organisation (WHO).

IMAGE: GOLDHANGER | DAVID NEWMAN



2. Noise and the planning process

2.1. National Planning Policy Framework (NPPF)

Paragraph 109 of the NPPF states that the planning system should contribute to and enhance the natural and local environment by preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of soil, air, water or noise pollution or land instability.

Noise is specifically considered in Chapter 11 of the NPPF Paragraph 123 states:

Planning policies and decisions should aim to:

- avoid noise from giving rise to significant adverse impacts on health and quality of life as a result of new development
- mitigate and reduce to a minimum other adverse impacts on health and quality of life arising from noise from new development, including through the use of conditions
- recognise that development will often create some noise and existing businesses wanting to develop in continuance of their business should not have unreasonable restrictions put on them because of changes in nearby land uses since they were established; and
- identify and protect areas of tranquillity which have remained relatively undisturbed by noise and are prized for their recreational and amenity value for this reason

When considering the potential impacts on health and quality of life the NPPF refers to the Noise Policy Statement for England (NPSE) as produced by DEFRA in March 2010.

2.2. Noise Policy Statement for England (NPSE)

The NPSE sets out government policy on noise management and sustainable development. The policy's overarching aims are to:

- avoid significant adverse impacts on health and quality of life
- mitigate and minimise adverse impacts on health and quality of life; and
- where possible, contribute to the improvement of health and quality of life

The NPSE introduces the concept of observable effect levels in relation to noise and uses the following concepts:

NOAEL – No observed adverse effect level

This is the level below which no effect can be detected. In simple terms, below this level, there is no detectable effect on health and quality of life due to the noise.

LOAEL – lowest observed adverse effect level

This is the level above which adverse effects on health and quality of life can be detected.

SOAEL – Significant observed adverse effect level

This is the level above which significant adverse effects on health and quality of life occur.

There is no specific defined measured level of noise which corresponds to these levels. The purpose of not setting levels is to ensure that the aims and principles are applied within the context of the area where development is taking place. This recognises that noise that does not cause a problem in a noisy urban environment may be unacceptable in a quieter location.

Developers and their consultants must therefore consider the context and nature of the area in which their proposal is located.

The first aim of the NPSE states that significant adverse effects on health and quality of life should be avoided while also taking into account the guiding principles of sustainable development

The second aim of the NPSE refers to the situation where the impact lies somewhere between LOAEL and SOAEL. It requires that all reasonable steps should be taken to mitigate and minimise adverse effects on health and quality of life while also taking into account the guiding principles of sustainable development.

The third aim seeks, where possible, to positively improve health and quality of life through the pro-active management of noise while also taking into account the guiding principles of sustainable development.

The NPSE makes significant reference to the government policy document 'UK sustainable development strategy - 5 principles of sustainable development'

Adherence to the aims of the NPSE will ensure that the aims of the NPPF can be satisfied and that noise will not cause a loss of amenity or unreasonably affect the quality of life, health and well being of residents that would potentially give grounds for refusal of a proposal based on the potential for noise to have an unreasonable effect on an existing property.

2.3. National Planning Practice Guidance (NPPG)

Developers should consider the NPPG when preparing their application to the Local Planning Authority. It provides further guidance on how planning can take account of noise when considering the suitability, design and layout of a development. It aims to provide a guide to local authorities on how to view and assess observed effect levels in line with the Explanatory Note of the Noise Policy Statement for England.

The NPPG states that noise needs to be considered when new developments may create additional noise and when new developments would be sensitive to the prevailing acoustic environment.

This is achieved by considering:

- Whether or not a significant adverse effect is occurring or likely to occur
- Whether or not an adverse effect is occurring or likely to occur
- Whether or not a good standard of amenity can be achieved

The table below summarises the noise exposure hierarchy, based on the likely average response. However, it should be viewed in the context of the area in which it is being applied.

Perception	Examples of outcomes	Increasing effect level	Action
Not noticeable	No effect	No observed effect	No specific measures required
Noticeable and not intrusive	Noise can be heard, but does not cause any change in behaviour or attitude. Can slightly affect the acoustic character of the area but not such that there is a perceived change in the quality of life.	No observed adverse effect Lowest observed adverse effect level	No specific measures required
Noticeable and intrusive	Noise can be heard and causes small changes in behaviour and/or attitude, e.g. turning up volume of television; speaking more loudly; where there is no alternative ventilation, having to close windows for some of the time because of the noise. Potential for some reported sleep disturbance. Affects the acoustic character of the area such that there is a perceived change in the quality of life.	Observed adverse effect Significant observed adverse effect level	Mitigate and reduce to a minimum
Noticeable and disruptive	The noise causes a material change in behaviour and/or attitude, e.g. avoiding certain activities during periods of intrusion; where there is no alternative ventilation, having to keep windows closed most of the time because of the noise. Potential for sleep disturbance resulting in difficulty in getting to sleep, premature awakening and difficulty in getting back to sleep. Quality of life diminished due to change in acoustic character of the area.	Significant observed adverse effect	Avoid
Noticeable and very disruptive	Extensive and regular changes in behaviour and/or an inability to mitigate effect of noise leading to psychological stress or physiological effects, e.g. regular sleep deprivation/awakening; loss of appetite, significant, medically definable harm, e.g. auditory and non-auditory.	Unacceptable adverse effect	Prevent

Environmental acoustics is a complex topic and a competent person will be necessary to identify measure and interpret the potential effects of noise.

The NPPG also recognises that noise is not simply a consideration of absolute noise levels and summarises the range of considerations when assessing noise in paragraph 006. These other considerations include:

- The nature/character of noise (including tonal elements, High/Low frequency content)
- Relationship of a noise source with existing background noise
- Time of day
- Duration of noise emissions
- Number of occurrences and pattern of noise events
- Cumulative impacts of noise sources

Paragraph 8 of the NPPG broadly outlines a hierarchy of mitigation options for noise making developments which encourage engineering solutions for noise sources, site layout considerations, planning conditions to restrict the time and duration of noise making activities or the maximum noise emission permitted.

For noise sensitive developments mitigation measures are suggested. In the first instance it is advised to avoid noisy locations. Where this is not possible or where the site is desirable in terms of the Local Development Plan mitigation should be led by designing the development to reduce the impact of noise this may include; considering the layout of the site to minimise the noise impact such as locating a commercial building between the noise sensitive buildings and the noise source and/or using acoustic barriers and lastly improved sound insulation.

The NPPG states that care should take when considering mitigation to ensure that the measures do not make for an unsatisfactory development.

3. Applying noise assessments to developments

3.1. What the Maldon District expects

As a predominately quiet rural district the intention is to sustain the high quality of life afforded to both existing residents and future residents occupying new housing.

Maldon District Council's primary approach to noise and noise sensitive development is to seek physical separation through planning controls. This approach applies both where noise sensitive development (such as housing) is seeking to locate in a "noisy" area, and where "noisy" activities are proposed, or exist and proposed to expand, in noise sensitive locations.

It is recognised however that urban development and localised land use changes may make it necessary to locate a development near to some of the areas in the District with a high acoustic environment.

If suitable separation cannot be achieved, consideration will be given to whether it is practicable to control or reduce noise levels, or to mitigate the impact of noise, through the use of conditions or planning obligations.

Where noise impact cannot be made acceptable through separation or other noise control measures, planning permission would normally be recommended for refusal.

When considering noise levels during a noise assessment it should be noted that there are no noise levels set in the NPSE.

Developers and their consultants must therefore consider the context and nature of the area in which their proposal is located. What is acceptable in London or Manchester for example is unlikely to be appropriate and therefore unacceptable in the Maldon District.

Developers should also ensure they are aware of existing business and land uses likely to generate noise such as leisure facilities and licensed premises for example, when locating noise sensitive dwellings and ensure their proposal is not likely to restrict the ability of those premises to continue to operate or expand in the future.

It is therefore important that developers use noise assessment's to inform their design proposals.

Noise assessments should be carried out by a competent person.

3.2. Basic principles of design

When considering a suitable location for a noise sensitive development avoiding noisy locations is the most effective way of preventing noise becoming a problem.

However if this is not possible, noise should be considered at the design stage of any development and there is a simple hierarchy of mitigation options in the following order:

- **Engineering:** In the first instance the most effective form of noise control is achieved by reducing the noise generated at source and/or containing the noise generated. This is typically only viable where an application involves the introduction of a new noise source. However, it may be possible for an applicant to work with the persons in control of a noise source prior to the application being submitted
- **Design and layout:** Where possible, site design and layout should be used to optimise the distance between the noise source and noise-sensitive receptors and/or incorporating good design to minimise noise transmission through the use of acoustic screening by natural or purpose built barriers, or other buildings. When considering the use of other buildings this should consider the use of business premises for example located between a road and residential dwellings
- **Using planning conditions/obligations:** This would include conditions to restrict hours of operation of noise emitting plant/activities/deliveries or specifying permissible noise levels differentiating as appropriate between different times of day, such as evenings and late at night. Conditions may also prohibit certain activities from occurring outside
- **Mitigation at the receptor:** As a final resort where no other viable options are available the impact on sensitive areas likely to be affected by noise can be mitigated, including through noise insulation when the impact is on a building. Where this refers to habitable rooms within dwellings closed windows will not normally be accepted as a noise control measure

In some cases locations identified as sites within the Maldon District Local Development Plan may be permitted to use windows and building envelope insulation as a final resort for a small number of habitable rooms where it has been demonstrated that there is no other viable option available in the hierarchy of mitigation options.

3.3. Residential development proposals

Proposed residential developments that are likely to be affected by noise from existing or future developments with planning permission will require a full assessment of the impact, in the form of a noise report carried out by a competent person, provided with the application details.

Noise sources to consider would typically include transport infrastructure such as roads and railways and commercial land uses. It should be noted that commercial noise

sources are not limited to heavy industry. Light Industry, offices, retail and food premises commonly have air conditioning units and extract ventilation systems that 'depending on distance' may create excessive noise at the proposal site. A noise assessment should be undertaken to find out the level of impact likely to be created by the noise.

The development should then be designed to minimise the impact of these noise sources.

If the noise source is from a specific source of noise and has characteristics that may cause annoyance appropriate guidance that relates to the sound source should be selected.

For example if the noise source is mechanical plant on an industrial or retail building BS4142:2014 should be used to assess noise impact. It will not be considered acceptable to use BS8233:2014 for assessing the suitability of such noise sources.

Where the noise source is a general ambient noise from roads or the general environment BS8233:2014 should be used to guide what is considered acceptable and determine whether mitigation is required.

Where noise mitigation is required, the criteria found in BS 8233: 2014 (Sound insulation and noise reduction for buildings) should be used as stated in World Health Organisation (WHO) Guidelines on Community Noise (1999).

The noise levels for amenity spaces and habitable rooms should ideally be met without the need for mitigation measures incorporated into the building itself. Developers should be attempting to use design and layout to provide separation from the noise source. Where this is not practical acoustic barriers should be provided. These can be purpose built or by locating buildings that are not noise sensitive to serve as barrier.

Where further mitigation is required after layout and barriers have been considered, building orientation, internal layout, building insulation and enhanced glazing can then be considered.

On some sites, where it is not possible to achieve acceptable mitigation fixed windows and mechanical ventilation may be required. If any mechanical ventilation is recommended please note that internal noise levels must be considered separately with the system operating on and off.

However, unless the development accords with the Local Development Plan and planning policies, the development within the high noise environment is not likely to be considered appropriate.

Where mechanical ventilation is proposed full justification should be provided that demonstrates that alternative methods of mitigation are not practical. In all other regards, the proposal should accord with the aims and policies of the Local Development Plan and NPSE.



3.4. Industrial, commercial and agricultural development

The most appropriate way to assess noise issues resulting from proposed commercial premises, near an affected building façade, is via the use of BS 4142: 2014 (Method for rating industrial noise affecting mixed residential and industrial areas).

This standard provides a tool to assess the likelihood of complaints about noise from industrial development. It can be carried out using a mix of predictive and actual measurements. Post construction verification checks are recommended.

Some circumstances and noise sources may be unsuitable for applying BS4142: 2014. For example, the standard relies on average noise levels over long time periods so short term high impact noise sources may need to be considered with regard's to the impact of maximum noise levels.

If there is any doubt developers or their consultant's should contact Maldon District Council Environment Services to discuss their concerns and agree an appropriate measurement and assessment methodology.

3.5. Wind turbines

The assessment of proposed wind turbines with a generating capacity f 50kW or above must be carried out with regards to The Institute of Acoustics (IoA) publication: A Good Practice Guide To The Application Of Etsu-R-97 For The Assessment And Rating Of Wind Turbine Noise.

Small scale wind turbines with a generating capacity below 50kW should be carried out in consideration the British Wind Energy Association (now known as Renewables UK) publication 'Small Wind Turbine Performance and Safety Standard'(2008).

Large scale wind turbine applications should also consider Amplitude Modulation and a condition will be applied should permission be granted.

IMAGE: TOM MCGANN

3.6. Minerals workings

The planning authority for minerals workings is Essex County Council. However, Maldon District Council is a consultee and its comments with regard to noise will consider the NPPF technical guidance which provides specific guidance on noise from minerals workings.

3.7. Schools, hospitals and other noise sensitive buildings

Detailed guidance on the design of schools and hospitals can be found In the Department for Education publication BB 93: Acoustic Design of Schools (2003) and the Department of Health specialist services – Health Technical Memorandum 08-01:Acoustics (2008).

3.8. Licensed premises

Where amplified noise break out from licensed premises is likely, the applicant should ensure that the insulation of the premises and the volume and sound frequency setting inside the commercial premises is adequate. These buildings will require an adequate mechanical ventilation or air conditioning system to be provided supplying sufficient fresh air for patrons, following the guidance in the Chartered Institute of Building Engineers (CIBSE) or any later replacement guidance.

Planning applications will therefore need to be accompanied by a suitable assessment demonstrating that the building is adequate in sound insulation terms for its proposed use, which will include ensuring there is sufficient sound attenuation of any ductwork, particularly at the inlet/outlet, to prevent excessive noise breakout and the provision of soundproofed doors, double door lobbies and windows, which must remain closed.

The installation of a suitable limiter device with electric contact breaks on fire doors and/or windows may in some cases be appropriate. Planning conditions may also be imposed to ensure compliance with the long term retention of such soundproofing measures and regulate opening times in line with the local area.

If a proposal is being made to build homes near to existing licensed premises it will not be acceptable to use BS8233:2014 as a standard to guide the suitability of the development.

3.9. Artificial grass pitch (AGP – all weather pitches)

There are currently no codes of practice or standards that are suitable for determining whether noise from proposed AGP or residential properties being located near to AGP is acceptable in terms of effect on amenity.

These facilities are commonly used during evenings and weekends and valuable facilities for community use. However they are a common cause of complaint from residential dwellings.

The main cause of complaint is typically due to the nature of the noise which is typically made up of shouts, whistles and impact noises from balls being struck. As these noises are short term, high noise events, the use of time LAeqTime noise levels to assess noise impact is not a suitable indicator for the effects of noise. Consideration of the likely effects of noise at the receptor and suitable noise control measures will involve professional judgement and where noise readings are used short and long term LA90, LAmax, LAeq, LA10 and LA1 are appropriate indices for this purpose.

The primary control of noise will be from maximising separation distances combined with landscaping and mounding of earth.

In noise sensitive locations, the materials of construction and the design should be made in consideration of reducing noise. Fences should be fixed to support posts and rubber damping pads or heavy netting to absorb ball impacts should be considered.

Sports England Guidance on the design and construction of AGPs is available on Sports England's website:

<https://www.sportengland.org/facilities-planning/tools-guidance/design-and-cost-guidance/artificial-sports-surfaces/>

TYPE OF DEVELOPMENT

NEW RESIDENTIAL (C3 AND C4 USE CLASSES)	
	<ul style="list-style-type: none"> Near to busy roads (A Class and Major B roads) and railways <p>A noise report will be required where noise levels at the development site exceed 50dBLAeq daytime and/or 42 dBLAeqnight.</p>
	<ul style="list-style-type: none"> Near to industrial estates and premises <p>A noise report will be required to characterise the effect of the industrial noise on proposed residential dwellings</p>
	<ul style="list-style-type: none"> Near to agricultural yards and buildings <p>Noise report will be required to characterise the effect of the agricultural premises on proposed residential dwellings</p>
	<ul style="list-style-type: none"> 10 + residential dwelling (major development) <p>Only required where an existing or proposed source of noise may affect the proposed residential uses. Please consult the environmental health department if unsure.</p>
CHANGE OF USE TO RESIDENTIAL	
If the premises applying for change of use is located near to existing or proposed noise sources such as roads or commercial activities a noise assessment will be required.	
	RESIDENTIAL INSTITUTIONS C2/C2a uses (care homes, hospitals, nursing homes, residential colleges etc) <p>Noise assessment will be required to assess both the impacts of existing noise and the effects of plant associated with the C2/C2a building e.g air conditioning units, ASHPs, extract ventilation.</p>
	INDUSTRIAL USES (B2 general industrial and B8 storage and distribution as well as minerals development and waste management sites) <p>Includes new development, mixed use developments, extensions, layout alterations or installation of new equipment at existing site. Consideration of noise from transport movements should be given.</p>
	LIGHT INDUSTRY AND OFFICES (B1 uses) <p>Includes new development, mixed use developments, extensions, layout alterations or installation of new equipment at existing site.</p>
	COMMERCIAL USES A1 and A2 uses (Shops, financial and professional services) <ul style="list-style-type: none"> > A noise assessment will be required where air handling units, extractor fans, or other noise emitting plant will be used > Consideration of the impact of the opening hours and customer access and egress should be provided > The noise impact of the car park should also be considered
	ENTERTAINMENT / FOOD & DRINK ETC <p>A3 restaurants and cafes / A4 drinking establishments / A5 hot food takeaways / D2 cinemas, concert halls, swimming baths, night clubs etc</p> <ul style="list-style-type: none"> > Should consider air handling units, extractor fans, or other noise emitting plant will be used > Consideration of the impact of the opening hours and customer access and egress should be provided > The noise impact of the car park should also be considered
	OUTDOOR SPORTS AND RECREATION <p>Includes multi use sports pitches (mugas), motor sports and shooting ranges</p>
	WIND TURBINES
	TRANSPORT SCHEMES roads, railways, ports including alterations to existing schemes <p>Early consultation with the LPA/EH would be expected</p>
	OTHER SUIS GENERIS USES e.g petrol stations, launderette, taxi businesses. <ul style="list-style-type: none"> > A noise assessment will be required where air handling units, extractor fans, or other noise emitting plant will be used > Consideration of the impact of the opening hours and customer access and egress should be provided > The noise impact of the car park should also be considered

NOISE REPORT REQUIRED YES



MAYBE

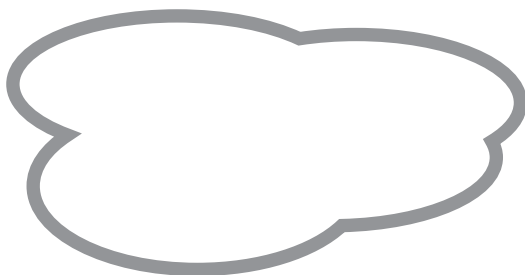


Maldon District Design Guide **Planning and Noise**

Technical Document to
Maldon District Design Guide
February 2017

If you need help with this information in a
different format, please call 01621 854477

Maldon District Design Guide Assessing Air Quality and Emissions Impacts from Development



Technical Document to
Maldon District Design Guide
February 2017

An illustration of a town with various buildings, trees, and a bus. The town is depicted in a stylized, hand-drawn manner with a color palette of greens, yellows, and greys. The buildings are of different sizes and shapes, some with multiple stories. Trees are scattered throughout the town, and a bus is visible on a road. The overall scene is a peaceful, suburban-like environment.

Air Quality Reading List

- Environment Act 1995 and associated Local Air Quality Management Regulations
- DEFRA: The Air Quality Strategy for England, Scotland, Wales and Northern Ireland Or equivalent document
- National Planning Policy Framework (NPPF)
- National Planning Practice Guidance (NPPG)
- Environmental Protection UK's (EPUK) and Institute of Air Quality Management (IAGM): Land-Use Planning & Development Control: Planning for Air Quality (2015)
- DEFRA's LAQM Technical Guidance TG (16)
- Institute of Air Quality Management: Guidance on the Assessment of Dust from Demolition and Construction
- The Control Of Dust And Emissions During Construction And Demolition Supplementary Planning Guidance: Mayor of London. Greater London Authority
- DEFRA Low Emissions Strategies using the planning system to reduce transport emissions Good Practice Guidance (2010)
- DEFRA Emissions Factor Toolkit:
<http://laqm.defra.gov.uk/review-and-assessment/tools/emissions.html>

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1. INTRODUCTION

1.1. The purpose of this guidance

This guidance is aimed at planning agents, developers and their consultants. It is intended to help ensure consistency in the approach to air quality and vehicle emissions considerations when dealing with air quality for development projects in the Maldon District at the planning stage.

Air quality can be a material consideration within the planning process with the potential to affect and influence the planning decisions. This guidance intends to ensure that air quality considerations are dealt with at the earliest opportunity and that the necessary and appropriate information concerning a development's potential impacts on air quality, both onsite and further afield, is provided with planning applications at the validation stage.

This document clarifies when an assessment is likely to be required and provides guidance on the information required to undertake such an assessment. It is important that air quality considerations can be taken into account early in the development management process.

1.2. Aims of this guidance

An important focus of this guidance is on minimising the air quality impacts from developments for which air quality assessments have been requested by the planning authority. With this in mind this guidance aims to:

- Sustain and where practical improve local air quality
- Provide a consistent approach to considering the effects of new developments on local air quality within the Maldon District
- Enable early engagement by Identifying which developments would require an air quality assessment to be provided and the points that need to be considered with the planning application, thereby minimising delays during the decision making process
- Ensure better regulation by formalising the approach to undertaking air quality assessments and applying appropriate mitigation consistently in planning decisions

1.3. Guidance process

- Section 1:** Sets the context for this document
- Section 2:** Explains the relationship between local air quality and the planning framework
- Section 3:** Outlines the current air quality situation in the Maldon District
- Section 4:** Explains when an assessment may be required
- Section 5:** Guides what is required of an air quality assessment and how the significance of an assessment is identified
- Section 6:** Discusses options for mitigation

1.4. Background to local air quality management

Local Air Quality Management (LAQM) in the UK was introduced with the Environment Act 1995. It was seen by Government as the best way to deal with localised 'hot spots' of poor air quality that were expected to remain after national and international measures, such as controls of emissions from new vehicles, brought about a general improvement.

LAQM has established air quality 'standards' and 'objectives' (AQOs) for the air pollutants that are of concern. The 'standards' have been set to protect human health and the environment based on scientific and medical evidence.

The Air Quality Strategy for England, Scotland, Wales and Northern Ireland (DEFRA 2007) estimates that life expectancy of the average person in the UK is reduced by up to eight months as a result of air quality with health costs exceeding £20 billion. It can be seen that improving air quality has considerable benefits to human health and well being, the environment and the economy.

Part IV of the Environment Act 1995 places a duty on local authorities to review and assess local air quality. Where a local authority considers that one or more of the objectives are likely to be exceeded and there is relevant exposure, it must declare an Air Quality Management Area (AQMA) and produce an action plan setting out measures to work towards an improvement of the air quality in the area. Local authorities are required to submit all relevant air quality reports to the Department for Environment, Food and Rural Affairs (DEFRA) for acceptance.

The UK objectives take account of the EU limit values and are either effectively identical, or more stringent. It is thus common practice to focus air quality assessments on the UK objectives. Formal Environmental Impact Assessments (EIAs) will, however, also need to take explicit account of the EU Limit Values. Furthermore, a formal EIA will introduce the need to assess the impact on PM_{2.5} concentrations, along with those pollutants that affect ecosystems and vegetation where appropriate.

2. LOCAL AIR QUALITY IN THE PLANNING FRAMEWORK

2.1. National Planning Policy Framework (NPPF)

The planning system is critical for managing local air quality, especially in rural areas where road traffic congestion and local built environment features are the common cause of areas where there are elevated air pollutants such as NO₂ and PM₁₀s.

The NPPF aims to guide development through a principle led set of criteria. The core principles include contributing to conserving and enhancing the natural environment and reducing pollution and taking account of and supporting local strategies to improve health, social and cultural wellbeing for all.

Paragraph 109 of the NPPF states that the planning system should contribute to and enhance the natural and local environment by preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels of soil, air, water or noise pollution or land instability.

Local air quality management is specifically referenced in paragraph 124 stating that

'Planning policies should sustain compliance with and contribute towards EU limit values or national objectives for pollutants, taking into account the presence of Air Quality Management Areas and the cumulative impacts on air quality from individual sites in local areas. Planning decisions should ensure that any new development in Air Quality Management Areas is consistent with the local air quality action plan.'

It is recognised therefore that land use planning and development management is considered to be an essential process to improve or sustain air quality. This may include locating developments in such a way as to reduce emissions overall, and secondly reducing the direct impacts of those developments. These impacts are not limited to industrial processes that emit pollutants but also include residential developments whose future residents will add further traffic to the existing road network with the potential to create or add to areas of congestion and rising air pollution.

Even where a development is not located within an AQMA there is still a need to regard air quality as a material factor in determining planning applications in any location. This is particularly important where the proposed development could have adverse impacts on air quality within an AQMA, or where air quality in an area further afield from the development site is close to exceeding guideline objectives itself.

Developers are directed to ensure that they, or their consultants, have reference to any relevant documents published by the local authority that identify areas that may be affected as part of the air quality review and assessment process.

2.2. National Planning Practice Guidance (NPPG)

The NPPG provides further guidance on how planning can take account of the impact of new development on air quality.

It recognises that plan making should not only take account of AQMAs but also account for other areas where there may be limitations on new development due to air quality.

Air quality is also a consideration in strategic environmental assessment and sustainability appraisal can be used to shape an appropriate strategy.

Local planning policy should consider the review of air quality carried under the LAQM regime and local plans may consider:

- The potential cumulative impact of a number of smaller developments on air quality as well as the effect of more substantial developments
- The impact of point sources of air pollution (pollution that originates from one place), and
- Ways in which new development would be appropriate in locations where air quality is or likely to be a concern and not give rise to unacceptable risks from pollution. This could be through, for example, identifying measures for offsetting the impact on air quality arising from new development including supporting measures in an air quality action plan or low emissions strategy where applicable

Developers and planners deciding whether air quality is a consideration for a particular development should consider whether the development would:

- **Significantly affect traffic in the immediate vicinity of the proposed development site or further afield.** This could be by generating or increasing traffic congestion; significantly changing traffic volumes, vehicle speed or both; or significantly altering the traffic composition on local roads. Other matters to consider include whether the proposal involves the development of a bus station, coach or lorry park; adds to turnover in a large car park; or results in construction sites that would generate large heavy goods vehicle flows over a period of a year or more
- **Introduce new point sources of air pollution.** This could include furnaces which require prior notification to local authorities; or extraction systems (including chimneys) which require approval under pollution control legislation or biomass boilers or biomass-fuelled CHP plant; centralised boilers or CHP plant burning other fuels within or close to an air quality management area or introduce relevant combustion within a smoke control area
- **Expose people to existing sources of air pollutants.** This could be

by building new homes, workplaces or other development in places with poor air quality

- Give rise to potentially unacceptable impact (such as dust) during construction for nearby sensitive locations
- **Affect biodiversity.** In particular, is it likely to result in deposition or concentration of pollutants that significantly affect a European-designated wildlife site, and is not directly connected with or necessary to the management of the site, or does it otherwise affect biodiversity, particularly designated wildlife sites

Further guidance on how these considerations may be quantified as detailed in chapter 4 of this document.

The NPPG offers further information on the information that may be required in an air quality assessment and mitigation where impacts are identified which applicants should refer to where necessary.



3. AIR QUALITY IN THE MALDON DISTRICT

3.1. Local air quality

The Maldon District is a largely rural district that currently does not have any declared AQMAs and as a rural district predominantly has good air quality.

However, like many rural districts review and assessment carried out under the LAQM regime has identified areas within towns and villages within the District where local circumstances are likely to give rise to an increase in some of the pollutants subject to AQOs within the legislation. Subsequent monitoring has quantified that risk and gives us the knowledge where there is a significant risk of the AQOs being exceeded and an AQMA being declared if there is a risk to human health at plausible receptor locations.

The review and assessment has identified areas where traffic congestion near to residential dwellings occurs as the main areas within the Maldon District where poor air quality is a risk with specific regards to NO₂ and PM₁₀.

For detailed information on the review and assessment and to check the air quality levels from monitoring sites developers and consultants should in the first instance visit the Essex Air Quality website for up to date information on how pollutant levels measure against the AQOs in and around the District. This website also contains an archive of air quality review and assessment reports, published to date.

Maldon District Council has also documented an Air Quality Action Plan that developers and consultants should be aware of. This is included in APPENDIX 2.

In keeping with the planning principles and national air quality strategies, Maldon District Council intends to sustain or improve air quality through the implementation of the Local Plan and when making decisions on development management. To do this it will be necessary to require air quality assessments or mitigation for certain developments.

Where an air quality assessment is identified as being required, it is recommended that developers or their consultants contact Maldon District Council's Environment Services to check that they have obtained the latest reports and monitoring data and have correctly identified the areas requiring assessment due to the potential effects of the development.

3.2. Areas at risk

As stated previously the main cause elevated air pollutants in the district are caused by road traffic. The ongoing LAQM assessment has determined that PM₁₀ monitoring is not currently necessary although continues to need consideration. However NO₂ levels have been monitored using diffusion tubes which have identified areas where NO₂ is significantly elevated above background levels

Background levels of annual mean NO₂ concentrations in 2013 were reported by DEFRA as ranging between 9 and 12 ug/m³ in the Maldon District. The following monitoring sites in TABLE 1 have found annual mean levels of NO₂ above 30 ug/m³ which are significantly above normal background levels and are within 75% of the AQO.

TABLE 1 – Monitoring sites exceeding 30 ug/m³

Site No.	Location
MD 1	Opposite Cherry Oak A414
MD 2	A414 Spital Road / A414 Bypass
MD 3	Heybridge Approach
MD 4	Heybridge Street / The Causeway
MD 5	Colchester Rd / Heybridge Street Junction
MD 6	High Street (Market Hill Junction)
MD 7	Wantz Rd / High St
MD11	Latchingdon Street
MD12	A414 Spital Road / A414 Bypass
MD13	Limebrook Way / A414 Bypass
MD15	The Causeway, Heybridge

The above sites are of particular concern where there is risk of extra vehicle movements or pollutant emitting process such as CHP scheme being developed. Consideration would also need to be given if new receptors are introduced.

The areas given are examples of current sites being identified and monitored. It should be noted that other areas may require assessment should traffic flows change and that LAQM is an ongoing process which may identify new areas requiring monitoring. It is important that the up-to-date air quality reports and monitoring results are used at the time an application is being made and not this document.

Developers will also need to consider areas in other neighbouring districts that may be affected by traffic generated from the development. In particular Danbury located within Chelmsford City Council's area and Hatfield Peverel within Braintree District Council's area may need consideration.

It is important to note that vehicle movements may be generated by residential, retail or industrial sites that are proposed further afield than the area of concern. It will be necessary to consult traffic models to identify where this may be.

A further point developers should consider is the geographical context and local transport infrastructure within the Maldon District which limit the routes that vehicles are likely to travel. For example, developments in Southminster, Burnham and Bradwell are located at the eastern end of a peninsula between two large rivers. There are only two main routes off of this peninsula.

4. WHEN IS AN AIR QUALITY ASSESSMENT REQUIRED?

4.1. The need for an assessment

With the national requirement to build more residential homes, significant development is going to take place. Very large individual development sites or the cumulative impact of major development sites (as defined in the Town and Country Planning Act) pose a risk of parts of Maldon District failing to meet the AQOs after site occupation, affecting the health and quality of life of current and future residents.

We encourage any applicant that holds pre-application meetings to consider the need for air quality assessment at this early stage,

It will therefore be necessary for certain developments to provide either a screening assessment or detailed assessment. These developments should consider the assessment criteria checklist in section 4.2 of this guidance. The criteria listed are the first stage of the assessment process and provide screening to determine whether a more detailed assessment is required.

4.2. Assessment Criteria Checklist

The assessment criteria used in the checklist below are derived from Environmental Protection UK's (EPUK) and Institute of Air Quality Management (IAGM): Land-Use Planning & Development Control: Planning for Air Quality (2015). Where any of the criteria set below are met by the proposed development an air quality assessment should be undertaken.

CHECKLIST 1: Screening Checklist

	Screening Checklist	YES	NO	Recommendations
1	Is the proposed development within an air quality management area (AQMA)			If any answer is YES Go To Checklist 2. If all are NO, no further assessment required
2	Is the proposed development categorised as a major development *			
* Major developments are defined by the Town and Country Planning (Development Management Procedure) Order (England) 2010				

CHECKLIST 2: Air quality and emissions mitigation assessment checklist

	Assessment Checklist	YES	NO	
1	Is the proposed development within or likely to impact upon an air quality management area (AQMA) or an area near to exceeding AQ Limits?			If any answer is YES contact MDC Environment Services to confirm that an air quality (AQ) assessment is required and then undertake an emission's assessment
2	Does the development require an EIA?			
3	Is the proposed development type likely to become either on its own or as part of several cumulative developments a large scale major development?			
4	Is vehicle parking in the development >100 (outside AQMA) or >50 (within AQMA)?			
5	Does the proposal result in an increase in vehicle trip generation within the local area which will lead to an increase or decrease in traffic volumes (annual average daily traffic (AADT) or peak traffic flow) of 5% on roads with 10,000 AADT or more, or 5,000 or more if narrow and congested?			Emissions mitigation assessments are discussed in section 6.
6	Does the proposal result in change in average vehicle speed by more than 10kph (6.2mph) than is present on the existing local road network on roads with 10,000 AADT or more (5,000 if narrow and congested)?			
7	Does the proposal result in a likely increase in heavy duty vehicle movements >15 per day?			If all answers are NO, Or Environment Services determine there is no need for an AQ assessment go to section 6 of this guidance
8	Does the proposal result in increased congestion – this will vary according to location, but can generally be considered to be where there is a build-up of traffic preventing efficient movement?			
9	Will the development introduce new sensitive receptors into or in close proximity to an AQMA or area near to exceeding the AQ limits?			
10	Is the site a Local Development Plan site?			
11	Will the development lead to new infrastructure such as rail, road, roundabout, signalling, bridges etc?			
12	Will the development create a street canyon or enclose roads and reduce dispersion of pollutants?			
13	Does the proposal include biomass energy or heating plant >50kW into an urban residential environment?			
14	Is the proposal in or close to sensitive designated sites such as Special Protection Areas, Special Areas of Conservation (SAC), Ramsar sites, designated ecological sites or Sites of Special Scientific interest (SSSI), County Wildlife Sites, Local Nature Reserves, Roadside Nature Reserves?			

A printable version of this table is included at APPENDIX 3.

5. AIR QUALITY ASSESSMENT CONTENT

5.1. Screening assessment

If the results of the screening assessment or Environment Services determine that no detailed assessment is necessary this should be provided with the application details.

Evidence should be provided as appropriate to show that the criteria in Checklist 2 (See Section 4.2) are not met.

In addition you should include an Emission's Mitigation Statement. Mitigation considerations which should be included in the statement are discussed in more detail in Section 6.

5.2. Detailed assessment

A detailed assessment should be carried out with regard to the guidance provided by Environmental Protection UK and the Institute of Air Quality Management: Land-Use Planning & Development Control (2015): Planning For Air Quality. Chapters 6 and 7 specifically deal with the assessment content, methodology and interpreting the significance of the results.

Further reference should be made to DEFRA's LAQM Technical Guidance TG (16) (also referenced in the EPUK guidance) and any other relevant guidance or standards.

NOTE: Applicants intending to undertake an air quality assessment should always seek the latest available information on local air quality from the local authority.

The assessment should show 'with' and 'without' comparisons and the primary goal will be to determine in the first instance whether the assessment shows an increase in the pollutants being assessed and secondly whether the increase will cause the AQOs to be exceeded and therefore cause an adverse impact on public health.

5.3. Determining the significance on air quality from development

- The AQ assessment provides modelled predicted concentrations for scenarios (for agreed year/period): without development (baseline, with development and with development including mitigation measures, including mitigation measures
- A comparison of the scenarios will be presented in the report. Compare scenarios 'without development (baseline)' with scenario 'with development including mitigation' (or without mitigation if none is considered necessary)

- The difference in the compared scenarios is used to determine the classification of the change in air quality concentration
- The scale of air quality impact due to changes of concentration or if the additional concentration causes local exposure to approach or breach the air quality objectives, determined the planning recommendations
- Planning recommendations are then required

The conclusions made on the significance of air quality impacts identified in a detailed assessment will be based on the professional judgement of officers reviewing the report, the factors that officers will consider are outlined in the EPUK/IAQM guidance and determined whether significance is minor, moderate or major.

The following table sets guidance on the classification of impacts to determine their significance.

Classification of impact	Pollutant concentration change due to development	OR if development contributes to
Very high	Increase >10%	Breach of air quality objective (AQO)
High	Increase > 5-10%	Exposure within 5% of AQO
Med	Increase 1-5%	Exposure within 10% AQO
Low / Imperceptible	Increase <1%	Exposure within 25% of AQO

5.4. Planning recommendation

If the air quality assessment determines an impact or likely impact on air quality in the District due to the development or a cumulative impact of developments, the following outlines the likely recommendations that will be made to the planning authority by Environment Services.

- An overriding consideration will be to ensure that pollutant levels do not increase above the AQOs and therefore cause a new AQMA to be created
- Refusal of a planning application may still be recommended if High to Very High air quality impacts from a development remain, even after all reasonable means to mitigate the impact on air quality have been exhausted

Magnitude of air quality impact		Very high
Recommendation	Require mitigation to remove very high quality impacts. If Impact is still high there will be a strong presumption for recommending refusal. LDP sites will be expected to explore all reasonable measures of mitigation which may include infrastructure improvements relating to road traffic. Agreed measures must be implemented in an agreed time scale	
Action	Recommend refusal if not an LDP site	
Magnitude of air quality impact		High
Recommendation	Recommend refusal unless appropriate mitigation measures are implemented to the satisfaction of the local authority Mitigation to include reducing exposure through various measures, emissions reduction and/or development design	
Action	Recommend refusal unless recommended mitigation is maximised	
Magnitude of air quality impact		Medium
Recommendation	Seek mitigation to reduce air quality impacts Mitigation to include reducing exposure through various measures, emissions reduction and/or development design Contribution Based on Emission mitigation calculation	
Action	Ensure On-site mitigation are maximised	
Magnitude of air quality impact		Low/imperceptible
Recommendation	Recommend the minimum mitigation for development scheme type, or Contribution based on emission mitigation calculation	
Action	Recommend minimum mitigation	

6. AIR QUALITY AND EMISSIONS IMPACT MITIGATION

Where an impact is found to occur it does not automatically mean that a planning application will be refused. However, it is likely that mitigation will be necessary if the development is approved through the planning process. The type of mitigation will depend upon the significance of the impact and the details of each application.

All major developments require a brief mitigation statement and the developer will be required to follow The Control of Dust and Emissions from Construction and Demolition, Best Practice Guidance to minimise dust and other emissions to the atmosphere during the construction phase

The mitigation statement should include:

- Development traffic input data for emissions calculation
- Emissions calculation and totals
- Mitigation proposed (should be at least to the equivalent value of emission's calculation)

6.1 Emissions calculator

The calculation uses the most current DEFRA Emissions Factor Toolkit to estimate the additional pollutant emissions from a proposed development. (Ref: DEFRA Emissions Factor Toolkit: <http://laqm.defra.gov.uk/review-and-assessment/tools/emissions.html>)

This will provide the relevant pollutant emissions outputs for the mitigation calculation, which is then multiplied to provide an exposure cost value.

This value is used for costing the required emissions mitigation for the development.

The emissions assessment and corresponding mitigation calculation follows this process:

1. An emissions assessment calculates additional trips generated by the development.
2. The emissions are calculated for pollutants of concern (NO_x & PM₁₀).
3. **Using DEFRA IGCB Air Quality Damage Costs for the specific pollutant emissions, the calculation then provides a resultant damage cost calculation. Details are provided on the government website at <https://www.gov.uk/air-quality-economic-analysis#damage-costs-approach>**
4. The emissions total is then multiplied x 5, to provide a 5 year exposure cost value*.

5. In addition the health values are to be uplifted by 2% per year**.
6. The resulting 5 year exposure cost value, is the value that is to be used to implement mitigation measures within the development. These mitigation measures should be agreed with the local planning authority to ensure that mitigation is in line with local policy and is appropriate for the type, size and location of the development.
7. If some or all mitigation measures cannot be accommodated within the development then mitigation may be provided through compensation via conditions or section 106 contributions. This will be determined by the local planning authority.

Calculating emissions from alternative fuels and technologies

The emissions calculator (above) provides a basic emission calculation. However, if a development proposal is to include alternative fuels or technology i.e. LPG, EV etc, then there are 'advanced options' within the EFT to accommodate this. Always check in advance with the air quality officer to agree these options.

*COMEAP (2010) and DEFRA Impact pathway guidance for valuing changes in air quality – section 44. (See reference section)

**DEFRA Impact pathway guidance for valuing changes in air quality - section 52. (See reference section)

EXAMPLE EMISSIONS CALCULATION

The following simple example demonstrates the calculation based on a development with 10 domestic properties.

EFT input:

10 Household (urban not London) (2012) (NOx and PM10)

X 27 (trip/traffic ratio for 10 houses)

X cars only (0% HGV)

X 50 kph (avg. speed)

X 10km (NTS UK avg.)

EFT Output = 32.55 kg/annum (NOx) & 3.795 kg/annum (PM)

= 0.0325 tonnes/annum (NOx) & and 0.003795 tonnes/annum (PM10)

X *£955/tonne (NOx) + *£48,517/tonne (PM10)

= £31.08 + £184.15

X 5 (years)

= £155.42 + £920.76

Total = £1,076

6.2 Requirements for mitigation measures

Scheme mitigation should be provided within the design of the development where possible. TABLE 1 lists the mitigation options to be considered.

TABLE 1. **Mitigation options**

- EV recharging infrastructure within the development (wall mounted or free standing in-garage or off-street points).
- Car club provision or support to local car club/eV car club.
- Designation of parking spaces for low emission vehicles.
- Differential parking charges depending on vehicle emissions
- All commercial vehicles should comply with either current or previous European Emission Standard.
- Fleet operations should provide a strategy for considering reduced emissions, low emission fuels and technologies.
- Use of ultra low emission service vehicles.
- Support local walking and cycling initiatives
- On-street EV recharging
- Contribution to low emission vehicle refuelling infrastructure
- Low emission bus service provision or waste collection services
- Bike/e-bike hire schemes
- Contribution to renewable fuel and energy generation projects
- Incentives for the take-up of low emission technologies and fuels

The above list is not exhaustive and further options may be suggested where authorities feel it is appropriate, depending on the scale of development and air quality issues within an area.

The mitigation options selected for a development should be relevant and appropriate to:

- Any local policies including Air Quality Action Plans, which may determine the mitigation priorities for a scheme that the local authority may wish to see be incorporated within a particular scheme
- Any local air quality concerns; to assist in the remediation of potential cumulative air pollution impacts of the development on the local community
- The type, size and activity of the development

DEFRA's 2010 Low Emissions Strategies Guidance provides further guidance on potential mitigation measures.

The following list taken from the EPUK guidance provides some examples of measures that may be appropriate.

CONSTRUCTION PHASE MEASURES

1. General commentary (where applicable)

- Agree a Code for Construction Practice with LPA prior to work commencing

2. Control of dust

- Building enclosures; use of screens; sheeted vehicles;
- Early implementation of paved haul routes
- Hard-standing cleaning
- Water spraying; wheel washing
- Consideration of location of stockpiles, stone-cutting activity; designated storage areas;
- Diversion routes
- Prohibit fires
- Just-in-time deliveries

3. Monitoring strategies

- Site boundary monitoring pre-development and post-development (at closest receptor)
- Liaison meetings with local residents
- Considerate contractor schemes (and their equivalent)

4. Construction plant emissions

- Age and type of plant
- Plant maintenance
- Alternative fuel use

OPERATIONAL PHASE MEASURES

1. Transport related measures

- Travel plans
- Car clubs
- Incentives for increased public transport use discounted fares, provision of information
- Parking standards set maximum number of spaces
- Preferential parking for low emission or car club vehicles or graduated parking
- Charges based on emissions
- Provision of alternative fuels electric charging points or biogas facilities
- Public transport fleet improvements e.g. provision of low emission buses
- Service vehicles agreement to achieve specified emissions standards
- Contribution to specific traffic management or road schemes

2. Non-transport related measures

- Monitoring programme (development specific). Needs careful consideration as to the usefulness of the specific monitoring programme, relevant assessment levels and the action that could be taken if the assessment levels are breached
- Ventilation. Mechanical ventilation becoming increasingly common. Care required on location of inlet. Need to ensure long-term maintenance
- Contribution to action plan and monitoring programmes. Can be a financial contribution to help the Authority develop and implement its action plan. May be a contribution to the Authority's air quality monitoring programme
- Buffer zone. Can be useful, but not simple to define extent. Not always practicable. May need to set against other benefits of development

3. Building design

- Flatted blocks and balconies may be best avoided in locations of poor air quality, especially at ground and first floors
- Habitable rooms. Consider placing away from façade fronting pollution source, e.g. in flats put corridors, stairwells, bathrooms etc. in these locations
- Avoid canyon streets or creating canyons. Creating gaps in building facades can help ensure free flow of air in the street

APPENDIX 1: AIR QUALITY OBJECTIVE LEVELS

Pollutant	Air Quality Objective		Date to be achieved by
	Concentration	Measured as	
Benzene	16.25 µg/m ³	Running annual mean	31.12.2003
	5.00 µg/m ³	Annual mean	31.12.2010
1,3-Butadiene	2.25 µg/m ³	Running annual mean	31.12.2003
Carbon monoxide	10 mg/m ³	Running 8-hour mean	31.12.2003
Lead	0.50 µg/m ³	Annual mean	31.12.2004
	0.25 µg/m ³	Annual mean	31.12.2008
Nitrogen dioxide	200 µg/m ³ not to be exceeded more than 18 times a year	1-hour mean	31.12.2005
	40 µg/m ³	Annual mean	31.12.2005
Particulate Matter (PM ₁₀) (gravimetric)	50 µg/m ³ , not to be exceeded more than 35 times a year	24-hour mean	31.12.2004
	40 µg/m ³	Annual mean	31.12.2004
Sulphur dioxide	350 µg/m ³ , not to be exceeded more than 24 times a year	1-hour mean	31.12.2004
	125 µg/m ³ , not to be exceeded more than 3 times a year	24-hour mean	31.12.2004
	266 µg/m ³ , not to be exceeded more than 35 times a year	15-minute mean	31.12.2005

APPENDIX 2: MALDON DISTRICT COUNCIL AIR QUALITY ACTION PLAN

Air Quality Action Plan 2016

Air pollution is associated with a number of adverse health impacts. It is recognised as a contributing factor in the onset of heart disease and cancer. Additionally, air pollution particularly affects the most vulnerable in society: children and older people, and those with heart and lung conditions. There is also often a strong correlation with equalities issues, because areas with poor air quality are also often the less affluent areas.

Traffic emissions are the most significant source of air pollution in Maldon District and the A414 is the principal route within the District.

The Council recognises the importance of working with partnering authorities such as Essex County Council to make improvements to local transport infrastructure and also to fulfil its own regulatory responsibility towards industrial processes.

Air pollution is considered to be generally low in Maldon District and monitoring of local air quality has measured no exceedances of air quality objective at relevant exposure. The trend of results across all monitored sites indicates that air quality is improving.

Air quality in Maldon District meets the national air quality objectives. As such, Maldon District Council does not have an Air Quality Strategy or Action Plan. However, future development is expected within the District and the emerging Local Development Plan will seek to use policy to support growth within the environmental limits.

To fulfil the requirements of Local Air Quality Management (LQMA) as set out in Part IV of the Environment Act 1995, an Annual Status Report (ASR) is prepared and published on the Council's website. The Council does not have any air quality management areas, so no formal action plan is required.

This document aims to provide an overview of the actions Maldon District Council intends to take to ensure the District continues to benefit from good air quality. Set out below are eight broad actions indicating what is planned to try to achieve this:

ACTION 1

Securing measures to reduce vehicle journeys and therefore reduce vehicle exhaust emissions via the major planning applications. This includes new and enhanced public transport links, cycle networks and the installation of EV charging points (to provide an infrastructure to encourage future ULEV use and therefore reduce exhaust emission).

In particular the planned garden suburb developments are funding a new bus service to Chelmsford and creating a cycle path network linking up to the existing settlement to provide viable alternative to road vehicles. Relief roads have also been secured to reduce congestion in parts of Maldon District.

ACTION 2

Continue to monitor nitrogen dioxide at numerous locations around the District. This is done on a monthly basis using diffusion tubes. The results are not as accurate as the real-time methods; however, three diffusion tubes are co-located at Morrison's roundabout on the A414 which allows us to bias correct the tubes. Annual results of the tube concentrations are published monthly on www.essexair.org.uk. A review of the existing tube locations was undertaken in December 2016.

ACTION 3

Use the procurement system to ensure that air quality is a consideration within contracts for Maldon District Council.

ACTION 4

Work with Public Health colleagues to inform the public about health impacts of air pollution and how they can change behaviour to reduce emissions and reduce exposure.

ACTION 5

Continue to actively work with operators of industrial processes that ensure that permit conditions are appropriate for the operation and they meet compliance.

ACTION 6

Local air quality management guidance recommends that every six years a District review is completed to identify any new industrial processes that are operating without a permit are regularised to ensure emissions to air are controlled. This work is ongoing.

ACTION 7

Encouragement of staff to car-share including the provision of a car sharing space in the staff car park.

ACTION 8

Explore grant options and the Council's appetite for the installation of electric charging points in two strategic locations within the District. By providing these points it will provide and encourage accessibility to both residents of Maldon District and visitors.

APPENDIX 3: ASSESSMENT CRITERIA CHECKLIST

CHECKLIST 1: Screening Checklist

	Screening Checklist	YES	NO	Recommendations
1	Is the proposed development within an air quality management area (AQMA)			If any answer is YES Go To Checklist 2. If all are NO, no further assessment required
2	Is the proposed development categorised as a major development *			
* Major developments are defined by the Town and Country Planning (Development Management Procedure) Order (England) 2010				

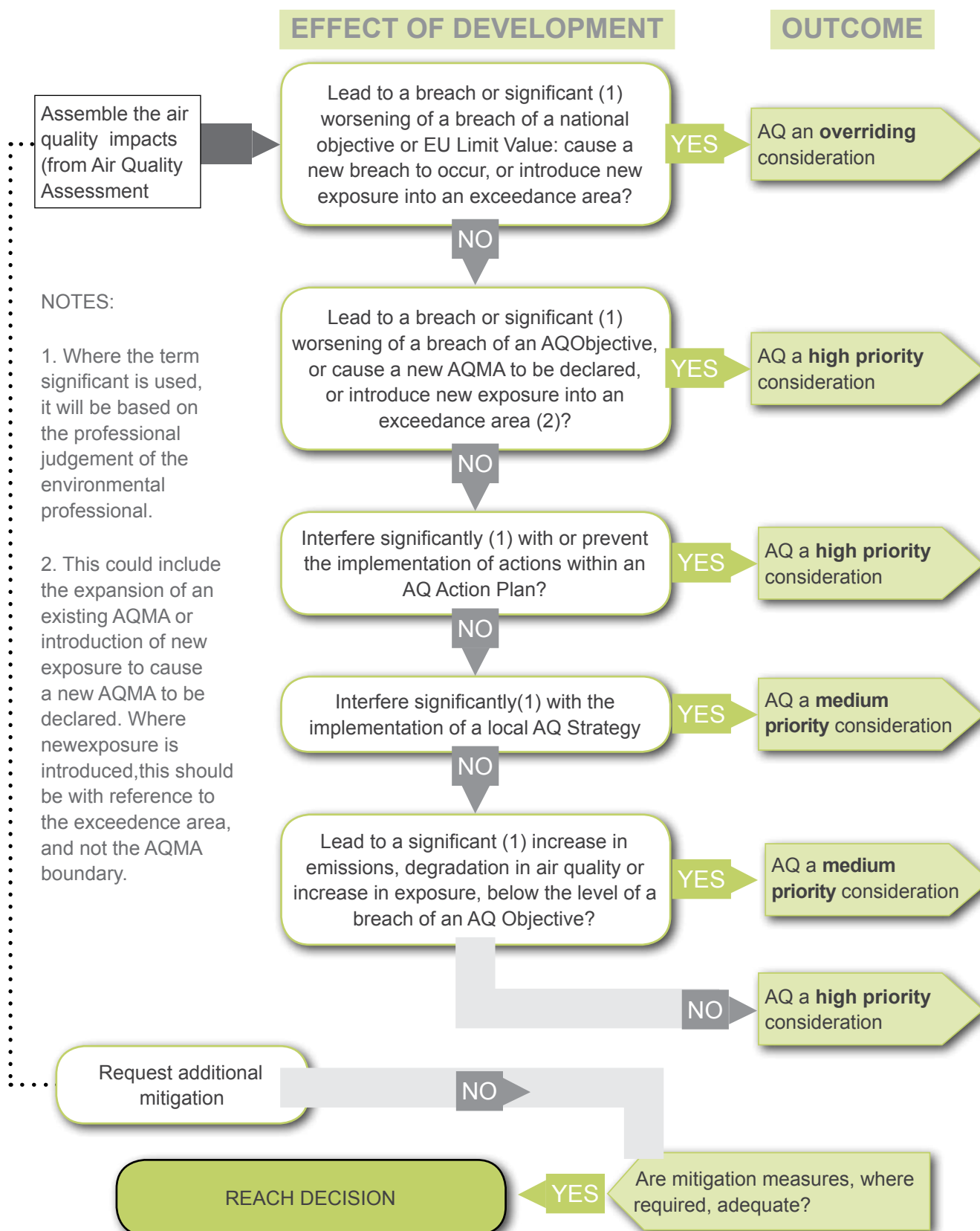
CHECKLIST 2: Air quality and emissions mitigation assessment checklist

	Assessment Checklist	YES	NO	Recommendations
1	Is the proposed development within or likely to impact upon an air quality management area (AQMA) or an area near to exceeding AQ Limits?			If any answer is YES contact MDC Environment Services to confirm that an air quality (AQ) assessment is required and then undertake an emissions assessment
2	Does the development require an EIA?			
3	Is the proposed development type likely to become either on its own or as part of several cumulative developments a large scale major development?			
4	Is vehicle parking in the development >100 (outside AQMA) or >50 (within AQMA)?			Emissions mitigation assessments are discussed in section 6. If all answers are NO, Or Environment Services determine there is no need for an AQ assessment go to section 6 of this guidance
5	Does the proposal result in an increase in vehicle trip generation within the local area which will lead to an increase or decrease in traffic volumes (annual average daily traffic (AADT) or peak traffic flow) of 5% on roads with 10,000 AADT or more, or 5,000 or more if narrow and congested?			
6	Does the proposal result in change in average vehicle speed by more than 10kph (6.2mph) than is present on the existing local road network on roads with 10,000 AADT or more (5,000 if narrow and congested)?			
7	Does the proposal result in a likely increase in heavy duty vehicle movements >15 per day?			



8	Does the proposal result in increased congestion – this will vary according to location, but can generally be considered to be where there is a build-up of traffic preventing efficient movement?			
9	Will the development introduce new sensitive receptors into or in close proximity to an AQMA or area near to exceeding the AQ limits?			
10	Is the site a Local Development Plan site?			
11	Will the development lead to new infrastructure such as rail, road, roundabout, signalling, bridges etc			
12	Will the development create a street canyon or enclose roads and reduce dispersion of pollutants?			
13	Does the proposal include biomass energy or heating plant >50kW into an urban residential environment?			
14	Is the proposal in or close to sensitive designated sites such as Special Protection Areas, Special Areas of Conservation (SAC), Ramsar sites, designated ecological sites or Sites of Special Scientific interest (SSSI), County Wildlife Sites, Local Nature Reserves, Roadside Nature Reserves?			

APPENDIX 4 - STEPS FOR LOCAL AUTHORITY TO ASSESS THE SIGNIFICANCE OF AIR QUALITY IMPACTS OF A DEVELOPMENT PROPOSAL



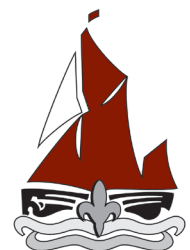
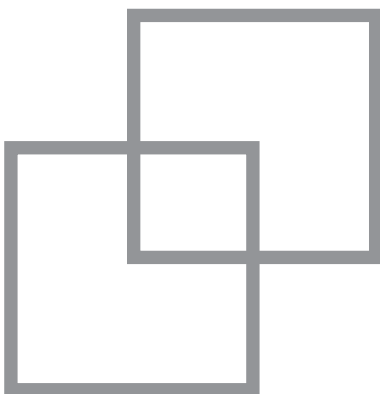
Assessing Air Quality and Emissions Impacts from Development

Technical Document to
Maldon District Design Guide
February 2017

If you need help with this information in a different format, please call 01621 854477

Maldon District Design Guide

Designing for Older Persons' Housing



MALDON DISTRICT
COUNCIL

**Technical Document to
Maldon District Design Guide
February 2017**



CONTENTS

1. Introduction

2. Policy context

3. Types of older persons' housing

4. Applying design guidance to new development

5. Principles of design and outside space



1. Introduction

1.1. The purpose of this guidance

This document provides further guidance on the design of older persons' housing in the District. It draws on relevant planning policy and published guidance on the design of older persons' housing and forms part of the suite of technical documents that support the Maldon District Design Guide (MDDG).

Design is a material consideration within the planning process. This document provides guidance on how to ensure the design of new development meets the specific needs and aspirations for older people within the District.

The objective is to provide a better quality of life, health and well-being for older people and to ensure development within the Maldon district delivers the appropriate type of housing that meets projected needs. New developments, ranging from retirement housing to extra care housing, should be appropriately located and designed to be flexible, capable of adaptation, inclusive and accessible.

Reference should also be made to the Older Persons' Housing Strategy (December 2013) and the Maldon Housing Strategy (currently in preparation) which provide further information on the District's housing needs with particular reference to the Strategic Housing Market Assessment (SHMA).

This guidance should be read in conjunction with the Maldon District Design Guide (MDDG) which relates to all types of housing and should be used to inform design development and the preparation of planning applications.

1.2 The importance of good design

Maldon District is a predominately rural district with an ageing population, therefore, design is paramount in the delivery of appropriate housing to meet the needs of older persons. Surveys undertaken to inform the Older Persons' Housing Strategy identified that 70% of those surveyed would seek single level living, close to bus routes or shops, parking and gardens and identified design and location as important factors in the choice of future homes.

This guidance builds on the principles set out in the MDDG to ensure that people have the opportunity to live in a property that continues to meet their needs as they grow older and where they are not unduly restricted due to location or mobility.

By setting standards for future developments, and working in partnership with stakeholders, the District should see an increase in the choice available to older people looking to move whether this is to alternative general needs housing or to homes intended specifically for older people.

2. Policy context

2.1. National Planning Policy Framework (NPPF)

2.1.1 What is an older person?

The NPPF recognises the older person and provides the following definition (Page 54 (Glossary)):

“People over retirement age, including the active, newly-retired through to the very frail elderly, whose housing needs can encompass accessible, adaptable general needs housing for those looking to downsize from family housing and the full range of retirement and specialised housing for those with support or care needs”. However, there is no age bracket that is specific to an ‘older person’.

2.1.2 Paragraph 159 states the local planning authority should:

“prepare a Strategic Housing Market Assessment (SHMA) to assess their full housing needs, [which].....should identify the scale and mix of housing and the range of tenures which ... addresses the need for all types of housing, the needs of different groups in the community (such asolder people.....)”

2.1.3 Paragraph 50 states local planning authority should:

“plan for a mix of housing based on current and future demographic trends, market trends and the needs of different groups in the community (such as older people.....)”

Running parallel with the requirement to deliver the appropriate mix of housing, and stated throughout the NPPF, is the core principle of securing high quality and inclusive design.

2.1.4 Paragraph 56 states:

“The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people to live”.

The concept of using design codes to help in the delivery of high quality outcomes is set out at paragraph 59

2.1.5 Paragraph 7 - Sustainable development

The planning system strives to deliver sustainable development which includes the need to perform a social role:

“supporting strong, vibrant and healthy communities, by providing the supply of housing required to meet the needs of present and future generations; and by creating a high quality built environment, with accessible local services that reflect the community’s needs and support its health, social and cultural well-being”

2.1.6 **Section 8** recognises:

“the planning system can play an important role in facilitating social interaction and creating healthy, inclusive communities”, particularly important to ensure older persons remain integrated within the local community..

2.2 Local Development Plan (LDP)

2.2.1 **Policy D1 – Design Quality and Built Environment**

Policy D1 sets out the criteria for high quality design to ensure that:

“new development is visually attractive, responsive to local character, helps to promote healthy communities, and creates buildings which are durable, adaptable, and function well within the surrounding area to create a safe and accessible environment”.

Paragraph 3.4 sets out the Council’s expectation that:

“all development [should] support the principles of inclusive design [and] ensure that new development can be used by all people. Design should also seek to reduce social exclusion within the District, and seek to improve people’s access to housing,required services and facilities.”

This criteria is reflected in the design principles set out in the MDDG and also forms the foundation for this guidance.

2.2.2 **Policy H2 - Housing mix**

Policy H2 states that:

“all developments will be expected to provide a suitable mix and range of housingparticularly the need for an ageing population”.

Paragraph 5.16 states:

“...The Council will require new development to incorporate a range of different housing types which contribute towards meeting the identified housing needs for different demographic groups in the District. ...The Council will also encourage development proposals which seek to meet the housing need of older people”

2.2.3 Policy H3 - Accommodation for ‘specialist’ needs

Policy H3 states that: *“Proposals for specialist needs housing such as homes for older people, people with disabilities, or homes for other specific groups who may require properties that are specifically designed and/or allocated will be supported ”*

Paragraph 5.21 states:

“Specialist housing may be required, but not limited to, meeting the needs of older people This could be achieved by providing a range of different types of housing including sheltered housing with care support, staffed hostels, residential care homes, wheelchair accessible housing or housing that is easily adaptable for wheelchair use, and generally homes for older persons ...”

2.2.4 Policy T2 – Accessibility

Policy T2 seeks to create an accessible environment for everyone and will include

“improving accessibility to buildings, streets and public spaces, particularly those with mobility impairments”

The LDP acknowledges that the ability of people to access key services and facilities can make an important contribution to the health and well-being of communities of all age groups.

3. Types of older persons’ housing

3.1 Housing categories

- 3.1.1 As discussed in section 2 above, the NPPF identifies older persons as people over retirement age. Without discussing this description in detail, it is acknowledged that ‘retirement age’ is flexible and can be extended to include the 55+ age group through to end of life.
- 3.1.2 As such, provision of housing for the elderly should be designed to meet all ages and abilities within this age group and should provide choices that allow them to be accommodated within their family home or move to a new home to meet their future needs as they grow older.

3.1.3 There are a number of different types of accommodation which fall within the category of older persons' housing. This guidance is applicable to the broad spectrum of housing provision for the older person and encompasses residential care, including retirement and nursing/care homes, as well as independent living. A document published by the Housing our Ageing Population: Panel for Innovation (HAPPI) in October 2014 categorised the types of housing provision into three groups:

- Mainstream housing
- Specialised housing
- Care homes

3.1.4 The table below sets out these three broad categories of accommodation referred to above and provides a brief outline of their functionality. New developments incorporating homes for older persons should be designed to be functional, adaptable, aesthetically pleasing and well located and should comply with the principles set out in the MDDG.




mainstream housing	specialised housing	care homes
<p>Individual homes to buy or rent - not designated for any specific user group though Lifetime Homes includes age-friendly features and wheelchair housing is specially designed. Personal care, support, other services and amenities available within the community.</p>  <p>Planning: current use class 3 'dwelling houses'</p>	<p>Groups of homes (usually flats) to buy or rent - designated for older people (typically 55+). Personal care and support usually arranged or provided within the development together with shared facilities and activities.</p>  <p>Planning: currently classified as C2 or C3 - would a new class or classes be useful?</p>	<p>Residential care rather than independent living.</p>  <p>Planning: current use class 2 'residential institution'</p>

FIGURE 4.1 "Mainstream, Specialised and Care-Home Housing: The Happi Spectrum (Add Footnote)

SOURCE: Ippr – For Future Living – Innovative Approaches to Joining Up Houses and Health

(Author: Bill Davies; Published 23/10/2014)

3.2 Use Class Categories

- 3.2.1 It is also helpful to understand how the different types of housing for older persons is differentiated in planning terms. The Town and Country Planning (Use Classes) Order 1987 (as amended) puts uses of land and buildings into various categories known as 'Use Classes'. There is some discussion on whether the older persons' housing would fall into Use Class C2 or Use Class C3. This is dependent on the type of housing and, for reference purposes, details of the categorisations are outlined below.

3.2.2 **Class C2: Residential institutions**

These include, amongst others, residential care homes, hospitals and nursing homes.

3.2.3 **Class C3: Dwellinghouses**

This class is made up of 3 parts, the relevant class being Class C3(a) which covers use by a single person or a family, domestic employees, a carer and the person receiving the care. In practical terms, the conventional concept of a self-contained home where people can live independently and exclusively within their own property, even though in the case of some elderly persons' housing, there may be communal facilities outside of their own personal property.

SOURCE: https://www.planningportal.co.uk/info/200130/common_projects/9/change_of_use

- 3.2.4 The provision of accommodation for the older person 'crosses' both Use Class C2 and Use Class C3. Whilst Class C3 comprise independent housing units, Class C2 is managed in a very different way and people do not have the same independence as they would in their own home. It is possible that on some sites there may be an overlap between the different categories, with some people occupying homes as owners, tenants or leaseholders (C3) alongside others who are accommodated on a more temporary basis in order to receive care or treatment (C2). This may be the case for extra-care housing with enhanced facilities to meet additional needs of residents if they become more frail, or to provide interim care and support for those recovering and undergoing a period of re-ablement. It is advised that when considering development, applicants should be explicit about the type of housing they intend to provide, including the proposed tenure and where appropriate, the method of allocating homes, and should refer to the HAPPI reports for more detailed information as well as the design guidance set out in the MDDG and this document and any further guidance that may be appropriate.

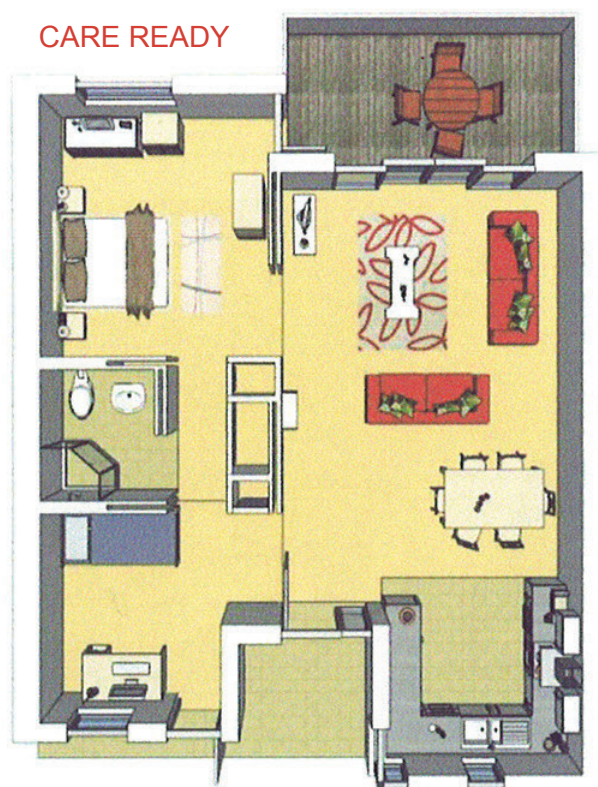
Further details can be found in: Planning Use Classes and Extra Care Housing Viewpoint 20 produced by The Housing Learning and Improvement Network (LIN) <http://www.housinglin.org.uk/AboutHousingLIN/>

4. Applying design guidance to new developments

4.1 What the Maldon District expects

4.1.1 The MDDG requires all residential development to be sustainable, inclusive and accessible, aesthetically pleasing and to promote high levels of social inclusion and meet the needs of residents.

4.1.2 Additionally, in order to provide adequate and sustainable housing for the older person, the development must offer an attractive alternative to the existing family home, and be capable of adaptation over time to meet changing needs. Additional space may be required to reflect the fact that in many cases people will be down-sizing from larger homes and in the future may need space for aids and adaptations. Through good design one-bedroom homes can be built to be better suited to possible future requirements such as the need to have an over-night carer, storage for mobility scooters and space to retain independence.



SOURCE: HCA Homes & Community Agency

4.1.3 The Council expects the design of proposals for elderly persons' housing to ensure a high quality of development which meets the principles set out in the MDDG. In particular, those principles relating to adaptability, and inclusive and accessible design, should take account of relevant guidance including:

- Guidance produced by HAPPI (Housing our Ageing Population: Panel for Innovation)
- Lifetime Homes Standards
- National Space Standards
- Town Planning and Dementia RTPi Practice Advice January 2017
- Inclusive Design for Getting Outdoors

5. Principles of design and outside space

5.1 The Maldon District Design Guide (MDDG)

As set out previously, the MDDG sets out clear principles to guide future development within the District and has been prepared in line with the LDP and NPPF requirements, local studies and strategies. This guidance supports the MDDG and whilst the whole document is applicable to all development, there are a number of elements that are pertinent to the design and delivery of housing for the older person, in particular:

5.1.1 Section A 04 – Key Design Objectives: Adaptability and Quality

Adaptability and Quality underpins the potential to deliver ‘lifelong’ housing by:

- anticipating the need for changes in buildings and outdoor spaces
- providing places that function well today, last for the future and are easy to adapt to changing requirements of occupants and other circumstances at any time, and
- providing buildings that can accommodate changing needs

5.1.2 Section C 16 – Inclusive and accessible design

A number of elements play a significant role in delivering inclusive design and accessibility. This section identifies important features that will guide delivery of housing for the older person, and seeks to ensure that:

- individual dwellings are designed to be flexible, capable of adaptation to meet the changing needs of residents in the future...
- the design of new dwellings takes into account Lifetime Home Standards

5.2 Housing our ageing population: Panel for Innovation (HAPPI)

5.2.1 A study undertaken by HAPPI (2009) identified ten key components for the design of housing for older people. Maldon District Council will expect these to be incorporated in all proposals including the development of elderly persons’ housing:

- Homes should have generous internal space standards
- Design of homes and shared spaces should ensure plenty of natural light in the home and in circulation spaces
- Buildings should avoid internal corridors and single-aspect flats and apartments should have balconies, patios or terraces and outdoor space
- To ensure adaptability, homes should be designed to be ‘care-ready’ to enable emerging technologies, such as telecare and community equipment to be installed

- Building layouts should promote circulation areas and shared spaces that offer connections to the wider context and avoid an 'institutional feel'. Imaginative use of shared balcony access to front doors and thresholds should be included to promote natural surveillance
- Multi purpose space should be made available with appropriate supporting facilities, which could serve the wider neighbourhood as a community 'hub', particularly where they are lacking in the existing community
- Homes should engage positively with the street and the natural environment should be nurtured through new trees and hedges. (further detailed guidance can be found in the Council's Landscape and Green Infrastructure Technical Guidance (February 2017))
- Homes should be energy efficient, well insulated and well ventilated to avoid overheating
- Provision for cycles and mobility aids should be made as well as additional storage both inside and outside the home
- Shared external areas such as 'home zones' that give priority to pedestrians should be considered

SOURCE: <https://www.gov.uk/government/publications/housing-our-ageing-population-panel-for-innovation>

- 5.2.2 A number of these recommendations reflect some of the key factors identified in local surveys undertaken as essential and important by older persons seeking a future home (see 1.2 above).

A further report: Housing our Ageing Population: Plan for Implementation (HAPPI2) Report (2012) provides further advice on improving the design and quality of specialist housing for the older person with the ten core design elements remaining in place

SOURCE: <http://www.housinglin.org.uk/topics/type/resource>

- 5.2.3 A recent report from HAPPI - Housing our Ageing Population: Positive Ideas HAPPI 3 Making Retirement Living a Positive Choice (June 2016) primarily focuses on issues beyond the design and construction of housing for older people. Whilst it explores best practice and service options, it reiterates the ten features identified in the 2009 HAPPI report set out above. This report also provides links to further studies, guidance and case studies.

SOURCE: http://www.housinglin.org.uk/topics/browse/design_building/happi/

5.3 Lifetime homes

- 5.3.1 The Lifetime Homes Standard establishes the principles of good design that maximise utility, independence and quality of life. Whilst Lifetime Homes has not been formally adopted by the Council, developers will be expected to take account of these design principles which provide particular benefits to older people and those with a physical impairment.

The Lifetime Homes design criteria are intended to provide accessible and adaptable accommodation for everyone, and over time, should allow older people to remain in their houses for longer and offer greater choice for independent living. The concept is based on five overarching principles:

- Inclusivity – the flexibility and adaptability within the design will meet a diverse range of needs over time
- Accessibility – inclusive design will provide convenient and independent access into and around the built environment (externally and internally)
- Adaptability – the original design should accommodate future provision for adaptation from the outset
- Sustainability – the accessibility, flexibility and adaptability will ensure long term demand and contribute to the creation of stable and popular neighbourhoods and communities
- Good value – carefully considered design, incorporating provision for adaptation, will give inherent flexibility and provide better value for the wider community in accommodating the changing needs of older people, thus reducing the future need for specialised housing.

SOURCE: <http://www.lifetimehomes.org.uk>

5.4 Technical Housing Standards – nationally described space standard (March 2015) published by Department for Communities and Local Government

- 5.4.1 This document deals with internal space within new dwellings and can be applied across all tenures. Whilst this is referenced for guidance only and has not been formally adopted by the Council, developers will be expected to take these standards into account in the design of new developments.

SOURCE: <https://www.gov.uk/government/publications/technical-housing-standards-nationally-described-space-standard>

5.5 RTPI Practice Advice (January 2017): Dementia and Town Planning - Creating better environments for people living with dementia

- 5.5.1 Whilst this document primarily discusses provision for people living with dementia, it provides useful design guidance on the provision of adaptable housing for the elderly in sustainable locations and the creation of an engaging living environment. The Guidance sets out the following key design principles to be considered in the design of new developments:

- A familiar environment where functions of places and buildings are obvious, and any changes are small scale and incremental
- A legible environment that provides a hierarchy of street types, which are short and fairly narrow with clear signs at decision points
- A variety of landmarks, with architectural features in a variety of styles and materials, e.g. trees and street furniture, to provide distinctions between areas
- Land uses are mixed with shops and services within a 5-10 minute walk from housing, and entrances to places are obvious and easy to use, and conform to disabled access regulations
- Open space is well defined with toilets, seating, shelter and good lighting with minimal street clutter. Background and traffic noise should be minimised through planting and fencing (this is covered in more detail in the Council's Assessing Air Quality & Emissions Impacts from Development, and Planning & Noise Technical Guidance)
- Wide, flat and non-slip footpaths with development orientated to avoid creating dark shadows or bright glare

Further information on this guidance, including links to summaries of expert advice on urban and housing design can be found in

"Dementia and Town Planning – RTPi Practice Advice January 2017 (endorsed by Alzheimers Society)"

SOURCE: <http://rtpi.org.uk/knowledge/practice/dementia-and-town-planning/>

5.6 Inclusive Design for Getting Outdoors (I'DGO)

- 5.6.1 It is recognised that access to safe outside space, with good views from inside the building as well as daily exposure to daylight, improves health. It is particularly important to ensure the design of housing for the elderly encompasses not only the aesthetics of the buildings themselves but also the provision of easy access to the outdoor environment. A well-designed outdoor environment will encourage older people to spend more time outdoors and the "experience" should be designed to meet their particular needs. This guidance supplements the MDDG which sets out the key elements to be included in the design of open spaces including accessibility and integration with the wider community.

Further detail can be found in "Inclusive Design for Getting Outdoors" – A research project funded by the Engineering and Physical Sciences Research Council (2003-2013).

SOURCE: http://idgo.ac.uk/older_people_outdoors

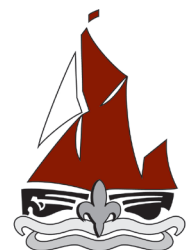
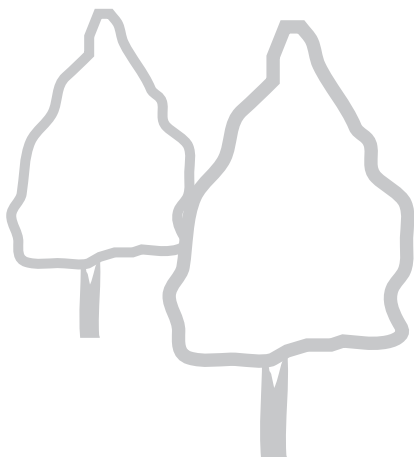
Designing for Older Persons' Housing

Technical Document to
Maldon District Design Guide
February 2017

If you need help with this information in a
different format, please call 01621 854477

Maldon District Design Guide

Landscape and Green Infrastructure (including Open Space, Sport and Play Facilities, Biodiversity and Trees)



MALDON DISTRICT
COUNCIL

**Technical Document to
Maldon District Design Guide
February 2017**



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2. Policy context

3. Landscape structure and design

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1. Introduction

1.1. The purpose of this guidance

This guidance provides direction on the importance for new development of environmental planning with respect to landscape and green infrastructure, including open space, sport and play facilities, biodiversity and trees. It is intended to supplement the information contained in the Maldon District Design Guide, providing further context and technical detail on the topics identified above, and highlighting the planning policy, local evidence documents and other available relevant guidance that can help create sustainable settlements in Maldon.

The aim of this guidance is to ensure that all residents living within Maldon District have access to a wide range of high quality landscapes and green infrastructure features that meet local green space and play standards to facilitate quality of life, health and wellbeing. Additionally, that biodiversity and trees are fully considered at an early stage in the development process and are seen both as an opportunity as well as a constraint in development proposals. This guidance aims to ensure that new development proposals design out negative landscape and green infrastructure effects rather than relying on mitigation to replace lost features.

1.2. Role of landscape and green infrastructure

Landscapes are the result of the action and interaction of natural and human factors and have an important role in the cultural, ecological and social purposes of place, whilst contributing to both economic activity and job creation. Green infrastructure is the network of multi-functional green space and linkages, whether accessible or not, which supports both ecological processes and is integral to the health and quality of life of local communities. Access to high quality open spaces and play facilities are known to contribute to the health and well-being of communities as a whole as well as those of individuals, particularly children. Biodiversity is the variety of life and the heritage of millions of years of evolution. Functioning natural systems are required to sustain biodiversity and successional processes. Trees contribute greatly to both urban and rural character and beauty whilst performing important ecosystem services such as filtering air pollutants, reducing storm event effects and decreasing urban temperatures.

1.3. The landscapes and green infrastructure of Maldon District

The Maldon District landscape consists largely of low-lying, open and tranquil estuary land, with shallow creeks, mud flats, salt marsh, grazing marsh and arable land which abuts the often sharply rising, more wooded ground inland. Green infrastructure (GI) within the District includes a range of natural green space (including the estuaries, designated nature conservation areas and ancient woodlands), man-made or intensively managed green spaces (such as parks, allotments and sports pitches), as well as linear spaces (such as footpaths, cycleways and sea walls). It is identified that the current supply of GI within the District is relatively good overall, although some areas of the District are better provided for than others and as population is projected to rise, new facilities will be needed to maintain the current level of provision.



IMAGE: PAUL HARRISS

2. Policy context

- 2.1. The National Planning Policy Framework (NPPF) recognises the importance of the natural environment including landscape and green infrastructure in providing essential support and opportunities for communities and to mitigate climate change effects.
- 2.1.1. **Paragraph 99** states that *'Local Plans should take account of climate change over the longer term, including factors such as flood risk, coastal change, water supply and changes to biodiversity and landscape... When new development is brought forward in areas which are vulnerable, care should be taken to ensure that risks can be managed through suitable adaptation measures, including through the planning of green infrastructure.'*
- 2.1.2. **Paragraph 109** states *'The planning system should contribute to and enhance the natural and local environment by:*
- *protecting and enhancing valued landscapes, geological conservation interests and soils;*
 - *recognising the wider benefits of ecosystem services;*
 - *minimising impacts on biodiversity and providing net gains in biodiversity where possible...*
- 2.2 **Paragraphs 114, 117 and 118** expand on the need to plan positively for biodiversity and green infrastructure networks and to minimise impacts, and the importance of local landscape character. Paragraph 118 describes a set of principles for the local authority to apply in decision-making.
- 2.2.1 **Paragraph 114:** *'Local planning authorities should:*
- *... planning positively for the creation, protection, enhancement and management of networks of biodiversity and green infrastructure; and*
 - *maintain the character of the undeveloped coast, protecting and enhancing its distinctive landscapes ... and improve public access to and enjoyment of the coast...*
- 2.2.2 **Paragraph 117:** *'To minimise impacts on biodiversity and geodiversity, planning policies should:*
- *plan for biodiversity at a landscape-scale across local authority boundaries;*
 - *identify and map components of the local ecological networks, including ... designated sites of importance for biodiversity, wildlife corridors and stepping stones that connect them and areas identified by local partnerships for habitat restoration or creation;*
 - *promote the preservation, restoration and re-creation of priority habitats, ecological networks and the protection and recovery of priority species populations, linked to national and local targets, and identify suitable indicators for monitoring biodiversity in the plan;*
 - *aim to prevent harm to geological conservation interests...*

- 2.2.3. **Paragraph 118:** *‘When determining planning applications, local planning authorities should aim to conserve and enhance biodiversity by applying the following principles:*
- *if significant harm resulting from a development cannot be avoided... adequately mitigated, or, as a last resort, compensated for, then planning permission should be refused*
 - *proposed development on land within or outside a Site of Special Scientific Interest likely to have an adverse effect on a Site of Special Scientific Interest ... should not normally be permitted...*
 - *development proposals where the primary objective is to conserve or enhance biodiversity should be permitted;*
 - *opportunities to incorporate biodiversity in and around developments should be encouraged;*
 - *planning permission should be refused for development resulting in the loss or deterioration of irreplaceable habitats ...unless the need for, and benefits of, the development in that location clearly outweigh the loss...’*
- 2.3. **Paragraph 170** supports the need for landscape character assessments:
- 2.3.1. **Paragraph 170:** *‘Where appropriate, landscape character assessments should also be prepared, integrated with assessment of historic landscape character, and for areas where there are major expansion options assessments of landscape sensitivity.’*
- 2.4. **Paragraph 125** deals specifically with the role of good design in mitigating light pollution in natural environments:
- 2.4.1. **Paragraph 125:** *‘By encouraging good design, planning policies and decisions should limit the impact of light pollution from artificial light on local amenity, intrinsically dark landscapes and nature conservation.’*
- 2.5. **Paragraphs 73 and 74** identify the importance of the protection and provision of high quality sport, recreation and green space opportunities:
- 2.5.1. **Paragraph 73:** *‘Access to high quality open spaces and opportunities for sport and recreation can make an important contribution to the health and well-being of communities.’*
- 2.5.2. **Paragraph 74:** *‘Existing open space, sports and recreational buildings and land, including playing fields, should not be built on unless:*
- *an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or*
 - *the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or*
 - *the development is for alternative sports and recreational provision, the needs for which clearly outweigh the loss’.*

2.6. Other national policy and guidance

- 2.6.1. **An Approach to Landscape Character Assessment (2014)** (NATURAL ENGLAND) This guidance sets down the process for landscape character assessment to create robust and transparent studies that aid landscape understanding and inform judgements as a result of development management or other change. Landscape Character Assessment can be used to inform policy, place-making, green infrastructure strategies, design briefs, project design and master planning.
- 2.6.2. **Guidelines for Landscape and Visual Impact Assessment, 3rd ed.(2013)** (LANDSCAPE INSTITUTE AND INSTITUTE OF ENVIRONMENTAL MANAGEMENT AND ASSESSMENT) Landscape and Visual Impact Assessment (LVIA) is a separate process that can operate within the framework of Environmental Impact Assessment or be used as an informal standalone tool. In the latter instance it is often referred to as a landscape and visual appraisal and is useful to aid thinking about forms of development. It aims to ensure that all effects on landscape change, views and amenity are considered.
- 2.6.3. **Green Infrastructure Guidance (2009)** (NATURAL ENGLAND) The provision of green infrastructure is recognised as contributing to places where people want to live and work. Natural England's guidance provides a definition and maps out wider policy priorities and drivers for green infrastructure. It outlines the protection for landscape under the European Landscape Convention (2000), which was ratified by the UK in 2012.
- 2.6.4. **Biodiversity 2020: A Strategy for England's Wildlife and Ecosystem Services (2011)** (DEFRA) In June 2011, EU Member States endorsed the European Commission's EU Biodiversity Strategy. Biodiversity 2020 sets out how the UK will address the EU strategy through our own policy. The mission for this strategy is: 'to halt overall biodiversity loss, support healthy well-functioning ecosystems and establish coherent ecological networks, with more and better places for nature for the benefit of wildlife and people'.
- 2.6.5. **Sporting Future: A New Strategy for an Active Nation (2015)** (HM GOVERNMENT) In this strategy, the government recognises that people are active in both urban and rural environments, and that people use a range of facilities including parks, canals, green spaces and skate parks. Access to local urban opportunities is seen as particularly important for those individuals who are currently inactive. This includes capitalising on the development of high quality local access networks, and support for infrastructure that is not restricted to pitches, sports halls and buildings. In future, all types of places where people are active, in both rural and urban environments, will be considered to be part of the active sports network.
- 2.6.6. **Designing and Planning for Play (2008)** (CABESPACE) This guidance document, now promoted by the Design Council, looks at the opportunities for enhancing outdoor play space, not just in designated playgrounds but in school

grounds and the wider public realm. It analyses the failings of standardised equipped playgrounds with often costly off-the-shelf equipment, safer surfacing and fencing. It presents a series of case studies to showcase how creativity from well-qualified professionals can be used to make cost effective but fun facilities.

- 2.6.7. **Cycling and Walking Investment Strategy (2016)** (DEPARTMENT FOR TRANSPORT) This strategy seeks, amongst other things, to double cycling by 2040 and reduce the decline in walking. This includes the ambition to deliver a wider network of walkways, cycle ways and open spaces that let people actively incorporate nature into their lives. The strategy also flags up the ability for local authorities to use the Community Infrastructure Levy for parks and green spaces.
- 2.6.8. **Improving Access to Green Space (2014)** (PUBLIC HEALTH ENGLAND) This health briefing looks at a series of case studies in the UK where new or improved greenspace has been created in order to reduce health inequalities. It recognises the role that greenspace can make to improve health outcomes particularly for the most deprived neighbourhoods.
- 2.6.9. **Trees in Hard Landscapes: A Guide to Delivery (2014)** (TREES AND DESIGN ACTION GROUP) This comprehensive document has been produced by a sector wide group that seeks to raise awareness about the role of trees in the built environment. It identifies practical strategies to ensure trees contribute effectively to the design objectives of a project, technical solutions to their incorporation and thoughts on species selection.

2.7. Local policy and guidance

- 2.7.1. **Maldon District Landscape Character Assessment (2006)** (CHRIS BLANDFORD ASSOCIATES) This study provides a baseline of the landscape character of the district identifying key landscape character types – areas of broadly similar patterns of geology, landform, soils, vegetation, land use, settlement and field systems - broken down into local landscape character areas that reflect variations based on visual analysis of physical and perceptual features such as scale, pattern, tranquillity, cultural associations, etc. Character area profiles describe, for instance, key characteristics, key planning and management issues, sensitivities to change, landscape objectives and guidelines.
- 2.7.2. **Green Infrastructure Study (2011)** (MALDON DISTRICT COUNCIL) The Study (or successor document) identifies the components which make up the District's Green Infrastructure (GI) network, including parks and amenity space, sports provision, natural and semi natural green spaces, and allotments. It investigates the general need for GI in the District, assesses the quantity, quality and accessibility of provision, and draws together recommendations for the future.

The current supply of GI within the District is relatively good overall, although some areas are better provided for than others. As population in the District is projected to increase over the next 20 years new facilities will be required to maintain the current level of provision.

- 2.7.3. **Maldon District Nature Conservation Study (2007) (EECOS)** This report describes the result of a general wildlife survey of the district at a Phase 1 level and evaluates the network of important wildlife sites with a view to reviewing the designation of Local Wildlife Sites. It identifies that Maldon District supports diverse habitats and describes the changes that are affecting priority habitats and species. It describes the role and requirements for wildlife corridors and discusses some potential impacts of climate change.
- 2.7.4. **Maldon District Local Wildlife Sites Review (2007) (EECOS)** This report lists the current Local Wildlife Sites in Maldon District, the changes from the previous study and the criteria used to determine these changes.
- 2.7.5. **Essex Biodiversity Action Plan 2010-2020 (2012) (ESSEX BIODIVERSITY PROJECT)** The Essex Biodiversity Action Plan provides guidance for the 19 Priority Habitats of the Biodiversity 2020 Strategy as well as the list of Priority Species and Habitats provided for in Section 41 of the 2006 Natural Environment and Rural Communities Act. Work on Rivers is being approached through the Water Framework Directive and Catchment Restoration Fund projects. For details see <http://www.essexbiodiversity.org.uk/biodiversity-action-plan>
- 2.7.6. **Maldon District's Children's Play Strategy (2007):** This strategy (or successor document) audited both equipped play space using national standards and carried out a review of the play service and open space. A consultation exercise was carried out with a sample of children and parents in the district. Key play projects were identified for development. The National Playing Fields Association (now Fields in Trust (FIT)) standards are proposed.

3. Landscape structure and design

- 3.1. Section B03 of the Maldon District Design Guide, Landscape Settlement and Character, flags up the importance of landscape character in shaping new or expanding settlements as well as detailed landscape masterplans. The Design Guide identifies that more information can be found in the Maldon Landscape Character Assessment. This document identifies 6 No. Character Types and 24 No. Character Areas. For each Character Area the assessment identifies the key characteristics, overall character, visual characteristics, historic land use and ecological features, as well as the key planning and land management issues, sensitivities to change, strategic objectives and suggested landscape planning and land management guidelines. These objectives and guidelines should be used to inform the development of scheme proposals.
- 3.2. All applications that would impact on the character and visual amenity of the undeveloped coast, countryside or rural fringe, or are otherwise outside the settlement boundaries, should undertake a baseline landscape and visual appraisal including a review of the local and relevant national landscape character assessment, as required in the Maldon District Council Planning Validation Requirements List, in order to understand local landscape context.
- 3.3. Where the impact is judged to need a more rigorous assessment, for instance where the proposal is for major residential development of 10 or more units or where the floor space to be created is greater than 1000m², the application may require instead a formal Landscape and Visual Impact Assessment. These are more detailed technical studies and should be carried out by a suitably qualified landscape professional, in line with current guidelines.
- 3.4. For all applications for major residential developments and all major commercial developments a landscape strategy should be produced. The landscape strategy should cover all public and private spaces including open space, sports pitches and play facilities, green infrastructure, biodiversity habitats, water bodies and landscape buffers, and refer to hard surfaces, boundary treatment and all soft landscaping proposed. The strategy should build on the principles set out in the scheme Design and Access Statement (where relevant). Emerging Neighbourhood Plans and existing Village Design Statements may also give additional detailed local information on local landscape character.
- 3.5. Landscape design should be based on detailed site survey covering landform, topography, aspect, soils, ecology, arboriculture, land use, landscape patterns (such as field boundaries), linkages, water bodies and site drainage, other site features and views. This baseline data should be analysed to provide a site appraisal drawing, and used to inform the overall landscape strategy. Site layout should seek to retain, protect and enhance on site features wherever possible.
- 3.6. The applicant should refer to Section C02-C011 of the main Maldon District Design Guide which outlines factors to consider when designing with the landscape and public realm in mind.

4. Green infrastructure

- 4.1. The National Planning Practice Guidance (NPPG), paragraph 027, recognises green infrastructure as a network of multifunctional greenspace, in both urban and rural areas, capable of delivering a wide range of environmental and quality of life benefits for local communities. It includes parks, open spaces, playing fields, woodlands, but also street trees, allotments and private gardens. It can also include streams, canals and other water bodies and features such as green roofs and walls.
- 4.2. Green infrastructure helps deliver sustainable communities alongside other infrastructure such as transport and utilities, and where well-designed, can deliver multiple benefits such as ecosystem services for society, the economy and the environment. (NPPG paragraphs 028)
- 4.3. Well-designed green infrastructure should create a sense of place by responding to, and enhancing, local landscape character. It should be used, where appropriate, to help mitigate and adapt to climate change risks through carbon storage, the use of sustainable drainage systems, managing flood risk and water resources, improving water quality, reducing the urban heat-island effect and supporting adaptive management in coastal areas. It can also provide networks to help vulnerable species and contribute to halting the decline in biodiversity (NPPG Paragraph 030).
- 4.4. The Districtwide Green Infrastructure Study (2011) identifies the elements that make up the existing green infrastructure network in the district and sets standards and recommendations for the key typologies, including the following:
- 4.4.1. Public parks and amenity spaces:

Typology	Standards to be maintained
District Park	1 ha per 1000 population The whole population within 10 minutes' walk/1km distance
Local Park	1.14 ha per 1000 population The whole population within 10 minutes' walk/1km distance
Neighbourhood amenity spaces	0.14 ha per 1000 population The whole population within 5 minutes' walk/500m distance
Total parks and amenity space	2.28 ha per 1000 population

4.4.2. Natural and semi-natural greenspace:

Typology	Proposed standard
Natural and semi-natural greenspace includes SSSIs, Ramsar sites, SACs, NNRs, Fishing Lakes, Common Land, Local Wildlife Sites, Ancient Woodland and Green Corridors.	<p>To investigate the potential to create new accessible greenspaces in or around Latchingdon and Tillingham.</p> <p>To improve connectivity between semi natural greenspaces.</p> <p>To apply a higher level of policy protection to Local Wildlife Sites through the emerging Local Development Framework.</p>

4.4.3. Sports pitches and allotments:

Typology	Proposed standard	
11-a-side football pitches	One pitch per 1,475 in Burnham, Heybridge, Maldon, Southminster. One pitch per 1,650 people in rural areas.	Qualitative improvements to ensure that all aspects of all pitches and ancillary facilities are rated as 'average' or better at all sites.
Mini-soccer football pitches	One mini-soccer pitch per 6,280 people.	The whole population within 15 minutes' drive or walk of the nearest pitch
Allotments	<p>0.2ha per 1000 population.</p> <p>The majority of the District is deficient in allotments. Priority for creating new allotments should be in parishes with larger populations where there are identified deficiencies.</p> <p>The whole population should be within 2km drive of the nearest site.</p>	

4.4.4. No quantitative standard is set for natural and semi-natural greenspace, linear space, cycleways or footpaths. Quantitative, qualitative and accessibility standards are also set for other outdoor sport provision. A vision, principles and action plan for an enhanced network of provision will be identified in the proposed Green Infrastructure Strategy. All applications will need to have regard to the standards contained within the Green Infrastructure Study and any subsequent guidance.

5. Open space, sport and play

- 5.1. Open space forms an essential part of the green infrastructure network delivering health and community benefits, ecosystem services and economic value (NPPG paragraph 002). There is significant and growing evidence of the health benefits of high quality greenspace, including improvements in mental health and well-being. It can also bring benefits of social cohesion and reduces social isolation but there is unequal access across England.
- 5.2. The Districtwide Green Infrastructure Study (2011) which identifies the key open space and sports facilities in the district sets local standards for these key typologies (See Section 4.4 of this document) including district parks, local parks, neighbourhood greenspace, playing pitches and allotments. Quantitative, qualitative and accessibility standards are also set for other outdoor sport provision.
- 5.3. High quality open space is well-connected, integrated with built infrastructure, makes use of site features, existing biodiversity and views, is located where homes can overlook key elements of greenspace such as footpaths and play spaces, uses high quality and robust materials that can withstand wear and tear and reduce maintenance needs, is multi-functional and contributes to climate change adaptation or mitigation.
- 5.4. Play spaces need to be integrated into the wider public realm. They form an essential part of strategic thinking for open space. But play does not happen only in playgrounds. A mix of play spaces can occur e.g. in pocket parks, streets and town squares. Natural play space including landform, vegetation, and natural elements such as logs are preferred by children and help them develop creative play. Good quality design needs to balance imagination and risk. An appropriate designer should lead on this. Play spaces should be accessible to all (Designing and Planning for Play (2008) Cablespace; available from <http://www.designcouncil.org.uk/resources/guide/designing-and-planning-play>)
- 5.5. Maldon District current play standards are those promoted by the National Playing Fields Association (NPFA) (now Fields in Trust (FIT)) for Local Equipped Areas for play (LEAPS) and Neighbourhood Equipped Areas for Play (NEAPS).
- 5.6. The main characteristics of a LEAP play area are:-
- Located within a walking time of 5 minutes from home
 - 5 play types, including safer surface
 - Fencing complete with 2 pedestrian gates
 - Minimum activity zone of 400sqm
 - Seating, litter bins and a notice should be provided
- 5.7. The main characteristics of a NEAP play area are:-
- Located within a walking time of 15 minutes from home
 - 8 play types, including safer surface
 - Provision of a MUGA/Skate Park
 - Fencing complete with 2 pedestrian gates
 - Minimum activity zone of 1000sqm
 - Seating, litter bins and a notice should be provided

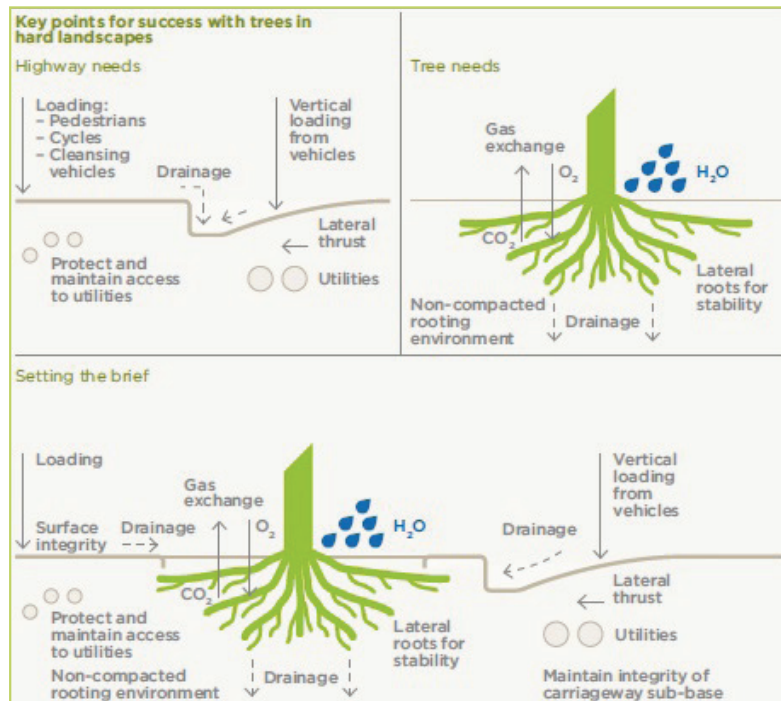
6. Biodiversity

- 6.1. The Government's National Planning Practice Guidance (NPPG) identifies that development can affect biodiversity but that biodiversity benefits can also be delivered through the planning system. Both the local authority and a prospective developer need to consider existing biodiversity strategies and studies covering the local and neighbourhood plan areas and biodiversity action plans. These include the Maldon District Nature Conservation Study (2007), the Maldon District Local Wildlife Sites Review (2007) and The Essex Biodiversity Action Plan (2012). (See Section 2.3 of this document).
- 6.2. It is also important to consider the potential effects of development on priority habitats or species (See reference to Biodiversity 2020 in Section 2.2.5 of this document).
- 6.3. Information on biodiversity impacts and opportunities should inform all stages of development where biodiversity may be affected, including site selection and design, through the use of an ecological survey. These might be either stand-alone surveys or form part of a formal Environmental Impact Assessment (EIA), depending on the nature of both the site and the proposed development (NPPG Paragraph 014). Surveys should be carried out by suitably qualified ecological professionals. The statutory obligations in regard to international and national designated sites of importance for biodiversity must also be considered (NPPG Paragraph 08).
- 6.4. The biodiversity value of a site should not be considered in isolation by developers but should be seen in the context of the overall ecological network of the area (NPPG Paragraph 09). The main components of the ecological network of the district are identified in the Maldon District Nature Conservation Study (2007), the Maldon District Local Wildlife Sites Review (2007) and the Green Infrastructure Study (2011) or any subsequent adopted documents. (See Section 2.3 of this document).
- 6.5. Outside of these specific identified sites or links, prospective applicants must consider of The Essex Biodiversity Action Plan (2012) (See Section 2.3 of this document) which identifies action plans in Essex for the 19 priority habitats identified in the governments Biodiversity 2020 Strategy, some or all of which may be of issue in Maldon District. Some of the most important priority habitats in Maldon District include Hedgerows, Lowland Dry Acid Grassland, Lowland Heathland, Traditional Orchards, Floodplain and Coastal Grazing Marsh and Coastal Saltmarsh. Prospective developers should take particular note if any of the priority habitats are likely to be on one of their potential development sites and seek expert advice on ways to conserve, enhance or mitigate any potential impacts.
- 6.6. At the species level, prospective applicants should be aware of the following protected species found in Essex including Maldon: Badgers, Barn Owls, Bats, Nesting Birds, Dormice, Great Crested Newts, Invertebrates, Otters, Reptiles, Water Voles, White Clawed Crayfish, and Wildflowers.

7. Trees in development

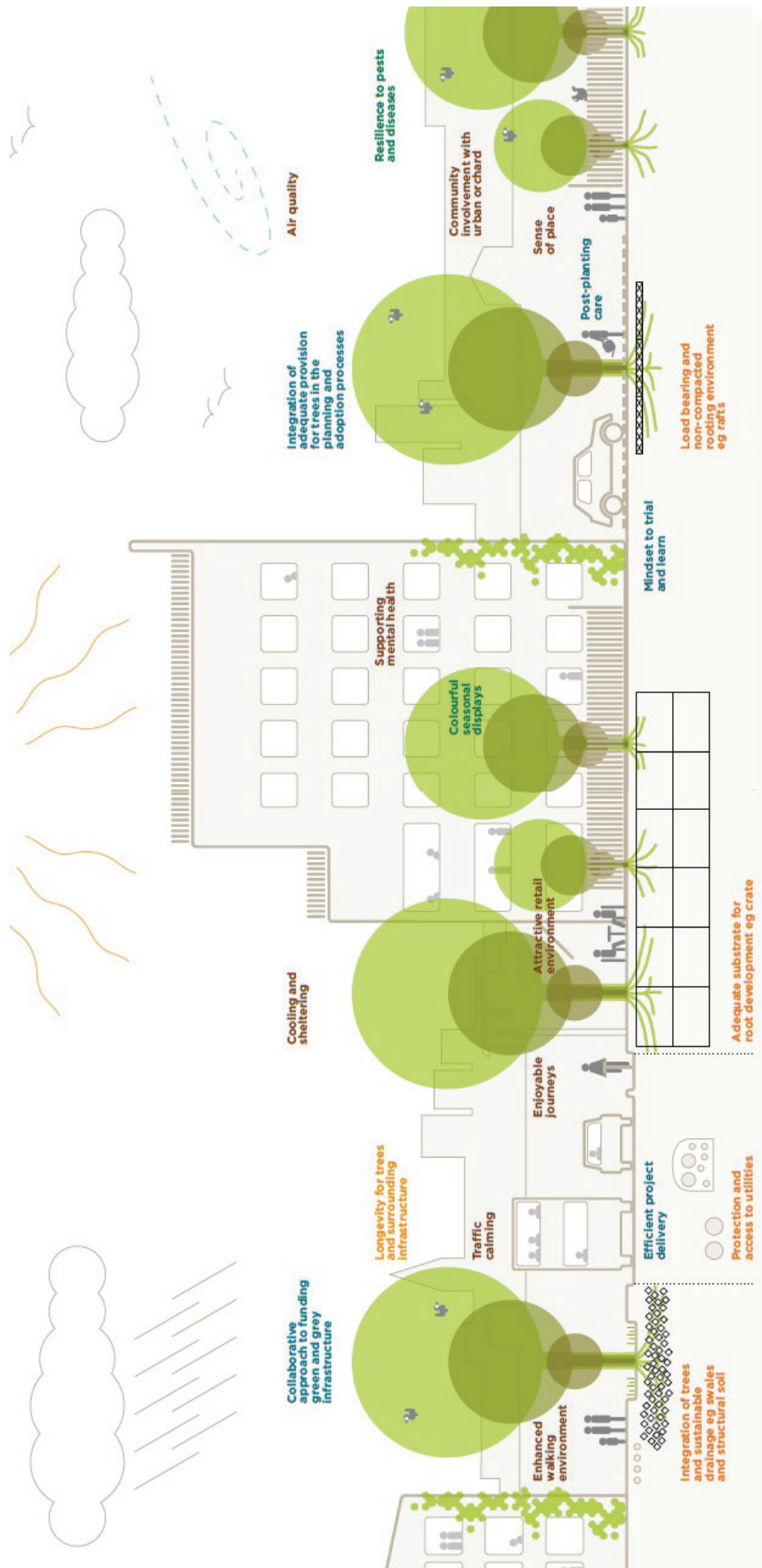
- 7.1. Trees, woodlands and copses are important in developments both for their aesthetic benefit and the ecological and other ecosystem services they provide. They can provide features that enhance the character and heritage of an area whilst giving an economic uplift, for example, to residential streets and retail properties. Their environmental benefits include air cooling in summer, filtering of pollutants, improving air quality, and absorbing carbon dioxide.
- 7.2. Some trees in both town and countryside are protected by a Tree Preservation Order. This is an order made by the local planning authority to protect specific trees, groups of trees or woodlands in the interests of amenity, and prohibits the cutting down, topping, lopping, uprooting, wilful damage or destruction of trees without written consent. Cutting roots is also seen as a prohibited activity. Trees in Conservation Areas are also subject to special protection. All trees should be inspected and maintained regularly by a competent professional to make sure they remain safe and healthy. (NPPG paragraph 001).
- 7.3. Ancient Woodlands are sites that are thought to have been wooded since 1600 AD which have unique features such as undisturbed soils and sometimes rare or vulnerable communities of plants and animals, mediaeval boundary banks, charcoal hearths and old coppice stools. Many species characteristic of ancient woodland are slow to disperse and do not colonise new areas easily, making them irreplaceable.
- 7.4. **DIAGRAM 1: The benefits of tree planting within the built environment (TAKEN FROM PAGE 4 AND 5 OF TREES IN HARD LANDSCAPES: A GUIDE FOR DELIVERY)** See page 15.
- 7.5. Existing trees should be integrated into a scheme at the assessment stage through a survey of all trees present on and immediately adjacent to the site. It will also enable the identification of root protection areas. A survey should consider species, location, current size, conditions and potential size, as well as integrating with data on any statutory protections, considerations of amenity, landscape character and biodiversity value. An appropriate professional arboriculturalist should be used for this work.
- 7.6. For new trees, designing below-ground is critical to tree survival especially in hard landscaped areas. Consideration is required at an early stage of the location of load-bearing road surfaces and service runs in relation to rooting space and water infiltration needs of trees. Soil aeration and rooting volume are critical for root growth and tree success. There must be enough lateral space for roots to grow for stability, good access to water and drainage.
- 7.7. Special care should be taken where existing soils are heavily disturbed or compacted and where, high levels of pedestrian or cycle use, or car-parking is required over the trees rooting area or utilities are less than 3m away (For more details see BS 8545:2014). Where load-bearing is needed the sub-surface may need to be engineered to protect the rooting area below. There are three broad areas of load-bearing solutions: structural growing media, crate systems and raft systems (See 'Trees in Hard Landscapes: A guide to Delivery' (2014) (TREES AND DESIGN ACTION GROUP)).

7.8. **DIAGRAM 2 Key points for success with trees in hard landscapes** (TAKEN FROM PAGE 89 OF TREES IN HARD LANDSCAPES: A GUIDE FOR DELIVERY (2014))



- 7.9. The integration and/or protection of trees in hard landscapes reduces and mitigates surface water runoff, especially by the use of forest scale trees with larger canopies. Good quality, non-compacted, well-aerated rooting environments that support effective tree growth provide the first step in increasing the capacity of trees to perform this role. Opportunities should be sought to integrate tree planting and SuDS, for instance through the use of pervious surfacing, below ground crate or structural growing systems, bio-retention tree planters, and swales with trees (See 'Trees in Hard Landscapes: A guide to Delivery' (2014) (TREES AND DESIGN ACTION GROUP)).
- 7.10. Tree support will be needed for specimen trees in the first few years of growth either through above ground stakes for most standard trees or below ground root ball anchoring for larger, semi-mature specimens. The bark of the tree must also be protected if the tree is to survive. Bark can be damaged at planting (temporary protection such as hessian wrapping can mitigate this) or by long term use of maintenance machinery (street cleaning equipment, mowers or strimmers) and other vehicles. Options for protection include metal tree guards, raised kerbs, low railings or bollards.
- 7.11. Tree species selection should be a balance between the constraints related to tree survival (available light, rooting area, soil pH etc.), amenity and biosecurity needs. Consideration of the strategic outcomes desired for the landscape design at the outset should inform the layout of built structures and infrastructure not just respond to it. Landscape character and biodiversity considerations should be strong drivers in schemes on the edge of towns and villages, particularly in relation to boundaries with the open countryside. However there are many non-native, ornamental species that can be successfully used within the internal streets, open spaces and civic squares in development schemes. The use of only a limited number of tree species in a scheme can have implications in terms of future risks of pathogens and pests, for instance. However the design impact of the use of same species and/or variety, for instance in avenues, is an important and traditional planting practice that needs to be retained.

Diagram 1: The benefits of tree planting within the built environment
(TAKEN FROM PAGE 4 AND 5 OF TREES IN HARD LANDSCAPES: A GUIDE FOR DELIVERY)



Landscape and Green Infrastructure (including Open Space, Sport and Play Facilities, Biodiversity and Trees)

Technical Document to
Maldon District Design Guide
February 2017

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Project Brief: Preparation of a Design Guide Supplementary Planning Document (SPD) for Maldon District

1.0 Introduction

- 1.1 Maldon District has a built environment with a distinctive character that is closely related to its local environment and history. National planning policy places great importance on the design of the built environment and states that high quality design should ensure that new development is visually attractive, responsive to local character, helps to promote healthy communities, and creates buildings which are durable, adaptable, and function well within the surrounding area to create a safe and accessible environment. Good design should enable and encourage people to live healthy lifestyles, reduce the risk of crime, create accessible environments which are inclusive for all sectors of society, and increase opportunities for social interaction.
- 1.2 A significant amount of development is expected to take place in Maldon district over the next 15 years and if the character and quality of our towns and villages is to be maintained and enhanced it is vital that new development is designed to a high standard. The District Council has therefore put good quality design at the centre of its drive towards sustainable development and wants to promote and deliver high quality design throughout the district and to inspire developers and designers to do the same.
- 1.3 Significant improvements in the quality of new development can be achieved if proposals are informed by a proper understanding of both key design principles and the character of the area in which they are situated. The Council is therefore seeking to appoint suitably qualified consultants with relevant experience to prepare a Design Guide SPD for Maldon District. The Design Guide, together with the design policies in the Local Development Framework (LDP) will be the key mechanism for delivering high quality design in the district.
- 1.4 The LDP sets out the planning strategy for future growth over the next 15 years which is based on an understanding of the unique and distinguishing features of the district. In accommodating growth requirements, the Council will seek to ensure that development will not have a detrimental impact on its surrounding area and local context and will actively seek opportunities for enhancement in the built environment. Policy D1 provides the policy basis within which the Design Guide SPD is to be prepared and states that all development must:
- 1) Respect and enhance the character and local context and make a positive contribution in terms of:-
 - a) Architectural style, use of materials, detailed design features and construction methods. Innovative design and construction solutions will be considered where appropriate;
 - b) Height, size, scale, form, massing and proportion;
 - c) Landscape setting, townscape setting and skylines;
 - d) Layout, orientation, and density;
 - e) Historic environment particularly in relation to designated and non-designated heritage assets;

- f) Natural environment particularly in relation to designated and non-designated sites of biodiversity / geo-diversity value; and
 - g) Energy and resource efficiency.
-
- 2) Provide sufficient and usable private and public amenity spaces, green infrastructure and public open spaces;
 - 3) Contribute positively towards the public realm and public spaces around the development;
 - 4) Protect the amenity of surrounding areas taking into account privacy, overlooking, outlook, noise, smell, light, visual impact, pollution, daylight and sunlight;
 - 5) Include safe and secure vehicle and cycle parking in accordance with the Council's adopted parking standards;
 - 6) Maximise connectivity within the development and to the surrounding areas including the provision of high quality and safe pedestrian and cycle routes;
 - 7) Maximise opportunities for sport and physical activity;
 - 8) Contribute to and enhance local distinctiveness;
 - 9) Incorporate design measures to reduce social exclusion, the risk of crime, and the fear of crime; and
 - 10) Encourage inclusive design and effective use of internal and external space.
- 1.5 The natural, historic and built environment of the District is distinctive in character and diverse in its location across towns and villages in rural, coastal, and estuarine environments. However, there are concerns that the unique character of some parts of the District is gradually being eroded by insufficient and inconsistent design principles. When located close to, or in the setting of a heritage asset, new development should respect the importance, character and local context of that asset. Good design should seek to positively respond to the important features of the asset, and enhance its overall setting and function.
- 1.6 The Council expects all development to support the principles of inclusive design, to ensure that new development can be used by all people, and to seek to deliver green infrastructure benefits where appropriate. Design should also seek to reduce social exclusion within the District, and seek to improve people's access to housing, employment, and required services and facilities. The design of new development should also maximise people's ability to access required services and facilities by sustainable methods of transport, particularly through the provision of high quality pedestrian and cycle routes.
- 1.7 In order to promote the objectives set out in Policy D1 and to ensure that development is of a high design quality which will enhance the natural, historic and built environment of the district, the Council is seeking to appoint suitably qualified consultants with relevant experience to prepare a new Design Guide for Maldon District. The new Design Guide, together with the design policies in the Local Development Framework will be the key mechanism for delivering high quality design in the district.

2.0 Aims

2.1 The Council is seeking to deliver an innovative and bespoke Design Guide to help developers, applicants and Council officers to improve the quality of design in new development across the district. Recognised principles of good design should be sought to create a high quality built environment for all types of development, irrespective of location within the District. Although visual appearance and the architecture of individual buildings are important factors, good design should improve connections between people and places, and should integrate new development into the natural, built and historic environment.

2.2 The SPD will be used by:

- Applicants and developers when preparing their development schemes
- Planning officers to assess the design quality of development proposals when determining applications and offering pre-application advice
- Council members when assessing development proposals

2.3 The Design Guide should be more than just a wealth of information. It needs to be an effective tool for achieving high quality design that can be used by developers and designers to prepare high quality development proposals and also by planning officers when assessing applications. It should be concise and easy to use, and should be tailored to Maldon District's particular characteristics and needs. This includes designing sites that take into account future growth whilst safeguarding and enhancing the natural and historic environment and mitigating and adapting to the potential impacts of climate change. Significant importance is placed on Place Shaping to encourage an integrated approach to development and the creation of successful places that will complement and enhance the character of the District.

2.4 The new Design Guide should:

- provide an effective tool that can be used by developers and their design teams and Council officers in the design and assessment of development proposals;
- set out clearly and briefly what the purpose of the guide is, how it is structured and how it should be used;
- set out the design process that should be followed in order to achieve high quality design – this should include the need to consider future growth planned for the area, and designing sites that deliver sustainable development;
- distinguish between best practice in rural and urban areas, where required;
- be highly visual to assist the user and inform their understanding;
- be easy to use and simple to navigate; and
- clearly signpost where the user can access more detailed guidance and when this should be taken into account.

2.5 In order for the Guide to be an effective tool it should be easy to identify the underlying national and local policy that support the Guide and give it weight in planning decisions. Consideration should also be given to how community groups could use the Guide to assist with the preparation of neighbourhood plans.

2.6 The Design Guide should also have strong links with the Building for Life 12 guidance and should encourage developers to achieve 'Built for Life' accreditation. Developers

should be able to identify from using the Guide whether their schemes would be eligible for the 'Built for Life' accreditation and recognise that by following the guide their schemes will be eligible.

- 2.7 It is intended that the Design Guide will be adopted as a Supplementary Planning Document.

3.0 Scope of Design Guide

- 3.1 Significant improvements in the quality of new development can be achieved if proposals are informed by a proper understanding of both key design principles and the character of the area in which they are situated. The Council does not want to stifle innovation but wishes to ensure that new development reflects the character of its context. The Design Guide should build on the Maldon District Characterisation Assessment (Qube, 2006) which provides essential guidance to developers on the essential characteristics of towns, villages and hamlets in the area. The Design Guide should be applicable to the whole of the district and distinguish between best practice in urban and rural areas, and reflect appropriate local characteristics and distinctiveness.
- 3.2 The District Characterisation Assessment provides a detailed assessment categorising the settlements within the District and the design forms representative of each settlement type and uses a number of case studies to identify and illustrate their defining qualities in terms of spatial form, layout, relationship to existing adjacent development and detailing. Settlements are classified according to the following settlement types:
- The Main Towns- Maldon, Heybridge and Burnham-on-Crouch
 - The Agricultural Settlements
 - The maritime/Riverside Settlements
 - The Arcadian Settlements
- 3.3 The Design Guide will be required consider design forms appropriate to each settlement type and to provide guidance on the form of development that would be appropriate in terms of overall scale, density, massing, height, landscape, layout and access. It will also be required to provide advice on the palette of materials to be used in the construction of new developments most appropriate and sympathetic to each settlement type. The main focus of the Guide should be to provide a tool that can be used to design and assess development proposals and there should be a strong and easily recognisable connection between the tool and national and local policy.
- 3.4 In preparing the Design Guide, reference should also be made to the Strategic Masterplan Frameworks (SMFs) for the South Maldon and North Heybridge Garden Suburbs. The Council is committed to the delivery of high quality, vibrant and distinctive new neighbourhoods based on garden suburb principles and the SMFs (which have been endorsed by the Council as material considerations for Development Management purposes) include design guidance for these areas. Strategic Design Codes are currently being prepared for both Garden Suburbs. The Council is also in the process of commissioning consultants to prepare a masterplan for the Central Area of Maldon and Heybridge.

3.5 The Design Guide should incorporate the following key elements:

- Review of local character and vernacular, and the policy context including national policy. This stage to be based on the Maldon District Characterisation Assessment
- Development of set of key urban design principles for creating successful places building on policies in LDP and other relevant guidance
- Development of specific district-wide guidance for the distinct areas of the district identified in the Maldon District Characterisation Study in terms of:
 - Layout
 - Scale
 - Density
 - Streetscape
 - Amenity
 - Facades
 - Materials
 - Climate change
 - Access and parking
 - Landscape design and/or greenspace
 - Householder extensions
- A design checklist that highlights criteria for assessing the design of development

3.6 The Design Guide should be user-friendly document and provide a clear design/assessment tool and relevant information necessary to inform both developers/designers and planning officers on how to deliver high quality design. It will be important that the design policies in the LDF can be easily tied to the guidance in the Design Guide. While the Design Guide will be used to determine applications, its principal focus should be to assist the design process. The Design Guide should be applicable to the whole of the district and distinguish between best practice in urban and rural areas, where relevant and necessary.

3.7 In preparing the Design Guide, the appointed consultant will be required to engage with officers and other key stakeholders.

4.0 Key Stages and Outputs

Phase 1 – Review of Baseline information

4.1 The Consultant will be required to review all existing sources of information to understand the work undertaken to date. This would include:

- Relevant National and Local planning policy guidance including LDP Policies;
- [Maldon District Characterisation Assessment \(Qube, 2006\)](#)

- Relevant supplementary planning documents within the Council's [Local Development Plan evidence base](#).
- [South Maldon Garden Suburb Strategic Masterplan Framework](#) and [North Heybridge Strategic Masterplan Framework](#) with particular regard to Character Areas and Garden Suburb Principles

4.2 In order to understand key design issues facing the District and the areas where particular guidance is required, the Consultant will be required to facilitate a workshop with officers and other key stakeholders.

4.3 In the light of this review, the consultant will advise the Council and agree the scope and content of the Design Guide.

Phase 2- Preparation of Draft Design Guide

4.4 The consultant will prepare a draft Design Guide to be expressed and presented through words and graphics including three-dimensional and section drawings and annotated photographs or sketches.

4.5 Preparation of the Draft Design Guide will include a workshop to be attended by officers and key stakeholders to test and review the draft guidance.

4.5 The consultant will present the Draft Design Guide to a meeting of the Planning & Licensing Committee.

Phase 3- Consultation on Draft Design Guide

4.6 The consultant will prepare material for public consultation and assist the Council as required in undertaking public consultation

Phase 4- Final Design Guide

4.7 The consultant will review responses to public consultation and recommend changes to the Design Guide for approval by Planning & Licensing Committee.

5.0 Submission Requirements and Assessment

5.1 In response to this brief, tenderers are required to provide:

- I. Details of proposed approach and how you propose to comply with the project brief within the defined timescale;
- II. Indicative work programme;
- III. A fixed price quotation for undertaking the work outlined in the submission including number of days for each named personnel (Phases 1-2 only) (time and expenses);
- IV. Examples of similar commissions involving the preparation of Design Guides including demonstration of subsequent outcomes;
- V. Hourly and daily fee rates for different grades of personnel - Director, Associate, Principal Consultant, Senior Consultant, Consultant for Phases 3 and 4 and any additional work which may be commissioned by the Council;

- VI. CVs of named personnel and their proposed role in the project including details of relevant experience in preparation of Design Guides;
- VII. Details of your professional liability insurance;
- VIII. Contact details of two referees.

5.2 Please confirm within your submission any conflicts of interest which should be considered in relation to other current or future work being undertaken by your company.

6.0 Budget

- 6.1 The maximum budget for undertaking Phases 1 and 2 is £30,000 (excluding VAT but inclusive of expenses).
- 6.2 Additional work may be needed to prepare detailed guidance to support the Design Guide. Any further work in Phases 3 and 4 will be commissioned on the basis of the submitted hourly rates. The maximum budget for phases 3-4 is £5,000.

7.0 Timescales

7.1 The indicative timetable for the award of the contract is as follows.

- 1) Deadline for Return of Quotation: February 2016
- 2) Interviews week commencing February 2016
- 3) Appointment of consultants: February 2016
- 4) Inception Meeting: w/c February 2016
- 5) Completion of Phase 1: March 2016
- 6) Completion of Phase 2: June 2016
- 7) Phase 3: Consultation on Draft Design Guide July/August 2016
- 8) Phase 4: Final Design Guide: October 2016

8.0 Reporting and project management arrangements

- 8.1 The consultant will work closely with the council to ensure that agreed services are being executed and delivered to the highest standards according to critical timings and best practice. The council will be able to supply OS base maps.
- 8.2 Your point of contact at the council will be Tim Parton, Spatial Planning Team Leader. You should nominate one person from your company to manage the brief through to completion and act as the primary contact.
- 8.3 The project will be administered by email and telephone wherever practicable with project meetings and workshops at the Council offices at inception and draft report stage.

9.0 Performance Indicators

- 9.1 In addition to the timescales set out in section 7 above, the Council will also require the following:

- Regular updates on the progression of the work will be provided to the Council every two weeks;
- Responses to email and telephone correspondence from the Council within two days;

10.0 Questions and further information

10.1 Please send your submission to the following contact details (submissions by email are preferred)

E mail: tim.parton@maldon.gov.uk

10.2 Postal: Tim Parton, Spatial Planning Team Leader, Maldon District Council, Council Offices, Princes Road, Maldon, Essex CM9 5DL

10.3 If you should require any further information please contact Tim Parton, Spatial Planning Team Leader, Tel: 01621 876203. Please note that, for reasons of fairness, we will send questions and their answers to the other consultants we have invited to submit a proposal.

11.0 Evaluation Methodology

The contract will be awarded taking into consideration the following criteria:

Criteria / Sub Criteria		Marks	Weightings	
Price		/10	40%	
Quality	Quality of proposed approach to meeting the requirements of the project brief set out in sections 3 and 4 above	/10	40%	60%
	Examples of previous work	/10	10%	
	Ability to meet the work programme set out in section 7 above	/10	10%	

The following scoring methodology will be used for the quality criteria;

Score	Interpretation
0 - 1	Either no answer is given or the answer provides no evidence that the Tenderer meets any of MDC's requirements.
2 - 3	The answer provides some evidence that the Tenderer meets some of MDC's requirements but only in a minimal way.
4 - 5	The answer provides satisfactory (or better) evidence that the Tenderer

APPENDIX B
Maldon District Council

	partially meets MDC's requirements.
6 - 7	The answer provides good (or better) evidence that the Tenderer meets a majority of MDC's requirements.
8 - 9	The answer provides good (or better) evidence that the Tenderer meets nearly all of MDC's requirements.
10	The answer provides excellent evidence that the Tenderer fully meets all/exceeds MDC's requirements.

The resultant price and quality scores will be transferred across to the overall evaluation model where weightings will be applied to give total scores out of 100%.

APPENDIX 1: Quotation Form

Please complete this form and include within the submission.

General Conditions

GENERAL CONDITIONS OF CONTRACT to be observed by the Contractor in the execution of the work as detailed in the specification and all work incidental thereto for and on behalf of the Maldon District Council.

DEFINITIONS

1 For the purpose of this Contract:

(a) "Employer" shall mean the Maldon District Council, Council Offices, Princes Road, Maldon

(b) "Supervising Officer" shall mean Mr David Coleman, Planning Policy Team Leader, Maldon District Council or other such person appointed from time to time by the Employer and notified to the Contractor to act as Supervising Officer for the purpose of the Contract

(c) "The Services" means all services included within the Specification Documents which are required to be carried out in strict accordance with the terms of the Quotation, General Conditions of Contract.

MARGINAL NOTES

2. The interpretation or construction of the conditions shall not be affected by any index, marginal notes or headings.

CONTRACTOR'S OBLIGATION

3. The Contractor shall with due diligence and in good workmanlike manner execute the services to the satisfaction of the Supervising Officer

VARIATIONS

4. The Supervising Officer may, without invalidating the contract change the work or the period in which they are to be carried out. Any such instructions shall be valued by the Supervising Officer on a fair and reasonable basis.

STATUTORY OBLIGATIONS, FEES AND CHARGES

5. The Contractor shall comply with all notices required by any statute, any statutory instrument, rule or order or any regulations or bylaws applicable to the Services and charges in respect of the Services legally recoverable from him.

CONTRACTORS REPRESENTATIVE

6. The Contractor shall at all reasonable times keep a competent person in charge and inform the Supervising Officer of their name and of any change.

SUB-CONTRACTING

7. The Contractor shall not sub-contract the services or any part thereof without the written consent of the Supervising Officer which consent shall not be unreasonably withheld.

INTELLECTUAL PROPERTY RIGHTS	8. Copyright, patent rights and any other intellectual property rights and title in any material produced by the Contractor for the Authorities during this contract shall vest in the Authority unless otherwise expressly agreed or approved by the Supervising officer or an Authorities' authorised representative in writing.
PERFORMANCE	9. During the Agreement Period the Contractor will comply with its obligations under the contract and will perform the Services strictly in accordance with the provisions of the Specification and quotation documents and to the entire satisfaction of the Supervising Officer.
VALUE ADDED TAX	10. The Contractor shall include in his quotation for all incidental costs and expense he may incur under the Finance Act 1972 and regulations made thereunder to pay Value Added Tax at the standard rate on the supply of all goods and services to him by suppliers and sub-contractors, whether nominated or otherwise.
PAYMENT	<p>11. (i) The Council will pay the fixed price sum in equal instalments in line with satisfactory completion of the work within each stage.</p> <p>(ii) Fixed Day rates shall be paid one month in arrears according to the contractor submitting accurate and substantiated schedules.</p> <p>(iii) Maldon District Council shall pay the undisputed sums due to the contractor in cleared funds within 30 days of receipt and agreement of invoices for services completed to the reasonable satisfaction of the Council.</p> <p>(iv) Each invoice shall contain all appropriate references and a detailed breakdown of the services and shall be supported by any other documentation reasonably required by the Supervising Officer to substantiate the invoice.</p>

Form of Quotation & Pricing Schedule

To: Maldon District Council

I/We the undersigned, hereby offer and undertake to execute and complete the whole of the services as detailed in strict accordance with the Specification, Brief and General Conditions of Contract to the complete satisfaction of Mr David Coleman, Planning Policy Team Leader, Council Offices, Princes Road, Maldon, Essex CM9 5DL for the total amounts as follows:

Fixed price for the completion of Phases 1 and 2:

£

For phases 3 and 4 the following hourly and day rates for personnel will be applied for any additional work which may be commissioned:

Personnel	Hourly Rate (£)	Daily Rate (£)
Director		
Associate		
Principal Consultant		
Senior Consultant		
Consultant		

I/We hereby declare that:-

- (1) This is a bona fide competitive quotation and the amount has not been fixed or adjusted under or by or in accordance with any agreement or arrangement with any other person.
- (2) The amount or approximate amount of my/our proposed quotation has not been and will not be communicated to any person other than the person calling for these quotations.
- (3) I/We have not made any agreement with any other person that they shall refrain from submitting a quotation or as to the amount of any other quotation to be submitted.

I/We understand that you are not bound to accept the lowest or any quotation you may receive.

I/We understand that no increased cost of labour and/or materials will be allowed during the period of the Contract.

I/We understand that the successful quotation, together with the Council's written acceptance thereof, will form a binding Contract between the Council and the successful contractor.

Yours faithfully

Signed

For and on behalf of

Address

.....Post Code.....

Date.....



**REPORT of
DIRECTOR OF CUSTOMERS AND COMMUNITY**

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

**RESPONSE TO NATURAL ENGLAND - ENGLAND COASTAL PATH, MALDON
TO SALCOTT SECTION**

1. PURPOSE OF THE REPORT

- 1.1 To present Officer recommendations that they may be incorporated into a formal letter of response to Natural England regarding the creation of the first of three sections of the England Coastal Path through the Maldon District.

2. RECOMMENDATIONS

- (i) that the guiding principles set out in section 3.3 of this report be agreed and, where necessary, incorporated into a response to Natural England consultation on the proposed route (Maldon to Salcott) of the English Coastal Path (ECP);
- (ii) that for land owned by Maldon District Council (MDC), the route of the ECP follows the proposed / mapped Natural England route, with boundary restrictions and exclusions as specified in the body of this report;
- (iii) that a voluntary dedication agreement of ECP route through land 'excluded' from the Marine & Coastal Access Act 2009; specifically the proposed short section connecting Promenade Park to Hythe Quay on riverward side of Cooks Boatyard and a second section through the Downs, Downs Road, be agreed;
- (iv) that for land not owned by MDC but falling within the District Area, the proposed / mapped route be considered to be favourable;
- (v) that subject to (i) to (iv) above and in consultation with the Chairman of the Planning and Licensing Committee the Director of Customers and Community be authorised to respond to Natural England's consultation on the ECP Maldon to Salcott proposed route and the subsequent two further sections of the ECP within the Maldon District (Maldon to Burnham-on-Crouch and Burnham-on-Crouch to Wallasea), subject to the detail of the route proposals and in good time for the submission of their reports to the Secretary of State.

3. SUMMARY OF KEY ISSUES

3.1 Background

- 3.1.1 Part 9 of the Marine and Coastal Access Act 2009 aims to improve public access to the English Coast by the creation of a long distance walking route and a margin of land adjoining the route where people will be able to spread out and explore in appropriate places.
- 3.1.2 The Act places a statutory duty on Natural England to prepare a series of reports for consideration by the Secretary of State. Each Report is to set out proposals for the route and the adjoining margin for a particular stretch of coast.
- 3.1.3 The English Coast Path is split into three sections that fall within Maldon District (from North to south) - Maldon to Salcott; Maldon to Burnham-on-Crouch; and Burnham-on-Crouch to Wallasea. This report is intended only for the first of these sections .
- 3.1.4 The Maldon to Burnham-on-Crouch section proposals will be outlined in a report to this Committee in April 2017, in line with Natural England's timescale for developing their report for that section.
- 3.1.5 Burnham-on-Crouch to Wallasea will be due towards the end of this calendar year as work on the development of this part of the route has just started.

3.2 Consultation with Natural England

- 3.2.1 Council Officers have been in discussion with Officers from Natural England (NE) since Autumn 2015, with regular update meetings to communicate information, current status of proposals, and developments in the local and National stances on the development of the England Coastal Path (ECP).
- 3.2.2 Consultation has involved several visits and on-site discussions to various sections of the proposed route, including viewing areas of concern from the waters of the River Blackwater itself.
- 3.2.3 There was clear understanding between MDC and NE Officers that the ECP must strike a fair and appropriate balance between the interests of the public in having rights of access over coastal land and the interests of owners and occupiers of land over which coastal access rights would be conferred.
- 3.2.4 Natural England expressed an interest in gaining a response / representation to both the proposed ECP route specifically through MDC owned land and the route in general through the District.

3.3 MDC Guiding Principles

- 3.3.1 Much of the immediate coast through the Maldon District is already accessible through existing Rights of Way along the Sea Wall and Sea defences.

3.3.2 Through the MDC ownership and leasing of large sections of the intertidal foreshore in the Blackwater Estuary (and byelaws governing such), there is *already* implicit public access to the foreshore from an MDC point of view. However, it is recognised that this usage of the intertidal area is largely confined to publicly accessible beaches and would not be suitable for enshrining under the Marine and Coastal Access (MCA) Act 2009.

3.3.3 Landward boundary

- Where the route falls upon sea wall / sea defence, the landward boundary of the ECP should be restricted to the landward edge of the crest of the sea wall.
- Where the route follows existing Public Rights of Way elsewhere, the landward boundary should be defined by that boundary or legal width stated in the Definitive Statement and Definitive Map for any such right of way.
- For land “excluded” from the MCA Act 2009, the route should follow an existing publicly accessible surfaced line, to be proposed by Natural England and agreed by MDC through a Voluntary Dedication agreement (*see maps: The Hythe & Downs Road*)

3.3.4 Spreading Room

3.3.4.1 This should exclude all areas defined as Saltmarsh and mudflats for the following reasons:

- Safety of ECP users – The landform and inherent nature of the majority of the Saltmarsh, Mudflats, and tidal river present hazards to the health and safety of users, particularly those unfamiliar with the area. By excluding these areas, officers consider that the risks associated with them are greatly reduced. (Exclusion justification under S25a of the Countryside and Rights of Way (CROW) Act 2000)
- Sensitivity and fragility of Habitat – Increased usage will have a detrimental impact on the Saltmarsh and intertidal Mudflat as habitats and ecosystems. (Exclusion under S26 of CROW Act 2000)
- Negative impact on species – The Blackwater Estuary holds National and International protected designations as a result of the species which reside and migrate through the area. (Exclusion under S26 of CROW Act 2000)

3.3.4.2 Concerns have also been raised regarding the use of the seaward face of the sea wall, however officers feel that this should be considered on a case by case basis in specific geographical areas and not be part of these guiding principles.

3.4 **Future considerations**

3.4.1 Rollback

3.4.1.1 The new right of Coastal Access brings in 'roll back' meaning that if a section of coast erodes or slips the path is not lost, it simply moves back with the new coastline.

3.4.1.2 Rollback is not considered to be a factor for the Maldon District as it generally applies to cliffs, however would allow for erosion of those sections of unprotected / undefended coastline.

3.4.1.3 Any major changes to ECP route would as a matter of course go through full consultation and report to Secretary of State for approval.

3.4.2 Trail Standards

3.4.2.1 Once created under the MCA Act 2009 the ECP will become the responsibility of the local Highways Authority, in this case Essex County Council.

3.4.2.2 Establishment costs for the Coastal trail will be 100% funded by Natural England and 75% of the ongoing maintenance costs will be met by them through Essex County Council.

3.4.2.3 National Trail standards have existed for many years elsewhere within the UK and will be adopted for the ECP.

3.4.2.4 It is not known yet how Essex County Council will allocate resources or implement its duties with regard to the ECP. These discussions will be undertaken as the final route is confirmed and formally created, however Officers feel it is worth highlighting that the maintenance of the ECP will be a statutory duty.

3.4.3 Creation of “higher” Rights

3.4.3.1 The ECP will create a right of use by Foot only, as is conferred if it were a Public Footpath. There has been some discussion by Officers and expressions of interest by existing users, both locally and nationally, regarding the future creation of higher Rights along the route – namely right of access by Bicycle or Horse (i.e. to Bridleway PRow).

3.4.3.2 MDC Officers would be keen to participate in discussions of these ideas however fully recognise that there would be a large number of concerns and obstacles to overcome, not least the availability of funding - both for the upgrading of the route, signage & surfacing and for future ongoing Maintenance – the concerns of Private Landowners over conferring any higher rights, and the impact of such use upon the essential role of the Sea Wall / defences.

3.4.3.3 Private landowners have every right to confer higher Access rights at their own discretion, for example Permissive Bridleways or Cycleways.

4. **CONCLUSION**

4.1 The creation of the ECP is being enacted under a piece of National Legislation and carried out by a suitable body as a Statutory Duty.

4.2 MDC Officers feel that appropriate and fair consultation process and time have been given by Natural England as the appointed Statutory body to develop a considered

and suitable route proposal for the Maldon to Salcott section of the England Coastal Path.

- 4.3 MDC Officers consider that all concerns as a Landowner have been recorded, incorporated, and responded to appropriately as part of the consultation process.
- 4.4 That the Officer recommendations at the head of this report should be enacted.

5. IMPACT ON CORPORATE GOALS

- 5.1 This activity supports the corporate goals of delivering good quality, cost effective and valued services.

6. IMPLICATIONS

- (i) **Impact on Customers** – The creation of the ECP will confer permanent access right by foot to the whole of the coastline through the Maldon district, where currently sections are not permissible through existing Public Rights of Way.
- (ii) **Impact on Equalities** – None.
- (iii) **Impact on Risk** – None.
- (iv) **Impact on Resources (financial)** – None.
- (v) **Impact on Resources (human)** – None.
- (vi) **Impact on the Environment** – None directly for MDC. Assuming that Landward and Spreading room recommendations in this report are enacted, impact should be no different than current usage of the coast.

Background Papers:

Natural England “Maldon District Council Absolute Freehold” maps 1-6

Natural England Maldon to Salcott ECP route proposal map

<https://www.gov.uk/government/publications/england-coast-path-plan-of-the-maldon-to-salcott-stretch>

Enquiries to: Matt Wilson, Countryside and Coast Manager, (Tel 01621 876275).

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REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

COMMUNITY INFRASTRUCTURE LEVY

1. PURPOSE OF THE REPORT

- 1.1 This report provides an update on the Community Infrastructure Levy (CIL). This is in light of the recent Examination Hearings into the Maldon Local Development Framework and the progression of the Maldon CIL Charging Schedule through to Examination, including recommended next steps ahead of the Examination.

2. RECOMMENDATION

That a review of the Community Infrastructure Levy Charging Schedule and Evidence Base is undertaken to test its suitability for Examination, and if required, Officers in consultation with the Chairman of Planning and Licensing Committee, commission further work on the Charging Schedule and Evidence Base for consideration at Examination.

3. SUMMARY OF KEY ISSUES

- 3.1 The CIL was introduced in the Planning Act 2008 as a local charge on new development towards the provision of infrastructure. It came into force on 6 April 2010 through the Community Infrastructure Levy Regulations 2010. Development may be liable for a charge under the CIL, if the local planning authority has chosen to set a charge in its area.
- 3.2 Most development which creates new floor space in excess of 100 square metres or involves the creation of one or more new dwellings is liable to CIL. However, certain types of development are eligible for relief or exempt from CIL.
- 3.3 The use of 'Section 106' planning obligations has been scaled back by the Government to ensure there is no overlap between obligations and CIL. Pooling of obligations from more than five schemes was restricted after April 2014. This is because CIL is intended to take over the collection and pooling of funds to deal with the cumulative demands on infrastructure caused by development across a district.
- 3.4 The Council submitted a Draft Charging Schedule to the Secretary of State on 12 September 2014. The Charging Schedule had been subject to public consultation at the Preliminary Draft Stage between January and March 2014, and again at Draft

stage in July 2014. The Inspector has indicated that the Local Development Plan (LDP) needs to be approved prior to the completion of the Charging Schedule.

- 3.5 The Charging Schedule sets out the rates per square metre that will be applied to qualifying developments as the basis for CIL payments. To set a CIL charge, an authority must first show there is an overall need for infrastructure funding to support the LDP, and that there is a funding gap because other sources of funding will not be sufficient to provide all that is necessary.
- 3.6 In order to do this the Council has produced an Infrastructure Delivery Plan, which is based on the draft Local Development Plan and assesses the value of the infrastructure requirements. As some of the infrastructure is being provided through the Garden Suburbs and Strategic Allocations set out in the LDP, where infrastructure is a direct requirement of those developments and will be delivered by the developers secured through Section 106 (S106) agreements, that infrastructure has not been assessed and the cost excluded from the CIL Charging Schedule.
- 3.7 As set out in the July 2014 Draft Charging Schedule, to 'set a CIL charge, an authority must first show there is an overall need for infrastructure funding to support the LDP, and that there is a funding gap because other sources of funding will not be sufficient to provide all that is necessary'. That report also concluded that 'based on the current assessment of infrastructure needs, costs and funding, there is currently a funding gap totalling £78,733,452'.
- 3.8 However, much of this infrastructure is being provided through developments. As set out in the 2014 Draft Charging Schedule 'for residential development, based on the proposed growth allocated in the Pre-Submission LDP and the charges proposed in the Annex A, we estimate CIL could raise in the region of £12.6m over the plan period to help address the funding gap and contribute to local infrastructure needs. This takes into account the Council's policy for major strategic sites to contribute to infrastructure needs they create separately via planning obligations, and so represents a significant contribution towards delivering sustainable development in Maldon District'.
- 3.9 Accordingly, the Draft Charging Schedule sought to set rates that helped achieve this gap. It is not envisaged however that CIL would necessarily fund the entire gap.
- 3.10 CIL rates are subject to testing through a viability appraisal, which assesses whether the rates would affect the viability and thereby deliverability of development. This is part of the balancing exercise between the need to fund infrastructure and also deliver new development.
- 3.11 The Council's viability appraisal which supported the Draft Charging Schedule was undertaken in 2013 and 2014. Therefore, it is now three to four years out of date. The Charging Schedule was also based on the growth strategy set out in the draft LDP, therefore it took account of the potential CIL income from qualifying development in that plan. However, due to the delay in the LDP, the Council has granted or resolved to grant planning permission for development which has met the draft LDP development requirements. As no Charging Schedule was in place, the Council was not able to collect any CIL income, but had to rely on Section 106 Agreements where appropriate.

- 3.12 The LDP Inspector, who has been appointed to chair the Examination into the CIL Charging Schedule, has asked for an update on CIL and whether the Council is relying on the evidence base to the Draft Charging schedule.
- 3.13 Given the above, there is a risk that the Inspector may deem the existing Draft Charging Schedule and evidence base is out of date, and, therefore, unsound. If that was the case the Council cannot proceed with the Charging Schedule.
- 3.14 To mitigate this risk, Officers recommend that a review is undertaken of the Draft Charging Schedule and the evidence base, prior to examination. If a review concludes that the Draft Charging Schedule and evidence base still applies, then the Council can proceed, and will have the evidence which justifies proceeding on the basis of the existing work.
- 3.15 If, however, the Draft Charging Schedule and evidence base is deemed to be out of date, the Council would have to re-consult ahead of an Examination. Whilst this would cause delays to adopting the CIL Charging Schedule, it will provide greater certainty in the longer term and reduce the risk of challenge through examination and the test of soundness.
- 3.16 **CIL Review**
- 3.16.1 The results of the Government's CIL review were published alongside the Housing White Paper on 7 February 2017. The Housing White Paper indicates that *'the Government will examine the options for reforming the system of developer contributions including ensuring direct benefit for communities, and will respond to the independent review and make an announcement in the Autumn Budget 2017.'*
- 3.16.2 The key proposed changes to CIL are:
- The introduction of a Local Infrastructure Tariff (LIT) to replace CIL by 2020.
 - LIT would be based on a national formula to which local values are applied.
 - No exemptions to LIT.
 - Replace examination on charging rates with a new process.
 - Remove the need for a Regulation 123 list (which sets out what CIL would be spent on).
 - Removal of pooling restrictions for infrastructure projects through S106 (currently restricted to five developments).
 - Proposed heads of terms for S106 agreements to be submitted with planning applications, and therefore, agreed at the pre-application stage.
 - Closer collaboration with Parish Councils in respect of infrastructure delivery.
- 3.16.3 Further announcements will be made by the Government in due course. As these provisions are unlikely to take effect until 2020, they should not affect the progression of the CIL Charging Schedule.

4. CONCLUSION

- 4.1 The Council needs to ensure that it enters into the Examination stage of the CIL Charging Schedule with a robust schedule and evidence base. An initial review of the existing Schedule will enable the Council to properly gauge whether a review of its approach to CIL is required or whether the existing work is sufficiently robust.

5. IMPACT ON CORPORATE GOALS

- 5.1 The preparation and adoption of a CIL Charging Schedule, including supporting evidence, supports corporate goals which underpin the Council's vision for the District and ensure that the infrastructure required to meet District's needs can be delivered in a timely manner.

6. IMPLICATIONS

- (i) **Impact on Customers** – The CIL Charging Schedule will enable the Council to collect CIL, provide a transition to the new Local Infrastructure Tariff regime, and deliver infrastructure for new and existing residents. It will provide developers with greater certainty that they can factor into viability assessments thereby reducing the reliance on S106 agreements.
- (ii) **Impact on Equalities** – The progression of CIL alongside the Local Development Plan and supplementary planning documents helps to provide sustainable and well planned communities which promote social cohesion and help to provide for the future needs of the local community. Planning policy documents are subject to equality analysis.
- (iii) **Impact on Risk** – CIL will lessen the risk to the delivery of infrastructure and development. It will provide greater certainty to the Council, developers, local residents and businesses and infrastructure partners.
- (iv) **Impact on Resources (financial and human)** – The cost of the CIL review, and any subsequent work on a new CIL Charging Schedule and evidence base would be funded through the identified LDP budget, which includes CIL.
- (v) **Impact on the Environment** – In delivering new development, the Council needs to ensure that infrastructure of all types, including protection and enhancement of the environment is achievable. A CIL Charging Schedule based on a robust infrastructure delivery plan, will provide greater certainty that projects in the Infrastructure Delivery Plan can be delivered.

Background Papers:

CIL Draft Charging Schedule 2014

Maldon District Infrastructure Delivery Plan 2013 and updates

Local Plan and Community Infrastructure Levy Viability Studies 2013 and 2014

Maldon District Local Development Framework and modifications

Enquiries to: Ian Butt, Interim Strategic Planning Policy Manager, (Tel: 01621 876203).



REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

APPROVAL OF WOODHAM WALTER CONSERVATION AREA DESIGNATION ARTICLE 4(1) DIRECTION AND CHARACTER STATEMENT

1. PURPOSE OF THE REPORT

- 1.1 This report relays the results of an 8-week public consultation on the proposal to designate a conservation area in the village of Woodham Walter. Following consideration of all the comments received as part of this consultation, the report seeks the Planning and Licensing Committee's approval for:
- a) designation of the proposed conservation area in Woodham Walter,
 - b) introduction of the proposed Article 4(1) Direction which would withdraw certain permitted development rights from a selection of historic houses in the area, and
 - c) adoption of the *Woodham Walter Conservation Area Character Statement* as background evidence to inform future development-control decisions affecting the conservation area.

2. RECOMMENDATIONS

- (i) that designation of the conservation area in Woodham Walter be approved;
- (ii) that an Article 4(1) Direction be introduced to withdraw certain permitted development rights from a selection of historic houses in the area;
- (iii) that the *Woodham Walter Conservation Area Character Statement* (**APPENDIX 1**) be adopted as background evidence to inform future Development Control decisions affecting the conservation area.

3. SUMMARY OF KEY ISSUES

3.1 The results of the public consultation

- 3.1.1 On 4 August 2016 the Planning and Licensing Committee authorised an eight week period of public consultation, which took place between 11 August and 6 October 2016 (Minute No. 358 refers). A document titled *Woodham Walter Conservation Area Proposal and Character Statement* was made publicly available which described the special character of the area and the implications of the proposed designation. An updated version of this document – titled *Woodham Walter Conservation Area*

Character Statement –, which it is now proposed to adopt as background evidence, is reproduced as **APPENDIX 1**. A questionnaire was sent to all residents and 17 responses were received. **APPENDIX 2** is a tabulated summary of the consultation responses received. The questionnaire sought feedback on four subjects:

- 1) the principle of designating a conservation area in the village;
- 2) the boundary of the proposed conservation area;
- 3) the proposed Article 4(1) Direction; and
- 4) the accuracy and quality of the document produced in support of the consultation.

3.1.2 The comments received in relation to each of these four areas are considered in turn in the following paragraphs.

3.1.3 All except one of the respondents supported the principle of designating a conservation area in the village. The single objection was made out of concern that the designation might hamper the future development of mixed and affordable housing in the area such that may be necessary to “allow the school and other village services to remain”. However, establishing a conservation area does not necessarily make development less likely to occur. As is explained in chapter 8 of the *Character Statement* “the purpose of the designation is not to prevent all change but to ensure that new development augments rather than erodes the special qualities of the place”.

3.1.4 None of the respondents suggested that the proposed boundary includes elements that do not merit inclusion. Five of the respondents felt that the boundary of the proposed conservation area should be drawn wider to include additional buildings or elements of the surrounding countryside. However, designating a wider area risks going against the advice contained in the National Planning Policy Framework (NPPF), that “when considering the designation of conservation areas, local planning authorities should ensure that an area justifies such status because of its special architectural or historic interest, and that the concept of conservation is not devalued through the designation of areas that lack special interest” (paragraph 127).

3.1.5 John Thompkins – a key member of the team which prepared the *Village Design Statement* – expressed disappointment that permission was in recent years granted to replace Whitegates – “the only remaining Arts and Crafts building in the village”. Mr Thompkins suggested that this property should be included within the conservation area, despite the outstanding permission to redevelop the site, in order to enable greater control over future development and to help safeguard the settings of St Michael’s Church and Bell Meadow. At the time of writing, the construction of the replacement dwelling on the site is well underway. Given the planning status of the site, and the lack of architectural and historic interest it will possess once the cottage has been demolished, it is not judged to be reasonable to include it in the conservation area.

3.1.6 One respondent requested that the proposed boundary be extended to include the “women’s club room [on Top Road] marked [on maps] as village hall”. This handsome weatherboarded building, thought to have been built in the 1920s, “is the property of the Woodham Walter Women’s Club, founded in 1906, [...] and is still running as a unique club”. While this is undoubtedly a building of local heritage value, it is somewhat divorced from the centre of the village, with six undistinguished

modern bungalows separating it from the proposed boundary. Extending the boundary of the conservation area to include the Women's Club would require the inclusion of the modern bungalows, which lack special interest, and this is not considered justifiable. However, it may in the future be appropriate to add the Women's Club to the Council's Local List of Heritage Assets, in recognition of its local architectural and historic interest.

- 3.1.7 Three respondents recommended that some or all of the surrounding fields, woodland and common land should be included within the designation. The draft character statement acknowledged that some "of the adjoining agricultural fields do form part of the wider setting in which the proposed conservation area is viewed and appreciated" but, "as they possess no 'special architectural or historic interest' in their own right", they do not merit inclusion within the boundary. In response to this suggestion, one resident highlighted the importance of Woodham Walter Hall. Indeed, Woodham Walter Hall is one of the finest 16th century moated house sites with contemporary formal gardens in the county. However, it is some distance from the village centre and is adequately protected by the designation it received in 2010 as a scheduled monument. It would not be possible to include Woodham Walter Hall within the boundary of the conservation area without also including large areas of farmland which lack interest.
- 3.1.8 All of the respondents agreed that the proposed Article 4(1) direction is justified. Two of the respondents only offered their support with the caveat that planning applications must be dealt with easily and without delay. One of these respondents also suggested that it would be "heavy-handed" to "insist that original materials and methods should always be used for repairs and improvements to the affected building". The objective of a conservation area is to preserve or enhance. Therefore, if modern materials exist on a certain property at the time of the designation, there is no obligation on owners to revert to the original materials (although this may be positively encouraged in order to achieve an enhancement). It is also accepted that modern methods, such as double glazing, can often be incorporated in ways that maintain a broadly traditional appearance and detailing, which may be acceptable in terms of the character of the conservation area. Each case will be dealt with on its own merits and officers will strive to work promptly and positively with owners to find practical solutions which meet the objectives of the conservation area designation.
- 3.1.9 Feedback on the accuracy and quality of the Character Statement was overwhelmingly positive, with various respondents describing it as "excellent", "first class" and "very well prepared and interesting". One respondent remarked that the statement was incorrect to suggest that a two-storey extension had been added to Fort Cottage earlier this century. However, the planning history shows this extension was granted permission in 2000.

3.2 The removal of the historic environment grant schemes

- 3.2.1 It is important to highlight to the Committee that the public consultation document referred to the availability of modest grant assistance from Maldon District Council in support of works of preservation and enhancement. Since this was written, the Planning and Licensing Committee agreed to support a proposal to withdraw the grant

schemes (Minute No. 705 refers). Maldon District Council now no longer offers grant assistance for the preservation and enhancement of the historic environment.

3.3 The process of designating the conservation area

- 3.3.1 Subject to the approval of the Planning and Licensing Committee, the following actions will be taken in order to formally designate the Woodham Walter Conservation Area. A notice will be published in a local newspaper and the London Gazette, a draft of which is reproduced as **APPENDIX 3**. Each of the residents within the boundary of the conservation area, the respondents to the public consultation and the Parish Council will be written to, informing them of the designation. The designation will be registered as a local land charge. Historic England will be notified of the designation. The Council's digital mapping, including the Building Conservation Interactive Map which is available to the public through the website, will be updated. Officers will make another photographic survey of the area, to capture a snapshot in time of the village at the point of designation.

3.4 The Article 4 (1) Direction

- 3.4.1 The properties that would be affected by the Article 4(1) Direction and the types of development to be brought within the remit of planning permission are the same as were proposed as part of the public consultation. The reasoning behind the Direction and its implications are clearly outlined in Chapter 8 of the Character Statement (**APPENDIX 2**).
- 3.4.2 Subject to the approval of Planning and Licensing Committee, the following actions will be undertaken in order to serve the Article 4(1) Direction as soon as practicable after the designation of the conservation area. A notice will be published in a local newspaper and the London Gazette and displayed in at least two public positions within the village for at least six weeks. A draft version of the notice is reproduced as **APPENDIX 4**. A copy of the notice, a map, and a clearly worded explanation of the direction's implications will be sent to the Parish Council and each resident effected. As the number of houses is small, these letters will be hand-delivered. The date of delivery will be the date that the Direction comes into effect. A copy of the Direction and the notice, including a copy of a map defining the area to which it relates, shall be sent to the Secretary of State on the same day that notice of the Direction is first published or displayed.
- 3.4.3 After a period of at least 28 days following the service of the notice, and having taken into account any representations received, Officers will make a recommendation to the Planning and Licensing Committee as to whether or not to confirm the notice. If the Council fails to confirm the notice, the Direction will lapse six months after it was served. In order to confirm the Direction a further notice will be published and distributed in the same manner as the original notice. A copy of the confirmed direction will be sent to the Secretary of State.

3.5 The Woodham Walter Conservation Area Character Statement

- 3.5.1 This is based upon the document produced for the public consultation titled Woodham Walter Conservation Area Proposal and Character Statement. The Character Statement which was produced in support of the public consultation has received

minor amendments in order to make it suitable as a permanent Character Statement. A final version of the document is reproduced as **APPENDIX 2** and is proposed for adoption.

4. CONCLUSION

- 4.1 Maldon District Council has a commitment to protect and enhance the natural and built heritage of the district now and for the enjoyment of future generations. By designating a conservation area and introducing an Article 4(1) Direction the Council is conveying its commitment to this.

5. IMPACT ON CORPORATE GOALS

- 5.1 The recommendations of this report will support Corporate Goals by protecting and shaping the District through the protection and enhancement of the District's distinctive character, natural environment and heritage assets, achieving high quality design in development.

6. IMPLICATIONS

- (i) **Impact on Customers** – Residents included within the new conservation area will be required to obtain planning permission to make certain alterations which are currently permitted development. This burden is, however, necessary in order for the conservation area designation to be meaningful. The duty to preserve or enhance the conservation area's special character and appearance will protect one of the District's most attractive villages for current and future customers to enjoy.
- (ii) **Impact on Risk** – The withdrawal of permitted development rights by an Article 4(1) Direction may give rise to a claim for compensation if a planning application is refused or granted subject to conditions. Compensation may be claimed for abortive expenditure or for other loss or damage directly attributable to the withdrawal of the permitted development rights (Section 108 of the Town and Country Planning Act 1990). This is considered to be a low risk. Maldon District Council has never received any successful claims for compensation in relation to any of its existing Article 4 Directions. A survey of 72 local authorities carried out in 2008 on behalf of the English Historic Towns Forum revealed no successful cases of compensation claims in relation to Article 4(1) Directions. All of the residents that would be affected by the Article 4(1) direction were consulted on the proposal and no objections to it were made.
- (iii) **Impact on Resources (financial)** – Cost of the publication of notices and printing will be met from the relevant budget.
- (iv) **Impact on Resources (human)** – Preparation and service of notices and letters by the Conservation Officer with assistance from Spatial Planning & Implementation Assistant

- (v) **Impact on the Environment** – Positive, due to the enhancement and safeguarding of both the natural and built heritage which will ensure its existence for the District now and generations to come.

Background Papers: None.

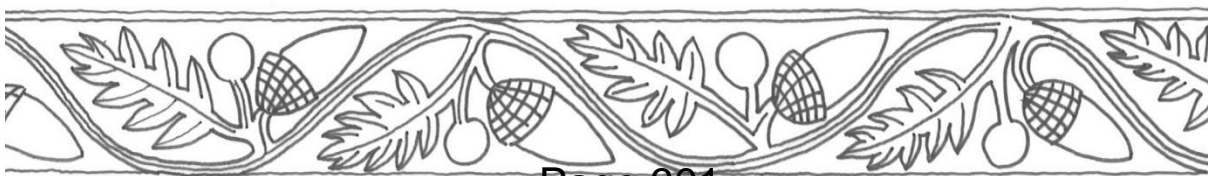
Enquiries to: Tim Howson, Conservation Officer (Tel: 01621 875725).



WOODHAM WALTER



CONSERVATION AREA CHARACTER STATEMENT



Front cover illustrations: Photograph of the Bell and Wingtons from beneath the canopy of an oak tree. The decorative border depicting oak leaves and acorns is based upon the carved decoration on the jetty bressummer of The Bell

This document was produced by Maldon District Council.

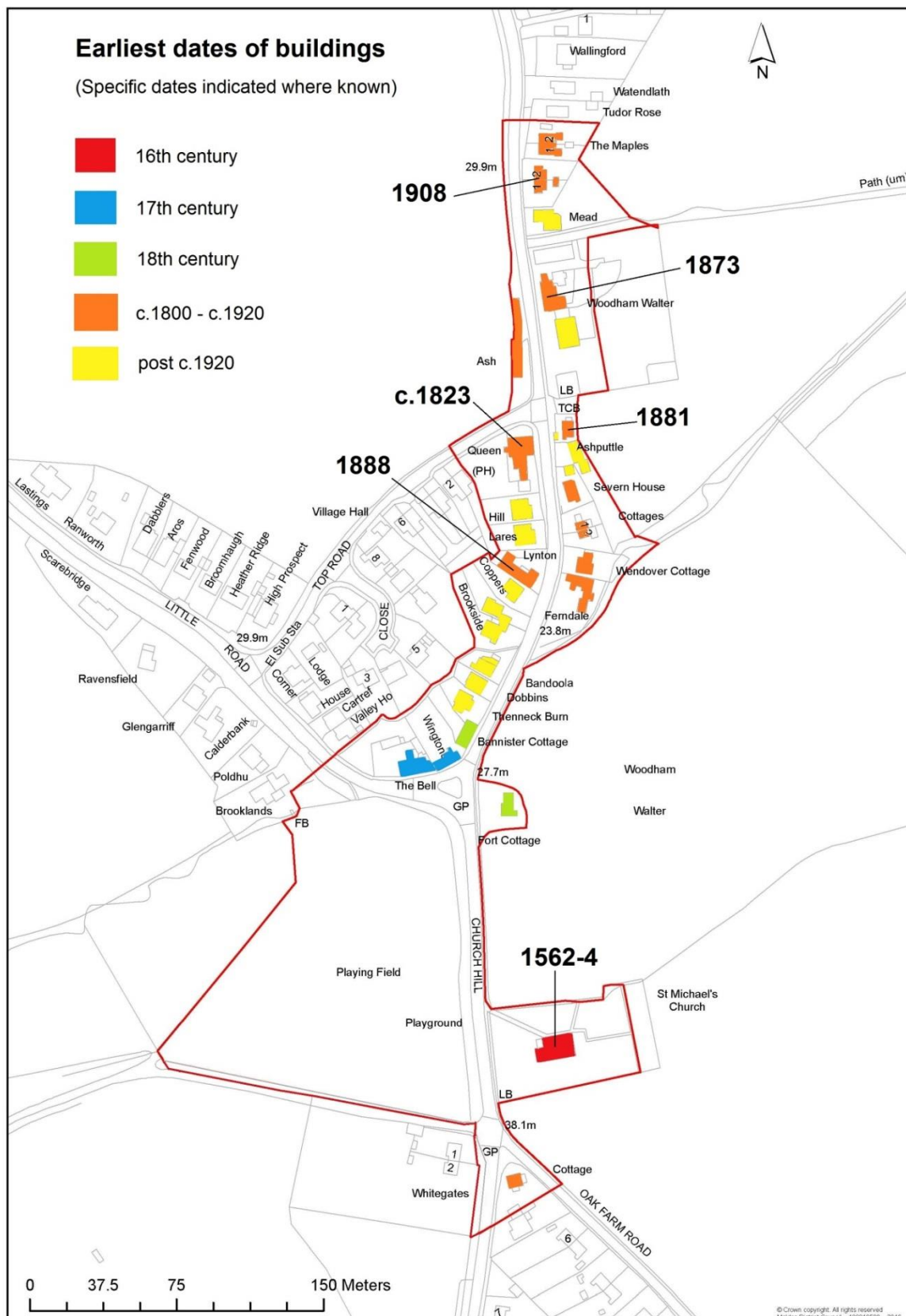
It was prepared by Tim Howson, Conservation Officer with input from Jackie Gilbert, Tree Officer and assistance from Devan Lawson, Planning Officer

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Map 1: Conservation area boundary highlighting the earliest dates of buildings

1. INTRODUCTION

Maldon District Council's Planning and Licensing Committee resolved to designate the Woodham Walter Conservation Area on the 2nd of March 2017. This is the date from which the designation came into effect. This followed an 8-week period of public consultation between the 11th of August and the 6th of October 2016, as part of which every resident within the area was written to. Those who responded to the consultation were almost unanimously in support of the area's designation as a conservation area.

Conservation areas are 'Areas of Special Architectural or Historic Interest, the character or appearance of which it is desirable to preserve or enhance' (*Planning (Listed Buildings and Conservation Areas) Act 1990*). They were introduced by the Civic Amenities Act of 1967. Local authorities have a duty to designate conservation areas, to formulate policies for their preservation and enhancement, and to keep them under review.

Woodham Walter possesses plenty of the ingredients of an attractive and vibrant historic Essex village. It boasts an ancient and beautiful parish church, a pretty Victorian primary school, a nursery housed within a Victorian chapel, a pair of gothic brick almshouses, two historic pubs, a public meadow and a good collection of old and characterful houses, all handsomely lined along a leafy street which winds down and up land that slopes either side of a stream.

While there are several modern houses within the conservation area, these are mostly set back further from the road edge than the older buildings, and their appearance is softened by attractive front boundary treatments and planting. This character statement describes the Woodham Walter Conservation Area's unique and special character and appearance which, in the words of the legislation, it is desirable to preserve or enhance.

The National Planning Policy Framework (2012) advises that 'when considering the designation of conservation areas, local planning authorities should ensure that an area justifies such status because of its special architectural or historic interest, and that the concept of conservation is not devalued through the designation of areas that lack special interest' (paragraph 127). With this guidance in mind, the boundary has been drawn to include the most valuable historic buildings and spaces whilst limiting the number of modern houses which possess no heritage value. The boundary of the conservation area is linear in form, taking in the parish church, Church Cottage and Bell Meadow to the south, and the Almshouses and 1&2 The Maples to the north, and including most of the street-fronting properties in between. Beyond this boundary, the houses are predominantly modern and architecturally unexceptional. Some of the adjoining agricultural fields are valuable insofar as they

form part of the wider setting in which the conservation area is viewed and appreciated but, as they do not possess any notably 'special architectural or historic interest' in their own right, they are not included within the boundary. To avoid confusion the boundary is generally drawn along property boundaries.

2. CHARACTER STATEMENT

The Woodham Walter Conservation Area encompasses the historic core of the rural village, the boundary drawn in linear form to take in the 16th century church of St Michael and Bell Meadow in the south up to the Victorian school and gothic brick almshouses in the north and including the various historic buildings in between. The historic settlement is a very good nucleated village with many of the traditional village elements: church, pubs, Victorian school, almshouses, and a recreational meadow. The surrounding, predominantly arable landscape supported a prosperous rural economy based on agriculture. In the past the village was composed mainly of the cottages of tradesmen, craftsmen and agricultural labourers. Several of the smaller semi-detached and terraced cottages were knocked into single larger houses in the 20th century. The old shops, whose former presence is sometimes highlighted by display bay windows, have now all closed. A golf course, established at The Warren early in the 20th century, and situated on the boundary of the conservation area, is now one of the settlement's major businesses. Situated on the fringe of the 'wooded hills' landscape region of Essex, the land slopes quite steeply either side of the brook which runs through the village, creating a lively streetscape and some impressive long views, particularly from Bell Meadow and the Church. The village is not on a main road so the settlement remains peaceful and unspoilt. The layout of the village follows a historic road pattern centred upon The Street (historically known as 'le Brook street' because it crosses the brook). A particular highlight of the village is the junction between The Street, Church Hill and Little Baddow Road; a relatively broad open area with a triangular island of grass in the centre. One side of this space is lined by a picturesque group of old buildings and the composition is complemented by a nearby oak tree and by the Maldon Ironworks fingerpost in the middle of the junction. Despite significant 20th-century infilling and replacement dwellings, the conservation area retains a rich and varied historic built environment. Properties span the centuries, exhibiting a variety of traditional building methods and materials. Many of the houses appear to nestle into their plots behind generous front hedges. The considerable number of trees helps generate an Arcadian character.

3. MATERIALS AND DETAILING

Traditional materials and detailing make a significant contribution to the character of local area.

External wall finishes

Around half the buildings in the conservation area are **rendered**. Normally the render is applied to a timber frame. A few of the modern houses have render applied to masonry. The earliest example of render over brickwork is probably Lynton House (1888), which has exposed brickwork quoining framing the rendered panels. There are also fragments of a plaster finish on the 16th century brickwork of the parish church. Wingtons has smooth render with a gently undulating surface arising from the movement of the timber frame over the centuries. Nos1 & 2 The Maples retain their Edwardian un-painted pebbledash render.



Fig. 1 Render above weatherboarding on Wingtons

All the houses pre-dating the middle of the 19th century are **timber framed**, but the framing is generally not visible on the outside of the building. The main exception to this is The Bell, which had its render removed in the



1930s to re-expose the timber frame. The studs are closely spaced in the East Anglian tradition known as “close studding” (fig. 2). In the mid-20th century Wendover Cottage had black-painted battens applied to its rendered finish in imitation of the half-timbered appearance of The Bell. More recently, the small extension added to the south side of the parish church in 2014 has an exposed oak frame in-between which there are vertical oak planks and battens, all left unpainted and allowed to silver over time (fig.3).

Fig. 2 Exposed timber framing on The Bell



Fig. 3 Un-painted oak cladding on the recent extension to the parish church



Fig. 4 White painted weatherboarding at Lynton

A more common type of timber cladding is horizontal, featheredged **weatherboarding**, normally painted black or white. This is a distinctly vernacular material which features on many rural historic houses in the Maldon District. The most notable example in the

village is probably Tadpoles Nursery (the former Congregational Chapel built in 1881). It is also found on Wingtons on the flank wall and along the bottom of the front wall. It is often found on outbuildings such as the former stables at The Bell and Lynton and on the former privies to the rear of the almshouses. Black painted weatherboarding is used to good effect on the

late 20th-century extension to Ferndale.



Fig. 5 Black painted weatherboarding at Ferndale

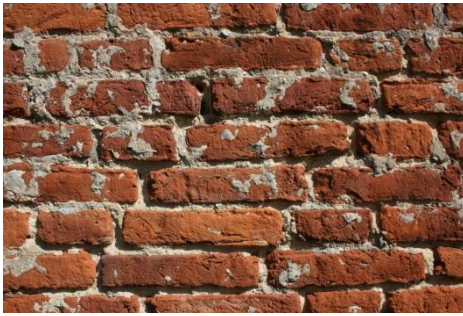


Fig. 6 Brickwork on the parish church (1562-4)



Fig. 7 Brickwork on the primary school (1873)



Fig. 8 Brickwork on the almshouses (1908)



Fig. 9 Brickwork on a house of the third quarter of the 20th century

The oldest **brickwork** in the conservation area is found on the parish church (fig. 6); an important example of Elizabethan brick architecture with delightful crow-stepped gables. It is not until the middle of the 19th century that whole houses are found built of brick (i.e. Lynton House, Redbrick Cottages and Ferndale). The use of red brick embellished with contrasting yellow or white brick banding occurs to good effect on the school and at Ferndale (fig. 7).

Before the early 20th century the bricks are predominantly soft reds, reflecting the fact that they were made from earth dug locally. The brown and yellow bricks used in some of mid-late 20th-century houses are quite different in appearance; lacking the texture and warmth of older brickwork. But some of the more recent brick developments have blended with the old quite successfully. The extension added to 1 Brick Cottages in the late 1980s and the new hall added to the school early this century use soft red bricks which are a good match for the neighbouring Victorian brickwork. The brickwork used for the plinth of the extension to the church is an excellent match for the thin red brickwork of the 1560s.

Before the early-20th century most brickwork was laid in Flemish bond. One exception is the parish church which has an unusual variation of Flemish stretcher bond. 20th century brickwork tends to be in stretcher bond and can appear quite monotonous in comparison with traditional bonds.



Fig. 10 Double-cambered clay plain tiles



Fig. 11 Single-lap clay pantiles



Fig. 12 Natural slate

Fig. 13 20th-century concrete tiles

Roof coverings

The oldest buildings in the village are roofed with handmade **clay plain tiles** laid steeply (47° to 50°). Ranging from deep orange to brown in colour, these old tiles have a textured and slightly irregular surface and a double-cambered profile (fig.10). Such tiled roofs are an important component of the special character of the area. The outbuildings to the rear of Lynton House and at the northern end of Ash Cottage are covered by distinctive orange clay single-lap **pantiles** (fig. 11).

Welsh **slate** was being imported to Maldon by sea from the 18th century and it became increasingly popular throughout the district in the 19th century. It is found on several of the mid-late 19th century buildings in the village, which tend to have slacker roof pitches as a consequence (typically 35° to 40°). Slate is also used to good effect on the recent extension to the school, where it reflects the roof covering on the Victorian building.

Brown **concrete tiles** are quite common in the village, notably on most of the mid-late 20th century houses. Their dull brown colour lacks the warmth of the older clay tiles. They have sadly also been used to replace the original roof coverings of some of the 19th-century buildings such as Church Cottage and the Queen Victoria pub, and detract from the traditional appearance of these properties as a consequence. The reinstatement of more traditional roof coverings on the older properties in the village, if and when the opportunity arises, would be positively encouraged.

Windows and doors

Old windows and doors can be rare and beautiful items, equivalent to antique chairs or tables in cultural and aesthetic value. Wherever possible they should be conserved rather than replaced.

There are a wide range of window types and dates throughout the village. Lead-glazed windows, and vernacular windows which were originally lead glazed, are a notably common feature of the village, although all the lead glazing itself appears to date from the 20th century. The oldest windows in the village are the early 17th-century ovolo moulded mullion windows in The Bell which were uncovered and restored in the 1930s (fig. 14). Wingtons also has one small and easily overlooked moulded mullion window of the 17th century on the first floor of its front elevation (fig. 15). To the right of this there is a vernacular flat-framed casement window dating from the 18th century. Although now plain glazed, the horizontal saddle bars reveal that it originally had lead-glazed panels. Bannister Cottage has similar casement windows which were originally lead glazed. Later examples of lead glazing are found in the almshouses where they are housed in brick gothic arched openings (fig. 16), in the extension to the south of the church and in Fort Cottage where the 20th century window frames are of unpainted oak (fig. 17).



Fig. 14 Early 17th-century mullion window



Fig. 15 Early 17th-century mullion window to left of 18th-century casement window



Fig. 16 Early 20th-century lead-glazed windows with pointed arch heads



Fig. 17 Lead-glazed windows in unpainted oak frames

Some of the painted timber windows at Tadpoles Nursery (fig. 18) have arched heads in reference to the original ecclesiastical function of the building. The bay windows at the east end of The Bell and the west end of Wingtons probably relate to the former presence of shops in these parts of the properties. Casement windows with asymmetrical fanlights replaced the sash windows in the Queen Victoria pub in the 1960's, considerably eroding this building's traditional character in the process. Vertically sliding sash windows dating from the 19th and early 20th centuries are relatively common and attractive features. More unusual is the horizontally sliding sash on the front elevation of Wingtons (fig. 19). Where plastic windows have been inserted they generally fail to replicate the traditional detailing and appearance of painted timber sash windows, demonstrated by a comparison of 1&2 The Maples (fig. 20). But the modern green-finished metal windows on the extension to the school prove that contemporary-style fenestration can appear sympathetic on new buildings in historic areas (fig. 48).



Fig. 18 Arch headed casement window



Fig. 19 Horizontally sliding 'Yorkshire' sash



Fig. 20 Original painted timber windows and plastic replacement windows

Doors are generally less prominent features than windows in the conservation area but there are good quality examples of historic and modern timber doors. Notable historic doors include the front door on Bannister Cottage, which is of a traditional four-panel design (fig. 21), and the front door of Wingtons, which is of six panels with the upper two glazed (fig. 22). Sympathetic 20th-century doors include those on the front of The Bell and the unpainted oak door on the front of Severn house (fig. 23).



Fig. 21 Four-panelled door



Fig. 22 Six-panelled door



Fig. 23 Timber door with vertical batten staining

External decoration

External decoration is very limited but of special note is the original carved decoration on The Bell. The gable bressummer was carved with a renaissance strapwork design with pendants of a similar style hanging from each end (fig. 24). This decoration helps date the building to the early-17th century. The first-floor jetty bressummer is carved with a band of interlocking ovals and lozenges above a band of oak leaves and acorns (see front cover), the latter perhaps in reference to the wooded character of the village. There are also ornamental terracotta blocks in the top of the gable to Lynton House. These blocks, primarily in the form of floral and foliate motifs including sunflowers, were highly popular in the 1870s and 80s.



Fig. 24 Strapwork carved decoration



Fig. 25 Ornamental terracotta blocks

4. BOUNDARY TREATMENTS

Boundary treatments are an important form of townscape enclosure, and their quality has a considerable impact upon the character of the area. Woodham Walter is notable for some good traditional boundary treatments. A number of properties have mature front hedgerows which can make houses appear to nestle into their plots, and contributes to an Arcadian character. Such hedgerows complement the setting of traditional properties such as Church Cottage and 2 The Maples, but can also soften the presence of more modern buildings and the clutter of parked cars as at Bandoola and Coppers.



Fig. 26 Mature front boundary hedge outside Church Cottage

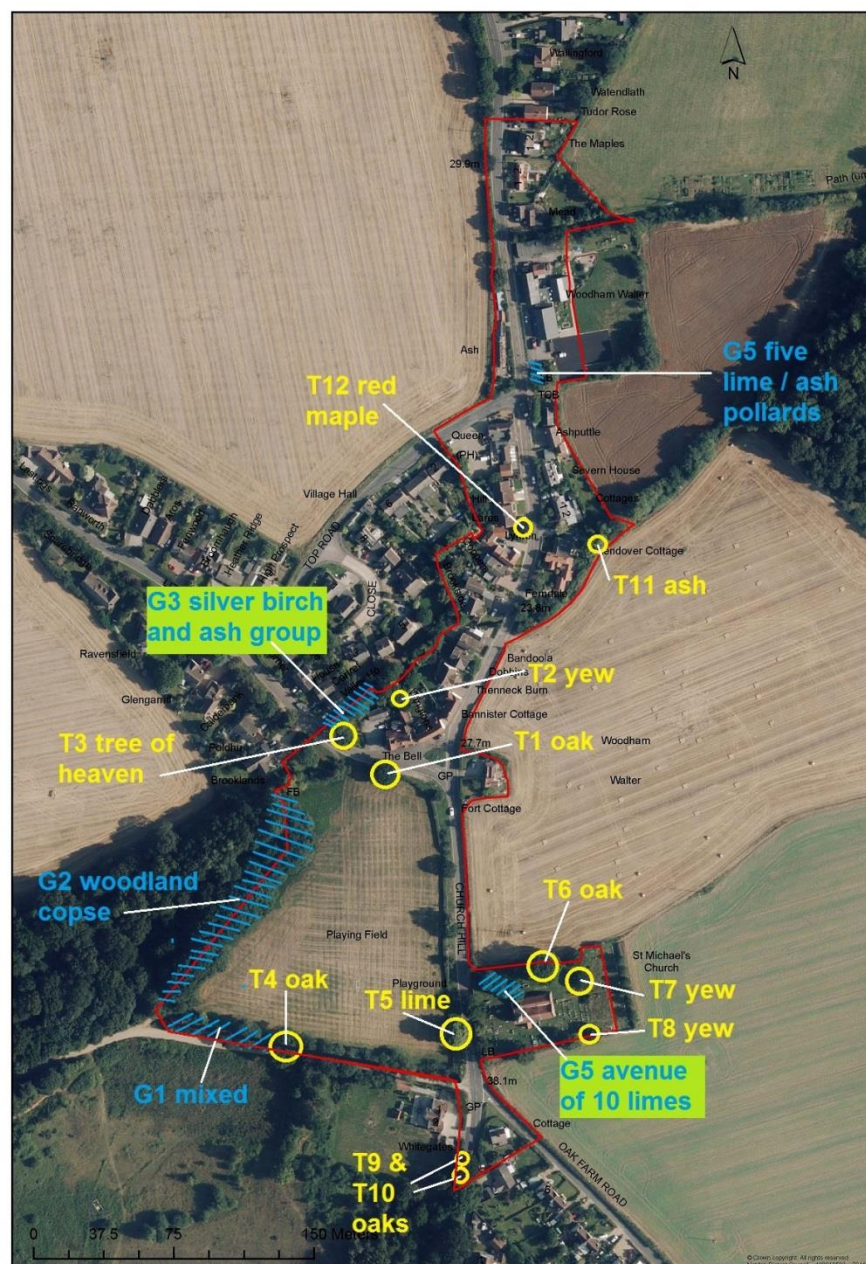
Low picket fences are highly characteristic of villages in this part of Essex and are found painted white outside Wendover Cottage and Brookside, and stained brown outside the school and the almshouses. Unusual metal railings with arched heads are used to good effect outside the parish church and Lynton House.



Fig. 27 Front railings outside Lynton House

5. TREES

Woodham is a Saxon name meaning settlement in the wood (Ryan, p. 10). Mature trees are a particularly valuable aspect of the village's special character. The best trees, and those which most deserve special care and retention where possible, are highlighted on the map below. Especially valuable trees, in terms of their contribution to the character of the village, include the oak tree opposite The Bell and the woodland copse which fringes the western side of Bell Meadow. Those trees not highlighted on the map may still have some amenity value by contributing to the general wooded character of the village.



Map 2: Aerial view indicating important trees



Fig. 28 The oak tree opposite The Bell has a particularly important relationship with the historic buildings nearby

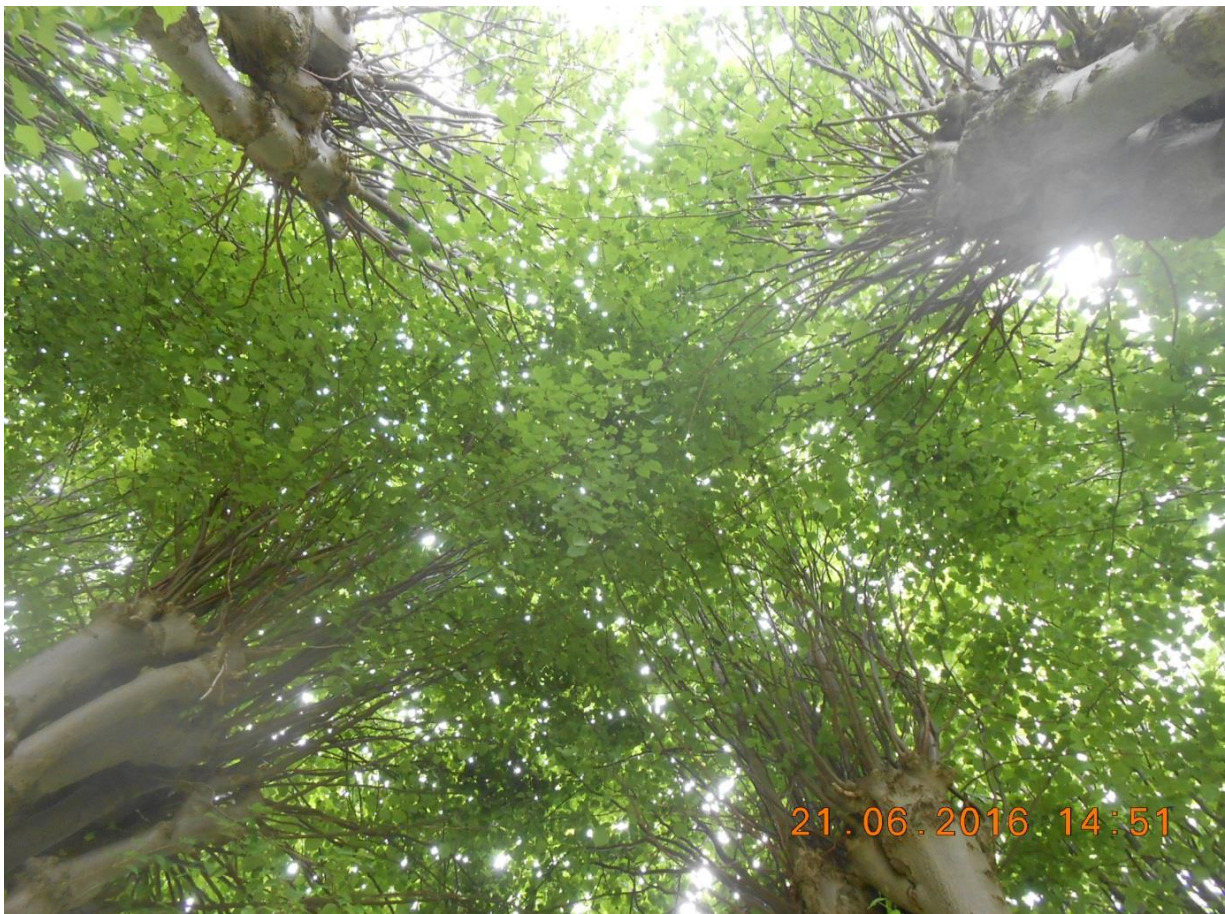


Fig. 29 Looking up into the canopies of the avenue of limes which line the church path

6. STREET FURNITURE

There are several examples of traditional street furniture within the conservation area which make a positive contribution to the special character of the village. As with the historic buildings, these are features which merit sympathetic maintenance and retention. Of particular note are two black and white painted cast iron **fingerposts** made by Maldon Ironworks in the first quarter of the 20th century. One is in front of Church Cottage on the junction between Church Hill and Oak Farm Road, and the other is opposite The Bell on the junction between Church Hill and Little Baddow Road (fig. 30). There are several good traditional **benches**, the best being two green painted wrought-iron benches with scroll shaped arm rests, one on Bell Meadow and another inside the bus shelter (fig. 31). A plaque inside the **bus shelter** explains that it was erected in the late 1940s by the British Legion of Woodham Walter as a war memorial. The shelter's modern brickwork is softened by the ivy growing up its front at the time of the survey. The red painted **phone box** and **post box**, situated in front of Tadpoles Nursery, are attractive and familiar features in the village scene, located at a focal point of the village in close proximity to the School, The Queen Victoria pub and the bus shelter (fig. 32).



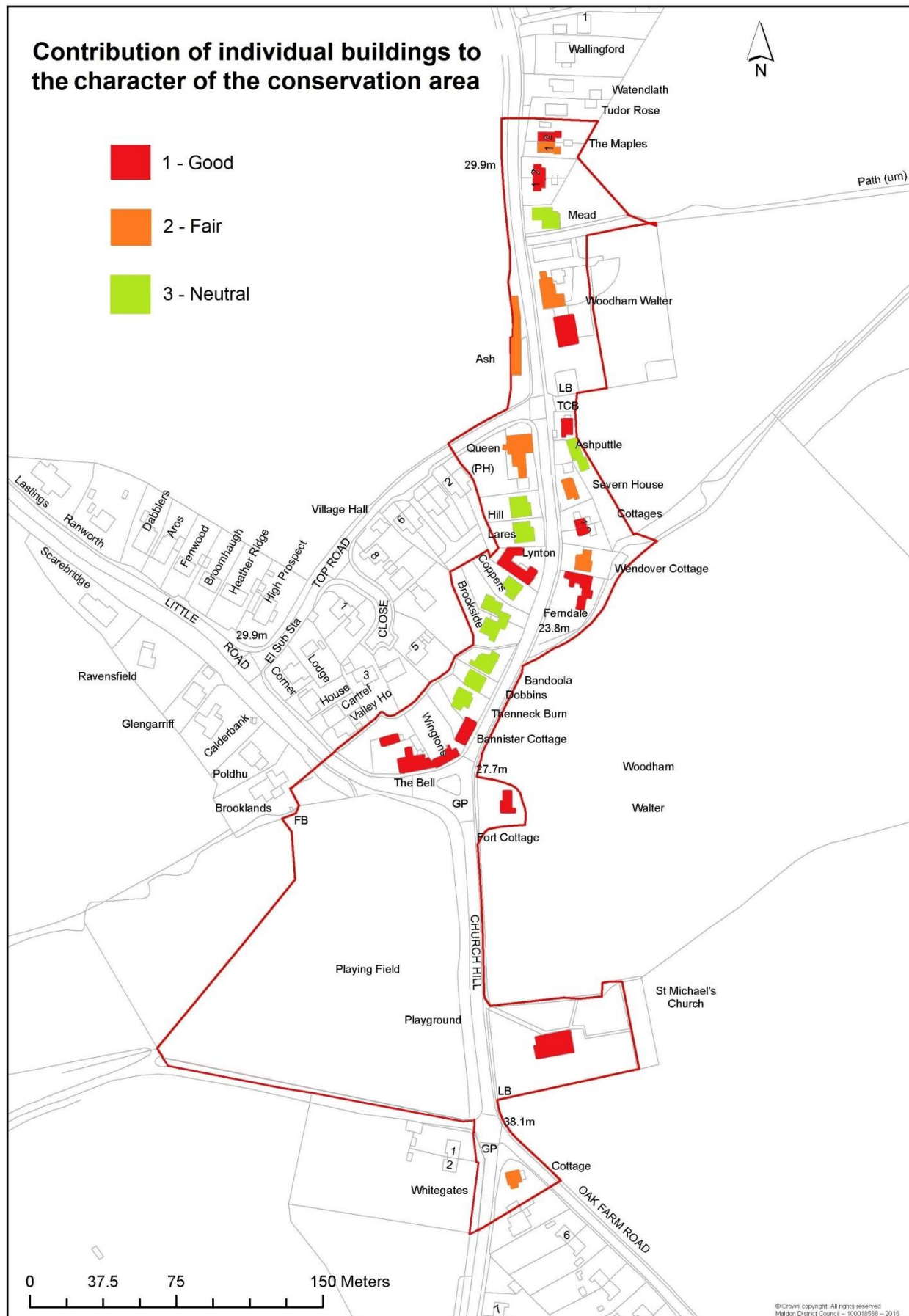
Fig. 30 Cast-iron fingerpost opposite The Bell



Fig. 31 Wrought-iron bench on Bell Meadow



Fig. 32 Phone box, post box and war memorial bus shelter



Map 2: Contribution of individual buildings to the special character of the area

7. AREA ANALYSIS

In *Map 2* each building has been assessed and its contribution to the appearance and character of the conservation area graded. Whilst this system is subjective, it aims to provide a guide to aid the planning process. The criteria for the grading are as follows:

1. “Good” = listed and unlisted buildings which make a significant positive contribution to the conservation area. They exhibit few, if any, unsympathetic alterations and may represent important landmarks
2. “Fair” = buildings which make a positive contribution through design, age, materials, siting or detailing, but which may have incurred alterations that do not relate well to the special character of the conservation area. Enhancements may increase significance to 1.
3. “Neutral” = buildings which have a neutral effect on the character and appearance of the conservation area. These are all modern houses which possess no heritage significance.

To provide greater detail and a more thorough assessment, the following paragraphs describe the components which make up the special character of the conservation area. Individual buildings and areas are analysed, beginning with the parish church then working in a clockwise direction down to Church Cottage, up the west side of The Street and Rectory Lane and back down the east side of these streets.



Fig. 33 The parish church view from the east

The Parish Church of St Michael is the most important historic building within the boundary of the conservation area. A grade II* listed building, the church is historically notable for being the only church in Essex to be built during the reign of Elizabeth the first. It was constructed in 1562-3 and consecrated in 1564. Its medieval predecessor is thought to have been located close to the site of Woodham Walter Hall ½ a mile to the south east. The church is quite small in scale, constructed primarily of soft red bricks with crow-stepped gables and a bellcote topped by a shingle clad spire. Interestingly for the date, the windows are all still perpendicular gothic in style. Some of the windows may originally have been recycled from the medieval church, although they now appear to be Victorian replacements. A small extension, employing high quality traditional materials, was added to the south side of the church in 2014. The extension, designed by Gerald Barret and built by Robbie Wilks, is similar in form to a long lost porch which used to occupy the same position and which features in an 18th-century drawing. The architectural features and history of the church are described in greater detail elsewhere and need not be reiterated here (i.e. the list description, Pevsner and Ryan)

The **churchyard** is an attractive and important communal space. It contains some valuable trees including the avenue of limes which flank the footpath from the road, an ancient oak on the northern boundary and a couple of yew trees. The oak entrance gate and the hoop-topped iron railings along the northern edge of the church yard are good quality traditional boundary treatments. Looking north from the churchyard there are important views of the village in the distance.



Fig. 34 Church Cottage viewed from the churchyard

Situated at the southern extremity of the conservation area, **Church Cottage** was first built in the Victorian period as a pair of cottages and was converted into a single dwelling in the 20th century. The house occupies the triangle of land at the junction of Church Hill, Herbage Park Road and Oak Farm Road. The plot is raised above road level making the building appear prominently in the streetscene, despite its relatively modest scale. The building's best feature is a bold 17th-century style red-brick chimney stack comprising four grouped lozenge-section shafts and moulded caps and base. The cottage is a full 2 storeys at the front, dropping to 1 ½ storeys to the rear under a catslide roof. Certain modern alterations – such as the brown concrete roof tiles, the uPVC windows and uPVC conservatory – detract from its traditional character. But the silhouette of the building remains striking and attractive and is complemented by the mature hedge along its front boundary and the oak trees to the south-west of it.



Fig. 35 The view north from Bell Meadow

Bell Meadow is a very important communal space with an intimate relationship with the village. It is bounded by mature woodland to the west, and established hedgerows and occasional trees along its other boundaries. The meadow slopes steeply downwards to the north-west, in the direction of the brook. The hillside affords very attractive views of the nucleated part of the village. It also hosts the annual popular Bell Meadow Day in the summer.



Fig. 36 The Bell, Wingtons and Bannister Cottage

The junction between Church Hill and Little Baddow Road is a highly attractive and memorable space, enclosed by picturesque old houses to the north and the steeply sloping Bell Meadow to the south. It is a triangular space with an island of grass in the centre which hosts a black and white painted fingerpost manufactured by Maldon Ironworks; an important historic feature in its own right. On the north side of the junction, The Bell, Wingtons and Bannister Cottage (each described in more detail below) comprise one of the most winsome groups of historic buildings in the Maldon District. The oak tree immediately to the south-west of The Bell is a notably valuable element in the composition; framing views of the historic buildings and at certain times of day filtering dappled sunlight onto The Bell.



Fig. 37 The Bell

Apart from the parish church, **The Bell** is the most significant historic building in the conservation area. It is grade II listed, although such is its architectural quality that arguably it could merit upgrading to II*. Its construction, layout and decoration all indicate it was built early in the 17th century. Around 1930 the southern and western elevations were restored to their original appearance with a commendable degree of accuracy. The building's close-studded timber-framed construction was re-exposed and lead-glazed windows were reinstated. The exposed timbers have been painted black, as was fashionable early in the 20th century but was probably not the finish in the 17th century. It was (and still is) quite a showy building, with most of the finances thrown at the front elevation which has carved decoration and almost continuous glazing. Notably there is no evidence of any original windows on the other elevations.

The most elaborate part of the building is the crosswing, at the left-hand end, which has a jettied first floor and a jettied gable. As described above (see p. 13), the crosswing has lavish carved decoration. The central windows on the crosswing originally projected as an oriel and bay. Many of the smaller windows retain their original mullions. The single-storey Georgian eastern extension was occupied by a shoemaker's workshop in the 19th century (Ryan, p. 84). The continued use of the building as a pub makes a valuable contribution to the vitality and character of the village.



Fig. 38 Wingtons

It is not obvious from the outside, but **Wingtons** (listed grade II) also dates from the 17th century, albeit from a little later in that century than The Bell. On the front elevation there is a small first-floor window with a moulded mullion (second from left) which is original, and there are features inside the building which confirm the 17th-century date. However, unlike The Bell, the charm of Wingtons derives less from its original features, than from the way in which it has been adapted organically over the centuries in a vernacular fashion. There are windows on the front elevation dating from the 17th, 18th, 19th and 20th centuries, and no two are the same! On the ground floor there are three bay windows. In addition to vertically sliding sash windows, there is one horizontally sliding sash (also known as a Yorkshire sash), and two wonderful Georgian casement windows of a type which has rarely survived in the Maldon District.

The upper parts of the front elevation are rendered and the lower parts are clad in weatherboarding, but the junctions between these two types of cladding are at variable levels. The eastern flank is clad in weatherboarding. According to Pat Ryan: “The property was leased by Thomas and Hannah Shynn, shopkeepers who first appeared in the parish registers in 1731. All the rooms listed in James Brown’s inventory of 1794 can be identified in the house as it stands today” (Ryan, p. 91).

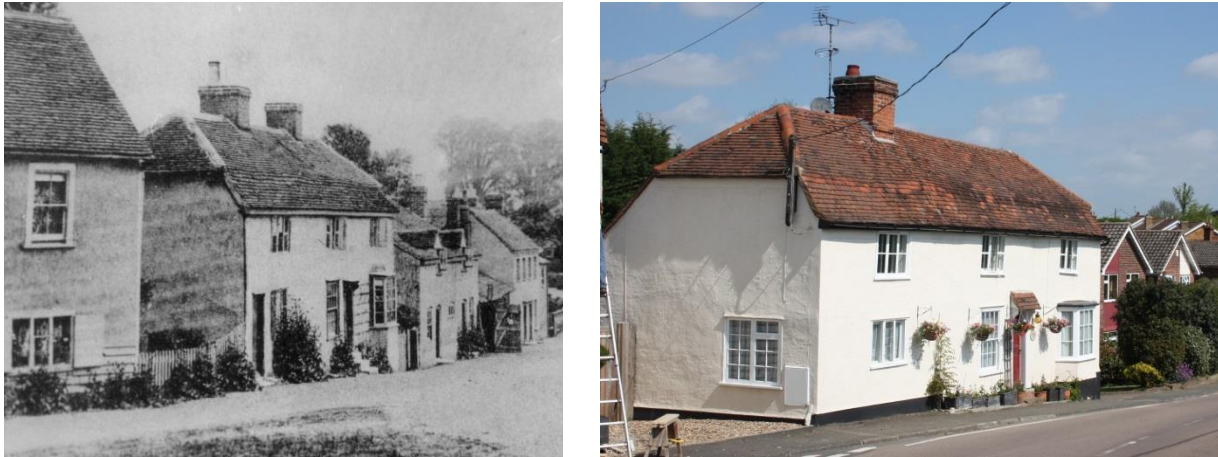


Fig. 39 Bannister Cottage in the early 20th century (left) and in 2016 (right)

To the north of Wingtons is **Bannister Cottage**, another grade II listed building. It is a classic example of local Georgian vernacular architecture, having a half-hipped gambrel roof and some formerly lead-glazed casement windows typical of the period. In the 18th century the building was in use as a pair of farmworkers’ dwellings (Ryan, p. 43). In 1801 the building was purchased by the parish vestry for use as a poor house, at which time it was probably extended by a further bay to the north, and it remained in this use until 1834, after which it reverted to three cottages. Apart from infilling the front doorway that existed at the left-hand end of the front elevation and the loss of a chimney the external appearance of the building has changed very little.



Fig. 40 Modern houses on the west side of The Street between Bannister Cottage and Lynton House

Between Bannister Cottage and Lynton House there are six modern houses built from the 1960s onwards. From south to north these are **Thenneck Burn, Dobbins, Bandoola, Brookside Colt House, and Coopers**. These houses are typical of their date and make a neutral contribution to the special character of the conservation area. The houses are set back further from the Street than the older houses, and their modern appearance is softened by some well-established trees and hedges.



Loading up the bread outside Allingtons Bakery,
about 1907



Fig. 41 Lynton House early in the 20th century (left) and in 2016 (right)

Lynton House is a late-Victorian brick property comprising a gabled crosswing to the left and a narrow 'hall' range to the right. The building is rendered except at the corners of the walls and around the windows, where the brickwork is exposed and arranged in the fashion of masonry quoins, creating quite a distinctive appearance. The gable to the crosswing is embellished with ornamental terracotta blocks primarily in the form of floral and foliate motifs including sunflowers. The original front door and porch, which feature in old photographs, have been removed and a doorway which once existed on the flank of the 'hall' range has been converted into a window. But the frontage is otherwise little altered.

The front fence is of an unusual, but charming, design comprising wooden post and rails surrounding iron balustrades that intersect in pointed arches (fig. 27). Attached to the rear of the house are structures which, early in the 20th century, were used as a flour store and bakery. Flanking the northern side of the driveway is an attractive former stable building clad in weatherboarding and roofed in Roman clay tiles (fig. 4).

A study of the 19th century OS maps indicates that this property was built between 1881 and 1896. Apparently there is a date of 1888 inscribed in the rear gable of the original house, now concealed within the roof of the flour-store extension.



Fig. 42 Lares and Robin Hill

To the north of Lynton are **Lares** and **Robin Hill**. These are two detached houses built at the same time in the 1970s, each incorporating an integral garage. They are constructed of yellow brick and roofed in brown concrete roof tile. On each house the right-hand end of the front elevation is stepped back and rises to taller eaves. The two houses are typical of the period and make a neutral contribution to the special character of the conservation area. They are quite prominent due to the way in which the land rises up from The Street. Lares has a pleasant Maple tree in its front garden. The planting of front hedgerows would augment the villages' Arcadian quality and soften the appearance of these modern houses.

Fig. 43 The Queen Victoria PH early in the 20th century (left) and in 2016 (right)

Occupying a prominent position on the corner of the junction between the The Street and Top Road is the **Queen Victoria PH**. The timber-framed structure of this pub represents three former houses. These were part of a terrace which originally comprised six houses – one facing The Street and five facing Top Road – and was described as ‘newly built’ in 1823 (Ryan, p. 89). The Queen Victoria is mentioned in the 1861 census at which time it occupied the eastern end-house of the terrace. In the 1960s, the western three houses were demolished to make space for a car park and the pub expanded into the remaining houses.

Although the historic form of the building is still apparent, some of the 20th-century alterations detract from its traditional character. The reinstatement of timber sash windows and the use of more traditional or softer boundary treatments would hugely enhance the appearance of this prominent building. The continued use of the building contributes positively to the vitality and character of the village.



Fig. 44 Ash Cottage

Occupying a narrow roadside plot on the west side of Rectory Road is **Ash Cottage**. This is a small, single-storey cottage. Roadside cottages are a common feature of the Maldon District, with most examples dating from the late-eighteenth and early-nineteenth centuries. Ash Cottage is quite a late example since it dates from early in the 20th century. The cottage nestles into the plot, its eaves only just peeping above a mature front hedge. The building has a long, linear form with two outbuildings attached to the northern end. The roof of the main house is covered with early concrete tiles laid in a lozenge pattern, and the outbuildings are roofed with corrugated tin and Roman clay tiles.

Properties on the east side of Rectory Road and The Street



Fig. 45 2 The Maples (left) and 1 The Maples (right)

When drawing the boundary of the conservation area, it seemed logical that the northern-most properties to be included should be **1 & 2 The Maples**. Beyond this the houses are predominantly modern and of no special interest. 1 & 2 The Maples are a handsome, symmetrical pair, the essential form of which is unaltered. A particular feature is the lean-to veranda roof which is continuous across the front elevation. The pebbledash render is probably original but the concrete roof tiles are not. Both houses retain wavy-profile bargeboards in their gables.

The front elevation of No. 2 is almost perfectly preserved, retaining the original windows and curious lattice-pattern brackets to the veranda. By contrast, the façade of No. 1 has been altered by the insertion of plastic windows and by the infilling of the veranda; alterations which have eroded that property's traditional appearance. The box hedging in front of the properties is in keeping with the village's Arcadian character. The loss of the front garden of No. 1 to concrete hardstanding has resulted in a harder appearance.



Fig. 46 1 & 2 The Almshouses

Immediately to the south of The Maples are the red brick **Almshouses**, which make a very positive contribution to the character of the conservation area. A carved stone plaque on the front reads:

ALMSHOUSES IN THE MEMORY OF
THE REV
R.H. FAULKNER. M.A.
RECTOR OF THIS PARISH 1875-1907.
ERECTED BY HIS THREE SONS
1908.

The best architectural features of these tiny houses are their gothic-style windows, which have pointed-arch openings and lead-glazed panes. The porches are later, but broadly respectful of the proportions of the original building. There is a good picket fence and pretty planting along the boundary. To the rear there is a small outbuilding, possibly former privies, clad in black weatherboarding and roofed in clay tiles.



Fig. 47 Woodham Mead

Woodham Mead is a modern house, built early in the 1980s, of loosely traditional form, which makes a neutral contribution to the character of the conservation area.



Fig. 48 Woodham Walter Primary School

A school was first built on the site of **Woodham Walter Primary School** shortly after 1829, and this was replaced by the current building in 1873 (Ryan, p. 61). The Victorian school was erected by the village builders, the Gozzetts. The original school building is a modest and fairly typical example of its type and date. But it is an important building in the context of the village. The Victorian building is built of soft red brick laid in Flemish bond with yellow / white bricks used for banding and to enliven brick arched heads. The plastic windows unfortunately dilute its Victorian character to some degree. The school features quite prominently in long views because it is sited upon the crest of the hill.

The modern extension adjacent to the road, added early this century, is a thoughtful and sensitive response to the character of the site. It employs warm, soft red bricks and slate-pitched roofs, in reference to the original school, and more contemporary-style fenestration in green-coloured frames which is quite effective.

The modern 'portakabin'-type structure at the northern end of the site, close to the road, is not attractive, but is relatively unassuming and a common and accepted feature of schools nowadays, ever under pressure to find additional space. The picket fence and box hedge front boundary is good, and successfully softens the appearance of the car park.



Fig. 49 Tadpoles Nursery

What is now **Tadpoles Nursery** was first built as the Congregational Chapel in 1881. Services and a Sunday School were held in the chapel until after World War II. It later became the village shop (the 'Chapel Stores'), before recently becoming Tadpoles Nursery. It is a charmingly vernacular building, clad in white-painted weatherboarding, its roof covered with natural slates. Its most memorable features are the pointed-arch timber windows, which are the only external sign of the building's formerly ecclesiastical function. The building is now partially hidden behind a row of trees, which are in themselves a pleasant feature in the street scene.



Fig. 50 Ashputtle

Ashputtle was built around 10 years ago, replacing a bungalow of the same name. The bungalow – a red brick structure with lead-glazed windows – was originally built shortly after the First World War as a village reading room. The new house is of a loosely traditional form, and makes a neutral contribution to the character of the village.



Fig. 51 Severn House early in the 20th century (left) and in 2016 (right)

To the south of Ashputtle is **Severn House** which was built in the mid-19th century as a timber-framed pair of semi-detached cottages. The cottages were converted to a single dwelling in the 1960s. The basic form of the building remains unchanged but there have been a number of modernisations. The sash windows have been replaced with timber casements which have top-opening fanlights and (by traditional standards) quite chunky detailing. A roughcast render has been applied over the building, including over the brickwork of the chimney stacks. The trees in the front garden are quite attractive.



Fig. 52 2 Brick Cottages (left) and 1 Brick Cottages (right)

Redbrick Cottages are another pair of mid-19th century cottages, this time of red brick with a hipped slate roof. In the late 1980s No. 1 was extended to the side in a matching style to the original. The front elevation of the cottages, including the modern side extension to No. 1, retains fine painted-timber sash windows. At the time of the survey for this appraisal the front hedge had grown to the point that it largely conceals the front elevation from view, which is a shame since the cottages make a valuable contribution to the traditional character of the village.



Fig. 53 Wendover Cottage (left) and Ferndale (right)

Wendover Cottage is a 19th-century house which has been 'Tudorised' in a playful manner in the mid-twentieth century, through the introduction of lead-glazed windows and black-painted battens to convey the impression of exposed timber-framing. The front picket fence has even been painted black and white to reflect the half-timbered effect of the house. The 19th-century form of the house is otherwise essentially little altered, retaining slate roofs and a gable-end brick chimney stack. Although the 'Tudorised' features are inoffensive, the restoration of the building's Victorian character would represent an enhancement.



Fig. 54 Ferndale

The gabled front of **Ferndale** provides a visual stop to the housing on this side of the street. It is a late 19th-century brick house, orientated with a gable facing the road. Like the school it is predominantly of red brick but deploys some horizontal bands of yellow brick. The property was quite extensively altered in the 1990s but, due to the way in which the land slopes down from the road and the presence of a mature front hedge, the extensions are not that noticeable, and the historic building remains the most prominent element in views from the street.



Fig. 55 Fort Cottage

Somewhat isolated on the eastern side of Church Hill, and perched on elevated land overlooking the junction with Little Baddow Road is **Fort Cottage**. It is probably Georgian in date but possibly earlier. It is known to have been occupied by two families in the mid-19th century (Ryan, p. 86). A two-storey extension was built on the north side of the cottage earlier this century. Prior to that, the two-storey front range was covered by a roof that is fully hipped to the north and half hipped to the south. The cottage has been extensively altered but in a manner which respects the spirit of the original, using hipped clay tile roofs and oak-framed and lead-glazed windows. The roof of the side extension is set back so that the form of the earlier roof can still be seen and appreciated.

There is a tall hedgerow along its boundary with Church Hill but, thanks to its elevated position, the cottage appears prominently in views from several directions. The most notable views of the cottage are from the junction with Little Baddow Road – where it is framed by the canopy of an oak tree – and from Bell Meadow – where its setting within the wider, partly-wooded and rolling landscape is highly picturesque.

8. THE CONSEQUENCES OF CONSERVATION AREA DESIGNATION

The objective of a conservation area designation is to preserve or enhance the special character or appearance of a place. Once a conservation area has been designated it becomes the duty of the local planning authority to consider the desirability of this objective when determining any planning applications which affect it or its setting. The purpose of the designation is not to prevent all change but to ensure that new development augments rather than erodes the special qualities of a place.

Establishing a conservation area immediately places firmer planning controls over certain types of development. The main additional requirements include the following:

- 6 weeks' notice must be given to the local authority before works are carried out to any tree in the area. This does not include trees whose trunks have a diameter of less than 75mm above ground level
- Planning permission is required for the demolition of any unlisted building with a volume of more than 115 cubic metres
- Planning permission is required to demolish a gate, fence, wall or railing over 1 metre high next to a highway (including a public footpath or bridleway) or public open space; or over 2 metres high elsewhere
- Planning permission is required for alterations to the roof of a dwelling house resulting in a material alteration to its shape, notably dormer windows
- Planning permission is needed for the installation of a satellite dish on a chimney, wall or roof slope which faces onto and is visible from a public highway
- Planning permission is needed to clad any part of the exterior of a dwelling house with stone, artificial stone, timber, plastic or tiles

Unless the local authority introduces further restrictions called Article 4 directions many minor, but potentially harmful, alterations can still be carried out to dwelling houses without planning permission. So, for example, without an Article 4 direction the owners of independent dwelling houses can replace front windows and doors, change roof coverings, paint over exposed brickwork and demolish chimneys. While such alterations may be of little consequence if carried out to modern houses (whose contribution is judged to be neutral), if such changes were made in an unsympathetic manner to any of the historic properties in the village, the special character of the area could be seriously undermined. In order for the conservation area designation to be meaningful an Article 4 direction is being introduced at the same time relating to the following properties:

- Church Cottage
- Lynton House
- Ash Cottage
- 1&2 The Maples
- 1&2 Almshouses
- Severn House
- 1&2 Brick Cottages
- Wendover Cottage
- Ferndale
- Fort Cottage

The effect of the Article 4 direction is that the following types of development would require planning permission:

- The alteration of front doors or windows, including the formation of new windows and doors.

REASON: This is to discourage the loss of historic windows and doors which contribute positively to the character of the area in the instances where it is possible to repair and retain them. It is also ensure that new windows and doors respect the special character of the conservation area in terms of design, detailing and materials and to discourage the introduction of windows and doors of unsympathetic design. With respect to any elevation on the house which faces a highway, complete replacement of a window or door or formation of a new window or door opening therefore require planning permission. The need for planning permission does not extend to such alterations on elevations which do not front a highway.

- The alteration of the front roof slope, including the replacement of the existing roof coverings with a different material or the installation of a micro-generation unit.

REASON: This aim of this measure is to ensure the continued use of roof coverings that are most in keeping with the special character of the Conservation Area, such as natural slates and clay tiles. While Maldon District Council supports efforts towards sustainability and energy efficiency, this measure will also ensure that micro-generation units are installed in a way that does not detract from the special character of the property or the conservation Area. Planning permission will be required when the front roof slope is completely re-covered with a different material or a micro-generation unit is attached to the front roof slope. Planning permission will not be required in the instances when re-roofing only involves replacing existing slates or tiles using materials which match the existing exactly.

- The painting over of unpainted brickwork.

REASON: Traditional historic brickwork was usually not meant to be painted and when it is painted it can seriously harm the character of a building. Once a historic brick wall has been painted it is difficult and expensive to expose again. This measure would therefore seek to discourage the painting of areas of unpainted brickwork which were always meant to be exposed, where its character as such is deemed to be important.

- Alterations to or the removal of a chimney on a house or buildings associated with the house.

REASON: Chimneys make an important contribution to the roofscape of the Conservation Area. This measure will encourage the retention of historic chimneys where they make a positive contribution to the character of the Conservation Area

Planning applications which are required as a result of the Article 4 direction are currently free of charge.

9. CONCLUSION

Woodham Walter is a very special rural village, with a good combination of attractive buildings, open spaces and trees. The rolling landscape creates a lively streetscape and some impressive views. The numerous mature trees and hedges generate a verdant and Arcadian character. Within the boundary of the conservation area there is a special character and appearance which it is desirable to preserve and enhance. On this basis, its status as a conservation area is justified. Such status, informed by this character statement, will help ensure that the local planning authority, local residents, developers and other organisations make considered decisions about changes affecting the conservation area and preserve its special character for future generations.

10. BIBLIOGRAPHY

Bettley and Pevsner: *The Buildings of England: Essex* (Yale, 2007)

Ryan, P.: *A Village History: Woodham Walter* (Plume Press, 1989)



Response	Resident of Woodham Walter	Objection to principle of conservation area	Objection to boundar of conservation area	Objection to Article 4	Proposed changes to the Character Statement	Summary of Comments
001	Y	N	N	N	N	Important to preserve the unique characteristics of the village. Proabably agree with introduction of Article 4 Directions but unsure of architectural significance of these properties.
002	Y	N	N	N	N	To preserve the character of the village.
003	Y (John Tompkins)	N	Y	N	N	Broadly agree with the boundaries of the Conservation Area but consider that the property known as 'Whitegates' should be included. First class character assessment which accords with the views of the VDS team. Dissapointing that the only remaining 'Arts and Crafts' building in the village has been approved for redevelopment but the inclusion of the site would give greater control over future developments to safeguard the historical setting of the heritage asset Grade II* St Michael's Church and the significant views over Bell Meadow and from other parts of the central village area.
004	Y	N	N	N	N	Lived in Woodham Walter for many years and would like to keep it as it is.
005	Y	N	Y	N	N	It is a lovely place to live and should remain that way so the Conservation Area is supported, however it is felt that the field at the rear of Rectory Road should be included.
006	Y (Ken Rennie, Watendlath, Rectory Road)	N	Y	N	N	Woodham Walter is an area of historic and architectural interest with exceptional character and appearance involving buildings differing in style across the ages. It is also an important agricultural asset. Object to the boundary of the Conservation Area as it is felt that the surrounding farmland, woodland and common land should be included as they define absolute boundary envelope proposed. The character statement states that these areas are not of historical interest, however these encompass centuries of such interest, to mention one: Woodham Walter Hall and Estates. How can conservation be managed without the inclusion of these areas? It is not unusual to have areas, even towns in total defined as conservation areas where applicable.
007	N (Sally Bull, Barnfield, Spring Elms Lane, Little Baddow)	N	N	N	N	Village has special character and appearance which is unique and should be preserved. Support the application of the Conservation Area.
008	Y	N	N	N	N	This is an attractive and old village centre which without protection could be damaged or lost.
009	Y (8 Rectory Road, Woodham Walter)	N	N	N	N	No comments
010	Y (Frank Herrmann, West Bowers Hall, Woodham Walter)	N	N	N	Y	Lovely village that needs support for its future. Agree with the Article 4 Direction as long as planning applications are dealt with without delay. Correction to the character statement - Fort Cottage extension built in the <u>LAST</u> century.
011	Y (Eric Whitfield, Tudor Rose, Rectory Road, Woodham Walter)	N	N	N	N	The overall appearance of the area, reflecting its long history, rather than individual buildings that justifies the designation. Woodham Walter fully meets the statutory definition for a conservation area. Planning legislation makes it clear that local authorities should give special attention to such areas and their enhancement.
012	Y	N	Y	N	N	More should be done to preserve our villages (on the whole) to prevent developers from ruining any surrounding countryside/farmland, then clearing off. Too often objections and concerns of existing residents are ignored and developments are forced upon them as if they have no voice. All of the surrounding countryside and farmland should be afforded the same conservation protection. The proposal is excellent as it is a step in the right direction.
013	Y (D. Weedon, Redbriek Cottage, The Street, Woodham Walter)	N	N	N	N	To help retain some of the character of the village
014	Y (Mrs P Herrmann, West Bowers Hall, Woodham Walter)	N	N	N	N	In favour of both the the conservation area notion and the imposition of Article 4 direction on some buildings, but only if the Council tackles two serious aspects arising from the proposals. 1. It would be heavy handed and counter-productive to insist that original materials and methods should always be used for repair. Please can conservation officers research and find suitable methods and materials that are modern and not excessively expensive, that can be recommended or even insisted on when allowing planned work on properties of all ages. Most owners would welcome help in such a realistic and affordable way. It would help to promote the solution so rightly praised about Wingtons: " the way in which it has been adapted organically over the centuries in a vernacular fashion". 2. It is considered vital for the Council to tackle and overcome the serious problem of the length of time taken to consider and decide any application under an Article 4 direction. Having had bitter experiences of inordinante and appalling length of time taken by the planning application section (not the conservation officers who were speedy and helpful) and it led me to wonder whether the Council deliberately puts free-of-charge applications at the bottom of their list and sees no harm in taking months to issue decisions. Delays causes difficulties to the applicant such as having to pay more for some works.
015	Y	N	Y	N	N	The proposal and statement is very well prepared and interesting. Strongly support the notion as the whole character of the village could be changed and spoiled by inappropriate and insensitive alterations to buildings. Boundary is good but the women's club room marked as village hall in top road should be included. It is the property of the Woodham Walter Women's Club founded in 1906, pre-dating the national womens institute by around 10 years and is still running as a unique club. The building had been a 'summer house' 'owned' by Henry Ayrton Chaplinm son of Edith Chaplin, one of the co-founders of the club. He died in 1905 whilst working with the West African medical staff. The land in Top Road was given to the club and I think that the building was erected on that site in the 1920's. Agree with the introduction of Article 4 Directions as it is too easy for people to make alterations that are not in keeping or of inappropriate materials. Therefore I think control is quite justified. It is felt the Women's club room , Top Road should be including in the statement.
016	Y (Daniel Mountier, 2 Gun Hill, Farm Cottages, Woodham Walter)	Y	Y	N	N	Although I agree with conserving the unique buildings in Woodham Walter, it is felt that the conservation area may have a detrimental effect on future development in the village. It is important to Woodham Walter to have a diverse community which from recent national surveys requires building of mixed and affordable housing to allow the school and other village services to remain.

Response	Resident of Woodham Walter	Objection to principle of conservation area	Objection to boundar of conservation area	Objection to Article 4	Proposed changes to the Character Statement	Summary of Comments
017	N (Edward Gittins of Edward Gittins and Associates, Unit 5 Patches Yard, Cavendish Lane, Giemsford, Suffolk, CO10 7PZ	N	N	N	N	Support Conservation Area in principle not only for its ability to protect the character of the village but also to encourage and embrace new development which will enhance the villages environment. There is no obvious village centre at present and therefore proposals have been submitted through the means of the Local Plan process to deliver a central Village Green in association with village housing on the north side of Top Road. A Village Green directly opposite the Queen Victoria Public House fringed by new village homes would create a central amenity space that could be utilised by the Parish and School for village events. A Village Green would enhance the setting of the proposed Conservation Area and would not harm the setting or conflict with the Conservation Area objectives. Therefore see the proposal as a force for positive change rather than solely for preservation, especially as the introduction of some additional growth will help to sustain and improve existing community facilities and the retention of the village Primary School in particular. We also consider a smaller housing development abutting the proposed Conservation Area, immediately south of the Primary School and adjacent to the Brook can also contribute to village facilities as set out in related representations on the Local Plan.

MALDON DISTRICT COUNCIL PUBLIC NOTICE
 Notice of Conservation Area Designation – Woodham Walter, by Maldon District
 Council
 PLANNING (LISTED BUILDINGS AND CONSERVATION AREAS) ACT 1990,
 SECTION 69 and 70

WOODHAM WALTER CONSERVATION AREA

NOTICE IS GIVEN under the provisions of the 1990 Act that Maldon District Council's Planning and Licensing Committee approved the designation of a new conservation area in the village of Woodham Walter on the 2nd of March 2017. This follows a period of public consultation in August, September and October 2016. The conservation area is linear in form, running from Church Cottage and the parish church on Church Hill in the south down to 1&2 The Maples, Rectory Road, in the north.

A boundary map of the conservation area and a document titled *Woodham Walter Conservation Area Character Statement* which describes all of the properties affected and the implications of the designation are available via Maldon District Council's website. Hard copies can be viewed at the District Council Offices, Princes Road, Maldon on weekdays from 8.30am until 5.00pm (4.30pm Fridays).

The principal effects of being included within a Conservation Area are as follows:

1. The Council is under a duty to prepare proposals to ensure the preservation or enhancement of the area.
2. Consent must be obtained from the Council for the demolition of any building in the area.
3. Special publicity must be given to planning applications for development in the area.
4. In carrying out any functions under the Planning Acts (and, in particular, in determining applications for planning permission and listed building consent), the Council and Secretary of State are required to take into account the desirability of preserving or enhancing the character or appearance of the area.
5. Six weeks notice must be given to the Council before works are carried out to any trees.

.....
Simon Rowberry
Interim Head of Planning Services
Council Offices, Princes Road, Maldon, Essex, CM9 5DL
 Date: March 2017

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MALDON DISTRICT COUNCIL

Town and Country Planning Act 1990

Town and Country Planning (General Permitted Development) Order 2015, as amended

**WOODHAM WALTER CONSERVATION AREA
DIRECTION UNDER ARTICLE 4(1) TO WHICH ARTICLE 6 APPLIES**

Maldon District Council, being the appropriate local planning authority within the meaning of article 4(5) of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) are satisfied that it is expedient that development of the descriptions set out in the Schedule below should not be carried out on dwellinghouses within the areas of land outlined in green on the attached plan, unless planning permission is granted on an application made under Part III of the Town and Country Planning Act 1990, as amended.

NOW THEREFORE the said Council in pursuance of the power conferred on them by article 4(1) of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) hereby direct that the permission granted by article 3 of the said Order shall not apply to development on the said land of the descriptions set out in the Schedule below.

SCHEDULE*Class A of Part 1 of Schedule 2 –*

- *The alteration of the front elevation of a dwellinghouse affecting windows, doors or other openings including the formation of new openings;*

Class A of Part 1 of Schedule 2 –

- *The application paint to unpainted brick or stonework of any part of a dwellinghouse fronting the highway.*

Class C of Part 1 of Schedule 2 -

- *Works involving the roof that would consist of or include an alteration to any part of the front roof slope, including the insertion of dormer or other windows in the roofs and the change of roof material or the installation of a microgeneration unit;*

Class G of Part 1 of Schedule 2 -

- *The alteration or removal of a chimney on a dwellinghouse.*

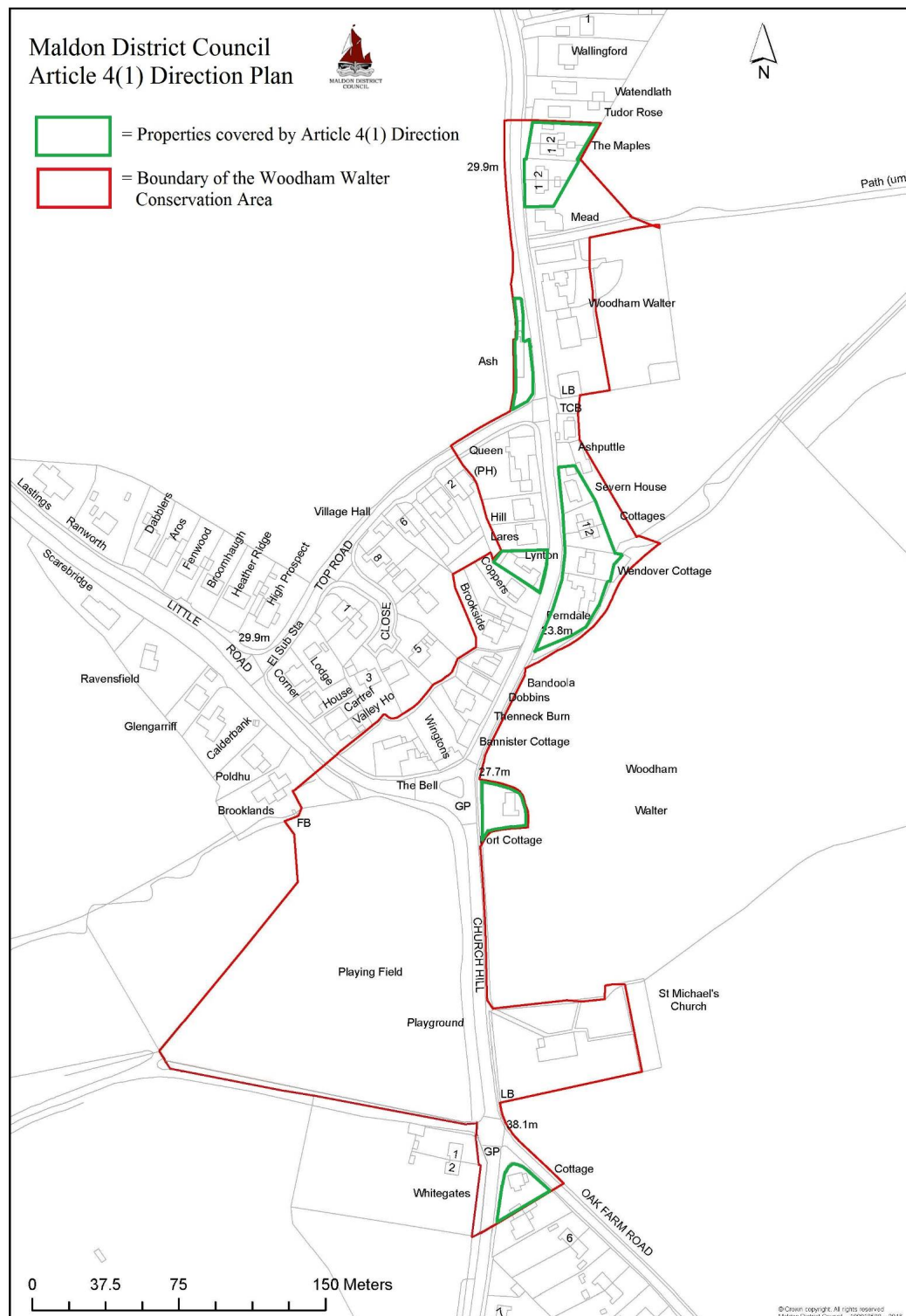
The effect of the Direction is that the permission granted by article 3 of the General Permitted Development Order shall not apply to such development and such development shall not be carried out within that area of land unless planning permission is granted by the Council. A copy of the Direction and a map defining the area of land to which it relates may be seen at the offices of the Maldon District Council at Princes Road, Maldon during normal office hours. The Direction shall come into force in respect of any part of the land on the date on which notice of the making of the Direction is served on the occupier of that part of the land or, if there is no occupier, the owner. Written representations will be accepted within 21 days following publication of this notice.

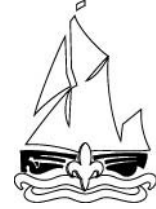
THIS DIRECTION is made under article 4(1) of the said Order and, in accordance with article 6(7), shall remain in force until (*insert date*) (being six months from the date of this direction) and shall then expire unless it has been confirmed by the

appropriate local planning authority in accordance with paragraphs (9) and (10) of article 5 before the end of the six month period.

Simon Rowberry
Interim Head of Planning Services
Council Offices, Princes Road, Maldon, Essex CM9 5DL

Date:





REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

WOODHAM WALTER VILLAGE DESIGN STATEMENT

1. PURPOSE OF THE REPORT

- 1.1 To seek Members' approval to endorse a Village Design Statement (VDS) for Woodham Walter (**APPENDIX 1**) to be used as a material consideration on planning decisions.

2. RECOMMENDATIONS

- (i) that the Interim Head of Planning Services be authorised to agree modifications to the Woodham Walter Village Design Statement (VDS) (**APPENDIX 1**); in consultation with the Chairman of the Planning and Licensing Committee and the Leader of the Council,
- (ii) that subject to (i) above the Woodham Walter Village Design Statement be endorsed as a material consideration on planning decisions.

3. SUMMARY OF KEY ISSUES

3.1 Village Design Statements and Maldon District Council

- 3.1.1 Village Design Statements are tools that local communities can use to influence the design of new development locally by identifying, describing and analysing local character. They provide clear information about the character and design of a particular village against which planning applications may be assessed. Unlike a Development Plan Document or a Neighbourhood Plan, they do not decide whether development should take place, rather their purpose is to show how development should respect local identity, design and the integrity of a particular village or area.
- 3.1.2 A VDS can be endorsed by the Local Authority; if this is the case they can be used in conjunction with the Replacement Local Plan, emerging Local Development Plan (LDP), the Essex Design Guide, the Neighbourhood Plan (if applicable) and other documents to assess new development. A VDS is also a useful tool for parishes to use when responding to statutory consultations and when making representations on planning applications.
- 3.1.3 Whilst the National Planning Policy Framework (NPPF) or National Planning Policy Guidance (NPPG) does not specifically refer to VDSs, their use will still support good

design, which is part of the NPPF (Chapter 7). Endorsing the VDS as informal guidance will allow it to be used as a material consideration in development control decisions.

- 3.1.4 The other Village Design Statements endorsed by the Council are, 2010 Wickham Bishops VDS (Minute No. 103 refers), the 2007 Heybridge Basin VDS (Minute No. 106 refers) and the 2015 Althorne VDS (Minute No. 254 refers). Endorsement enables these documents to be used as a material consideration in the determination of planning applications.

3.2 **Woodham Walter Village Design Statement Summary**

- 3.2.1 The Woodham Walter VDS (**APPENDIX 1**) has been developed by the Woodham Walter Village Design Statement Working Party and in consultation with the local community and the Local Planning Authority. The objectives of the document are listed below:

- Provide a record of local uniqueness by describing the qualities and character of the village
- To identify the principle aspects of the natural and built environment to be respected and protected from the impact of inappropriate development.
- To provide design guidance to householders, designers and developers so that change is managed and development is in harmony with its rural setting.
- To achieve a higher standard of design to enhance the local environment.
- To increase the involvement and influence of the local community in the planning process.
- To work with neighbouring authorities, partners and stakeholders to ensure cross boundary co-ordination and future planning.

- 3.2.2 These objectives are supported by an analysis of the village's evolution and the built environment. Within these chapters there is detailed guidance on features such as environment and landscape, building styles and heritage assets, threats to the village and the accompanying conservation area, materials and also design treatment of aspects such as roofs, garages and street furniture. The document likewise analyses different areas of the settlement to provide assessments which will provide a basis for guiding decisions.

- 3.2.3 Each section of the document references back to relevant policies within the emerging Local Development Plan. Blue boxes with yellow lettering highlight the relevant policy from the emerging Local Plan. Text with a yellow background relates to planning policy context, whereas a green background provides general guidance based on feedback from local residents. Any text with a blue background outlines the objectives hoped to be achieved.

3.3 **Maldon District Council Officer Comments**

- 3.3.1 The Local Planning Authority was consulted on draft versions of the VDS in 2015, 2016 and 2017. Comments were made by the Planning Policy, Conservation, Urban Design and Countryside and Coast Officers.

- 3.3.2 The final draft VDS submitted to the Council on the 14 February 2017 (**APPENDIX 1**) addressed Officers concerns and is therefore considered to be in general compliance with the District's Replacement Local Plan and the emerging LDP policies. However, some minor modifications to the document maybe necessary in order to clarify certain aspects of the VDS before the VDS can be formally endorsed by the Council as a material consideration in decision making.

4. CONCLUSION

- 4.1 The Woodham Walter Village Design Statement Working Party has produced a Village Design Statement for Woodham Walter. A request has been received for the VDS to be endorsed by the Council and made available on the Council's website. The VDS is in general compliance with the District's Replacement Local Plan and the emerging LDP policies. However, some minor modifications to the document are necessary, mainly in response to whether or not the Conservation Area is approved; in order to clarify certain aspects of the VDS before the VDS can be formally endorsed by the Council as a material consideration in decision making.

5. IMPACT ON CORPORATE GOALS

- 5.1 The endorsement of the Woodham Walter VDS will support the following corporate goals:
- Strengthening communities to be safe, active and healthy;
 - Protecting and shaping the District.

6. IMPLICATIONS

- (i) **Impact on Customers** – Endorsement of the Woodham Walter VDS will help to provide material planning guidance for the village of Woodham Walter.
- (ii) **Impact on Equalities** – None identified.
- (iii) **Impact on Risk** – The content of the Woodham Walter VDS has been reviewed to ensure that there are no significant contradictions with local planning policies for the District. There is likely to be an increased demand for Officers to work with the Parish and Town Councils in the District in the future to implement community-led plans and neighbourhood planning.
- (iv) **Impact on Resources (financial)** – Maldon District Council Officers have provided assistance and guidance towards the development of the Woodham Walter VDS. There is likely to be an increased demand for Officers to work with the Parish and Town Councils in the District in the future to implement community- led plans and neighbourhood planning.
- (v) **Impact on Resources (human)** – None identified.

- (vi) **Impact on the Environment** – The endorsement of the Woodham Walter VDS will support the future environmental sustainability within Woodham Walter, and will help retain the local character of the village.

Background Papers: None.

Enquiries to: Devan Lawson, Planning Officer, (Tel: 01621 876202).



The Woodham Walter

Village Design Statement 2016

Contents and Foreword

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Foreword

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Foreword

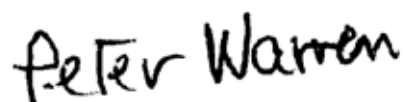
Following the introduction of neighbourhood planning by Central Government, in the spring of 2013, the Woodham Walter Parish Council set up a Working Party to prepare a Village Design Statement document as part of the community led planning initiative.

During 2014 every household in the Parish was circulated with a Questionnaire designed to gauge public views on how they wished to see their village environment developing in the future. The response to this Questionnaire was outstanding, in excess of 75% returns and the results were published at Bell Meadow Day 2014 as well as being incorporated on the Parish Council Website.

Those results have now been collated into this document, the first draft of which was exhibited at the 2015 Annual Parish Meeting and has now been submitted to the Maldon District Council for approval as a supplementary document to the Maldon District Characterisation Document that in turn will relate to the Council Local Development Plan and therefore is pertinent to the planning process.

This Village Design Statement is Woodham Walter specific and gives a detailed description of the existing character and main features of design in the village and guidelines for how this should be respected in any new development. It is important to bear in mind that the Village Design Statement applies to the whole Parish and not just the central core of the defined settlement. The guidance is evidence based, relating to the Questionnaire responses and therefore embodies villagers' views. It provides positive guidance to householders, designers and developers on design acceptable to the community and will help to protect important or historic buildings and open spaces. This will in turn assist the Parish Council and the District Council in determining planning applications.

The Parish Council would like to thank the Village for its high level of response and the VDS Working Party for the extensive work that they have put into this project culminating in this document.



Peter Warren
Chairman of the Parish Council.

Introduction 1

The Woodham Walter Village Design Statement

The Woodham Walter Village Design Statement endeavours to give a detailed description of the existing character and main features of design in the Village together with guidelines for how these should be respected in any new development. The aim is to avoid inappropriate development throughout the entire Parish, not just the central core. The Village Design Statement is wholly compiled from, and embodies villagers' views gained from a comprehensive questionnaire circulated to every household during 2014 for which the very significant return was in excess of 75%. It has also been the subject of extensive public consultation.

Villages have evolved over centuries and occupy a unique position within the surrounding countryside, altering and adapting to suit the needs and circumstances of the inhabitants. In consequence, we are inevitably drawn to the elements that make our own village different from others, and those aspects that make it unique.

Page 35 Since the Second World War, standardisation in construction has generally failed to reflect both the subtle and sometimes obvious elements that create local verve and character. In association with this, governmental interference, personal tastes and cultural developments have all played their part in the design of buildings often to the detriment of the environment.

The Village Design Statement is not intended to be prescriptive by invoking standardisation, but rather be a tool to promote inspiration to design modern development in the form of alterations, extensions and new build, however large or small, that respects and is in sympathy with its surroundings. It covers minor works such as replacement windows, doors and external lighting as well as the more major projects such as extensions and new buildings. Whilst planning permission may not be required for some projects, especially where permitted development rights pertain, it is hoped that residents will adopt the design standards that would apply to other projects within the Village.

In Woodham Walter, by the high return of questionnaires, involvement of the community at large and their comments, the residents have demonstrated that they recognise that local individuality is vital in helping to integrate new development and in creating a sustainable community. This can be achieved through an understanding of local character and ethos and by ensuring that this understanding is shared with anyone considering development.

Objectives

Purpose

- 1.01 *Provide a record of local uniqueness by describing the qualities and character of the Village.*
- 1.03 *To identify the principle aspects of the natural and built environment to be respected and protected from the impact of inappropriate development.*
- 1.04 *To provide design guidance to householders, designers and developers so that change is managed and development is in harmony with its rural setting.*
- 1.05 *To achieve a higher standard of design to enhance the local environment.*
- 1.06 *To increase the involvement and influence of the local community in the planning process.*
- 1.07 *To work with neighbouring authorities, partners and stakeholders to ensure cross boundary co-ordination and future planning.*

- 1.08 *Throughout this document the blue boxes with yellow lettering **S1** highlight the pertinent Maldon District Council LDP policies; text with a yellow background specifically relates to planning policy; text with a green background is intended as general guidance to Residents based on Questionnaire returns; text with a blue background indicates objectives.*

2 Evolution - Geology, Topography and Landscape.

Geology, Topography and Boundaries.

The village sits on sand and gravel glacial outwash over London Clay, producing an undulating topography of hills and valleys rising gently southwards, away from the River Chelmer which forms the majority of its northern boundary and gently westwards, as height builds towards the Danbury Ridge. The Little Baddow – Danbury Ridge forms the western Parish boundary and the watershed between the Woodham Walter and Woodham Mortimer brooks the southern. The eastern boundary is not marked by any physical feature but is more than likely the edges of strips of common fields.

Landscape Character.

The main village settlement is located in the centre of the Parish and sits in the basin of a stream that rises in Woodham Walter Common. The landscape around it is characterised by the ancient woodlands of Woodham Walter Common (SSSI) and The Wilderness and several smaller woods, a leftover from the densely wooded topography that once dominated the area and probably gave rise to the name Woodham Walter or 'Wudeham', a settlement in the wood. There are rolling arable fields enclosed by native species hedgerows dotted with oak and ash, with open skies and long views and to the north the more intimate scenery of the river meadows. Roads and lanes are winding, rising and falling gently with the landscape and are likewise enclosed. A network of footpaths criss-cross the woodlands and arable fields.

Views.

Woodham Walter is a village with many fine views. From various locations there are views northwards across the Chelmer flood plain to Boreham, Ulting, Hatfield Peverel, Langford, Wickham Bishops and Great Totham and from Gunhill in particular, eastwards over village centre rooftops to The Wilderness and beyond. From Old London Road there is a distant view over Witham to Silver End and beyond and from St Michaels there is magnificent view of The Bell and The Street nestling in the basin of the stream which rises in Woodham Walter Common. Trees and tall hedgerows dominate almost every view to the extent that buildings are frequently wholly or partly hidden. The opportunities for a fine view are endless!



From Church Hill Towards Little Baddow Road



From Church Hill Towards the Wilderness



Footpath 13 Towards the Village

Evolution - Settlement Growth 2

Settlement Growth.

Like many other villages the development of Woodham Walter appears to have followed the track layout with settlements appearing at junctions and other principle points. The principle routes were east-west following the river and making their way towards Maldon. Where these tracks were bisected by a north-south route, settlements sprang up. Factor in a good water supply and employment and the settlement soon grows. The settlement growth can be traced by various maps.

The sale of the Woodham Walter estate by 1745 meant that the area became divided into smaller farms giving rise to an urgent need for accommodation. It is during this period that there was an increase in building not only in the Village centre but also in the outlying settlements.

The Chapman and Andre Map of 1777 indicates that a village had become established in the valley of the stream rising in Woodham Walter Common, with a cluster of buildings from The Bell to the stream fronting what is now The Street, the historic route to the River Chelmer. Scattered around the Village core are a number of small settlements (Spring Elms, Little London, West Bowers, Curlingtye Green) and a number of dispersed farmsteads.

By 1875 a proper Village centre had developed with a Post Office, National School (1873), baker, butcher, beer shop (now the Queen Victoria PH), smithy and wheelwright and a Congregational Chapel (1881).

Growth slowed towards the end of the century, but in the early C20th the Women's Club (1906), almshouses (1908) and Reading Rooms were added. Two Arts and Crafts houses were built - The Warren House and Whitegates. Others in a similar style soon followed - Crossways and two lodge houses at The Warren. In 1926 tenants moved into the first Council houses in the Village (Church Corner).

Additional Council Houses were built after WWII in Rectory Road and later some bungalows in Top Road. Mead Pastures was built, followed by Spar Hawks, Brook Close and three large houses in the grounds of Mead House, the former Rectory (1814). Most recently a number of houses away from the Village centre have been extended or completely re-built (Jasmine House, Grove Manor, 1 West Bowers Bungalows now Longwood).

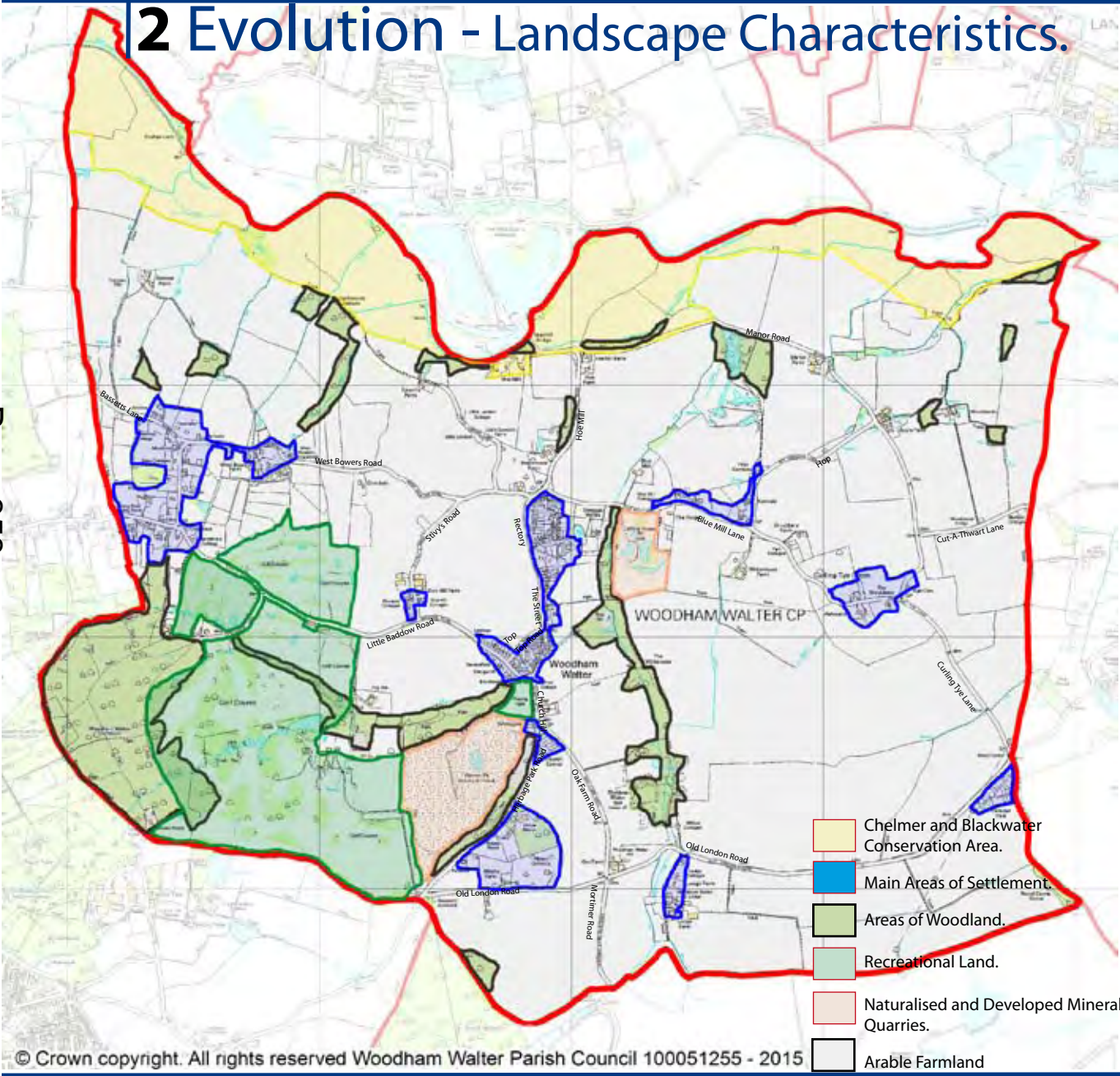
In the 1990's a 20 pitch Traveller Site was constructed and in the early part of C21st work commenced on building holiday lodges and a small leisure centre at The Warren.

At the last census (2011) the Village had a population of 532 people living in 256 dwellings. Of these, 109 dwellings are located in Village centre as defined overleaf and the remainder are dispersed throughout the Parish, either singly or in groups of up to 30 dwellings, each with their special character.



2 Evolution - Landscape Characteristics.

Page 358



Natural England has divided the country into National Character Areas for each of which there is a profile. These profiles tend to follow natural lines and are areas that share similar landscapes.

Woodham Walter lies within the Northern Thames Basin, which is rich in geo-diversity, archaeology and history stretching north-eastwards to Suffolk, westwards to Hertfordshire and south-wards to the River Thames. It includes the Essex wooded hills and ridges amongst which is the Danbury Ridge, encompassing Woodham Walter Common, and the gentle drop along the length of the River Chelmer that flows through the north of the Parish. There are several main areas of settlement and the remaining land is either historic woodland, recreational use or farmland interspersed with isolated settlement and small groups of dwellings.

The Parish has an agricultural tradition offering a varied land use pattern that includes arable and pasture land. There has been a significant decline in apple and pear orchards together with livestock in recent years leaving arable farming to become predominant. Since WWII the historic fields have become significantly enlarged to accommodate modern farming methods.

The canalised River Chelmer runs west to east along the northern boundary towards Maldon and the North Sea in a flat valley with large slopes rising from the arable grassland to the wooded ridges to the south and the more central settlements of the Village. The River valley is a conservation area throughout its length and there are long views across the valley from the higher ground. The diverse range of landscape provides a natural habitat for a wide range of species including deer, newts, water voles and dormice.

The influence of the growing conurbations of Chelmsford and Maldon has given rise to extreme pressure on land for housing and on the road infrastructure changes to which would in the main result in the loss of important arable land.

Evolution - Defined Areas of Settlement 2

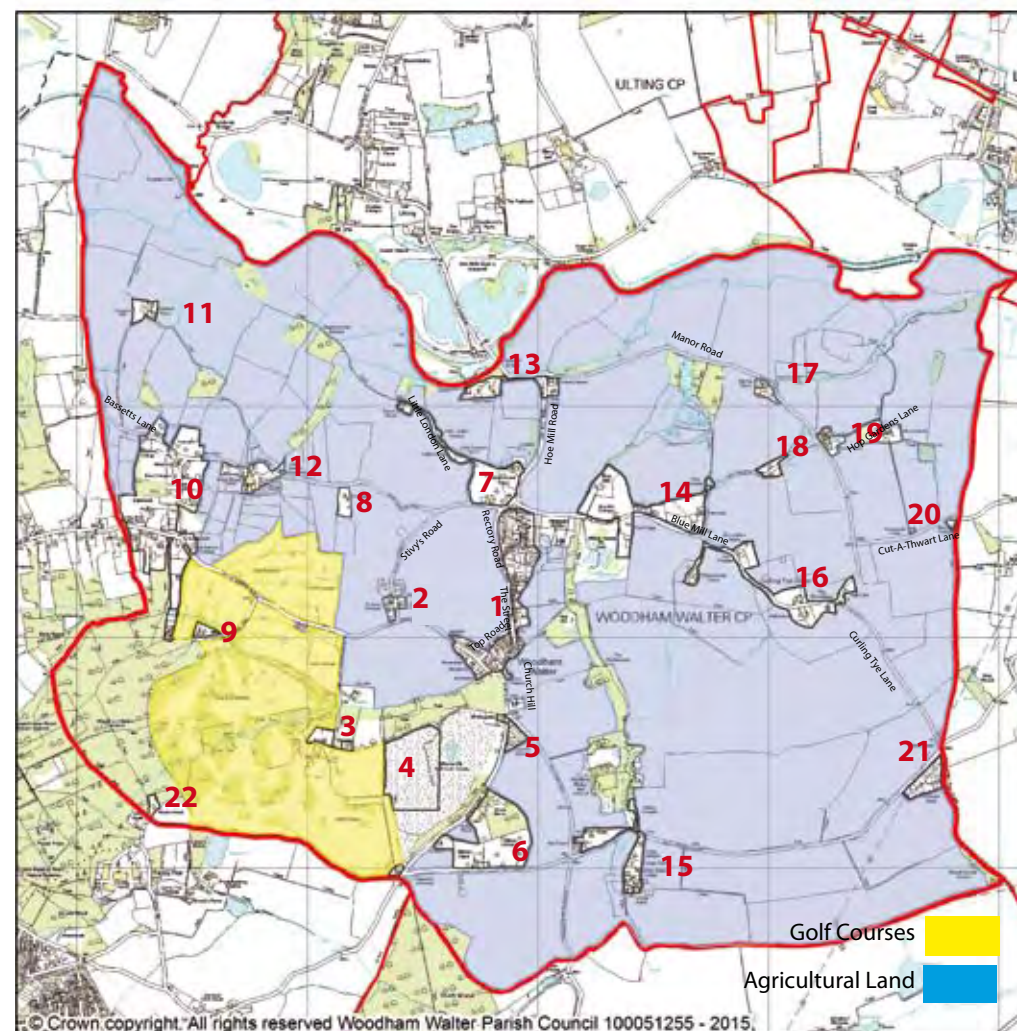
Areas of Defined Settlement

Woodham Walter as a village is referred to in the Braintree, Brentwood, Chelmsford, Maldon and Uttlesford Characterisation Assessment document as being an Arcadian settlement. It is a typical English Village with outlying groups of settlement; it is distinguished from a Hamlet by the fact that it has its own church and a centre. Being a dispersed rural settlement with a central core, it is categorised as 'Rural in a sparse setting' in DEFRA's Rural-Urban Classification for Output Areas in England. This is amply illustrated in the adjacent map showing the distribution of defined settlement areas set in amongst extended agricultural land use within the parish boundary. In the main this settlement is based around the junctions of roads or isolated such as Retreat Farm and Robin's Wood.

Whilst Woodham Walter does not have the dramatic scenery of mountains and rocky coasts nor the expansive parkland and dominant landscape inferred by 'Arcadia', the area epitomises all that is best in the Eastern Region of England. The scenery varies from open farmland to historic woods and includes a canalised river and two well laid out golf courses. Long views across open farmland interspersed by small wooded areas are characteristic of the area and can be experienced from all parts of the Village. The open farmland is criss-crossed by a significant number of footpaths shown on page 42 and two protected lanes shown on page 20.

The extent of the two golf courses and the old Warren Pit area can be readily determined on the map and both areas were originally farmland with the Warren being laid out as a golf course following the purchase of the house by Henry Thompson in 1904

Most of the areas of defined settlement contain listed Buildings together with a number of buildings that are of historic or architectural interest although not Listed. In the central area these have been incorporated into the new Conservation Area and are shown on page 22.



Defined Areas of Settlement

1	Central Village Area	7	Little London	13	Hoe Mill	19	Guys' Farm
2	Gun Hill	8	Glendale	14	Blue Mill	20	Cut-A-Thwart Lane
3	Warren House	9	Common Lane	15	Lodge Farm	21	Wood Corner
4	Holiday Lodges	10	Spring Elms	16	Curling Tye Lane	22	Robin's Wood
5	Church Corner	11	Retreat Farm	17	Manor Farm		
6	Grove Manor	12	West Bowers	18	Hop Garden Lane		

2 Evolution - Area Settlement Characteristics.



The Bell and Wingtons from Bell Meadow



Top Road



Rectory Road towards the Village Centre

Central Village Area (1)

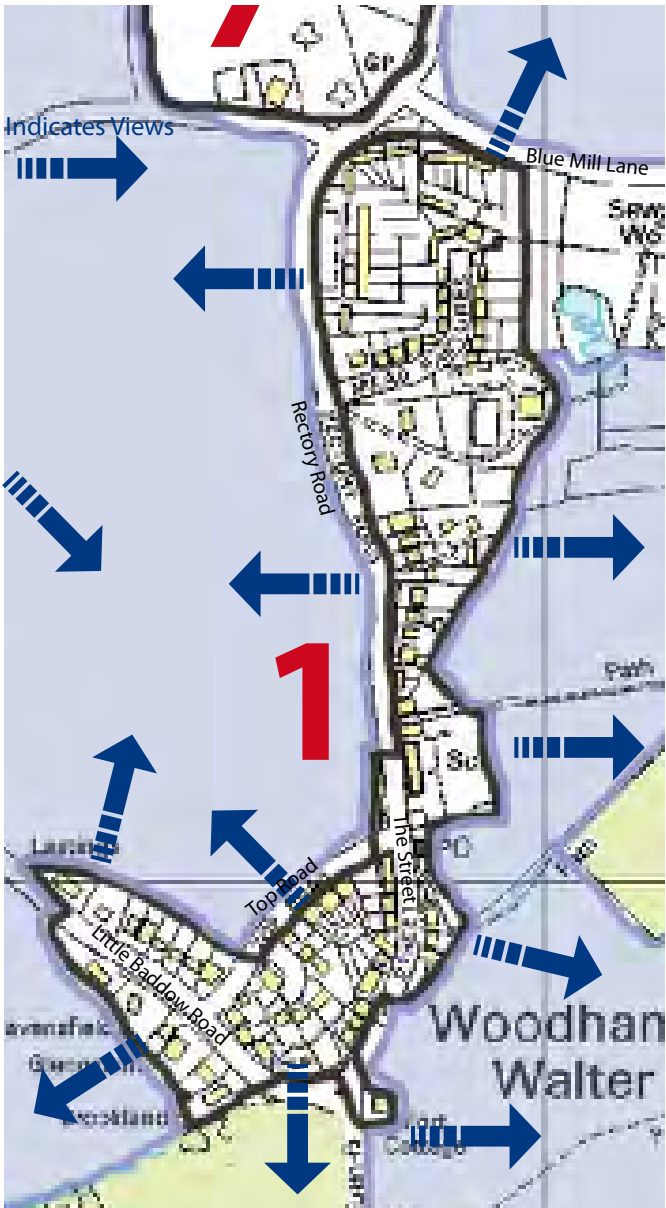
From St Michael's, Church Hill drops into the basin of a stream nestling in the historic centre of the village. 'Wingtons' and the gable end of Bannister Cottage (both late C18th) sit behind a triangular greensward and Little Baddow Road branches south westwards, past The Bell (early C16th), crosses the stream and rises out of the village between single and 2-storey generally rendered detached houses with concrete tiled roofs set back from the road behind greenswards, grassy banks, indigenous hedges, trees and gardens.

Church Hill becomes The Street at Bannister Cottage, a row of houses initially on one side with an arable field opposite. It crosses the stream at a slight bend, now with housing on both sides as it rises to culminate at the Congregational Chapel (1881), and opposite, The Queen Victoria public house (c1820). Houses are detached C19th and C20th, brick or rendered, some with gable ends fronting the road.

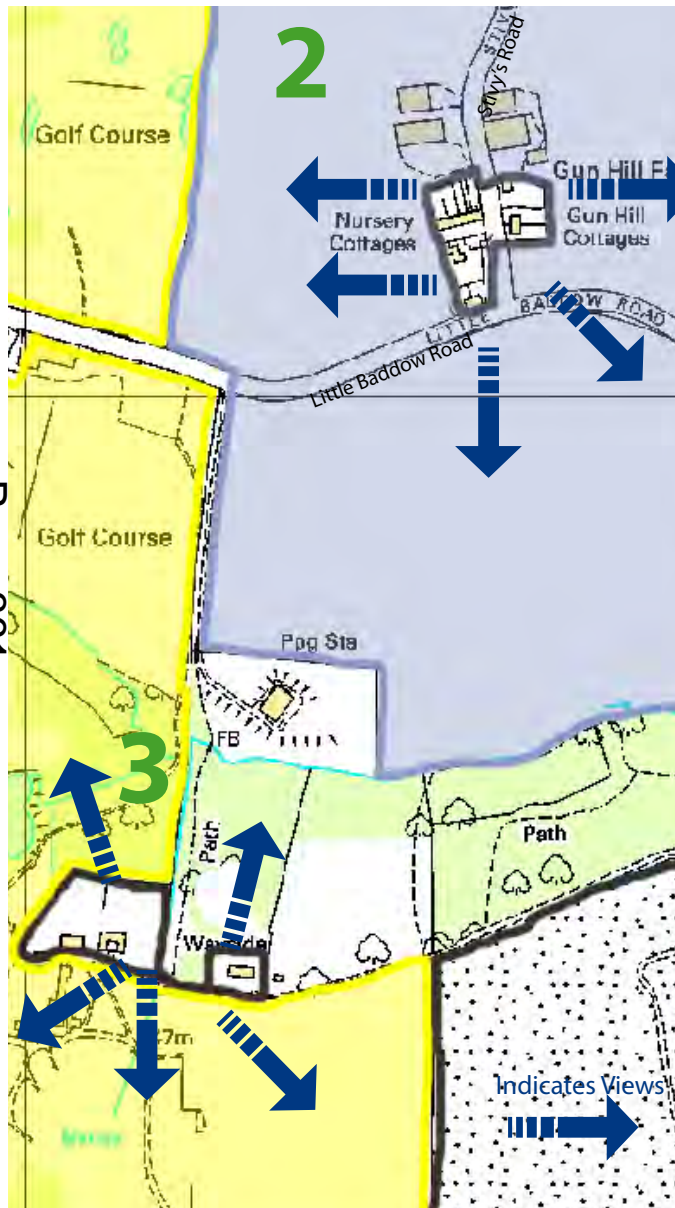
At the pub Top Road branches south westwards, forming a triangle with The Street and Little Baddow Road. Generally C20th single and 2-storey housing and the Women's Club (1906) sit inside the triangle, with a grass bank and rising arable land opposite. There is a fine view of this part of the village from St Michaels, with rooftops and gable ends rising and falling with the change in elevation either side of the stream.

Beyond the pub The Street becomes Rectory Road with the School (1873), Almshouses (c1908), Village Hall (1928) and mostly C20th houses on it's east side behind a narrow pavement, hedges, overhanging trees and small front gardens.

There is a mix of materials throughout the Village Centre. Walls are mostly rendered or brickwork, with some weatherboard, roofs mostly pantiles, interlocking concrete or plain tiles with some slate and windows predominantly casements, with some sash windows.



Evolution - Area Settlement Characteristics. 2

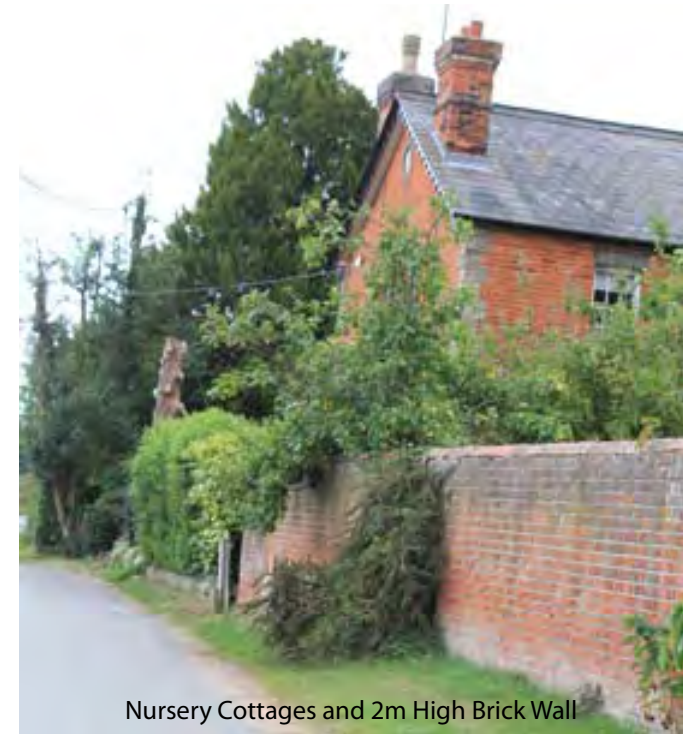


Gunhill (2)

In an elevated arable landscape clustered around the Listed Stivvys Road is a collection of twelve predominantly C19th, 2-storey slate or plain tile roofed red brick or rendered houses with timber sash windows. Set in well-cultivated gardens with native hedges, greenswards and a prominent 2m high brick wall adjacent to a terrace of five houses, originally part of a C19th nursery and hothouse. Mid C20th farm buildings bound the settlement to the north and by Little Baddow Road to the south with open views to the north over the Chelmer Valley, to the east overlooking the village centre and southwards overlooking woodland.

Warren House (3)

At the top of Herbage Road between two early C20th lodge houses is a narrow horse chestnut lined road winding gently downhill through the undulating golf course to the Warren Golf Club. At the bottom, opposite the Grade 2 C18th weatherboard clubhouse, overlooking gardens and fairways are 'The Warren House', a part C17th Arts and Crafts fronted house and an early C20th outbuilding. Nearby, along a tree lined track is 'Wayside', also Arts and Crafts. All three properties are parge plastered with plain tile roofs and timber casement windows and are backed by dense woodland.



Nursery Cottages and 2m High Brick Wall



Stivvys Road looking North

2 Evolution - Area Settlement Characteristics.



Warren Pit



Church Corner, Oak Farm Road to the Left



Church Hill towards the Church

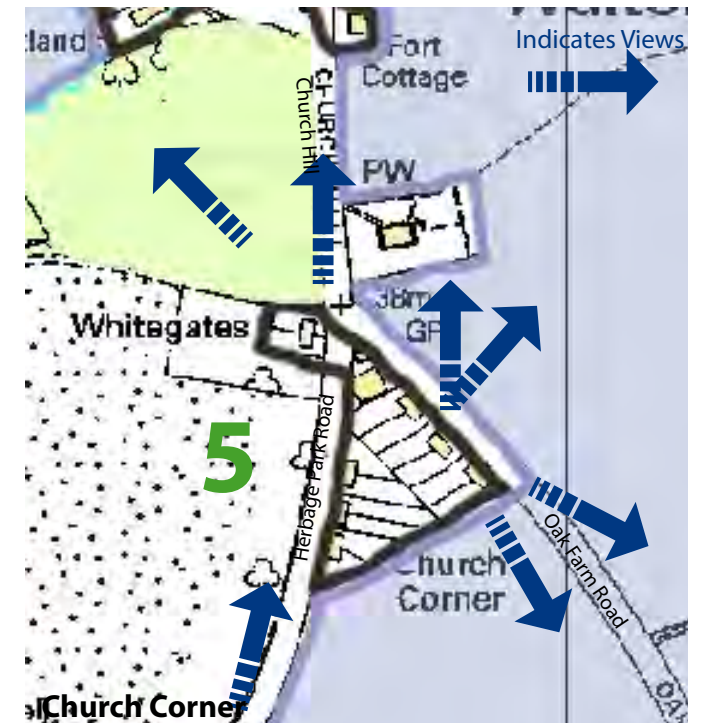
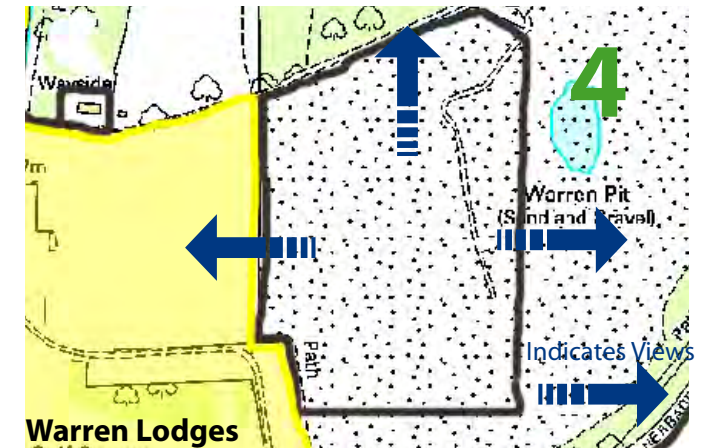
Warren Pit Lodges (4)

Returning half way back up the Golf Club access road is a small made up track running steeply downhill to Warren Pit. Enclosed by high hedgerows dotted with trees, this was previously used for gravel extraction, but is now the site for 36 single storey cedar clad lodges and an associated leisure centre and offices.

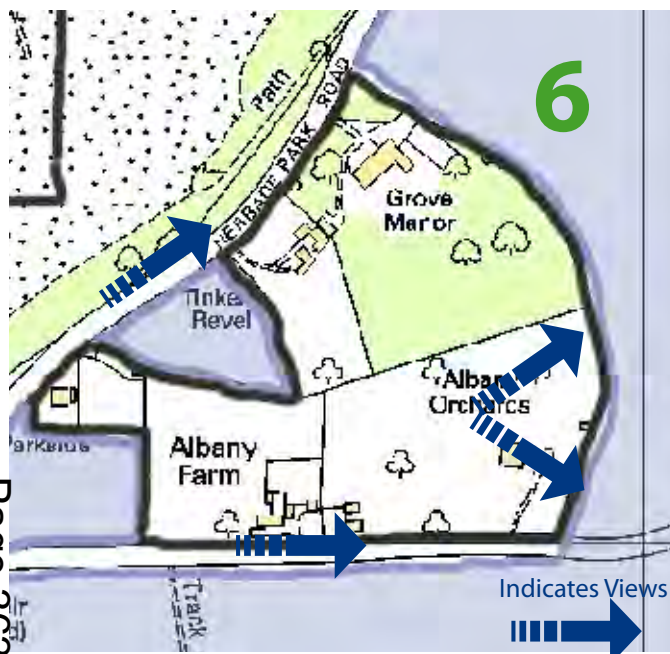
Church Corner (5)

Formed at the point where Oak Farm Road and Herbage Park Road converge, Church Corner sits on rising ground adjacent to the Grade II* Elizabethan St Michaels Church, occupying a triangular site fronted by the rendered C19th Church Cottage. In Oak Farm Road are three pairs of mid C20th slate roofed, rendered semi-detached houses with casement windows which are mirrored in Herbage Park Road, which also has a rendered bungalow of the same period. Facing the gable end of Church Cottage is the plain tile roofed, parget rendered Arts and Crafts Whitegates (originally two cottages).

Indigenous hedgerows form most boundaries and views west, and east down to The Wilderness, are constrained by the density and height of hedgerows and trees.



Evolution - Area Settlement Characteristics. 2



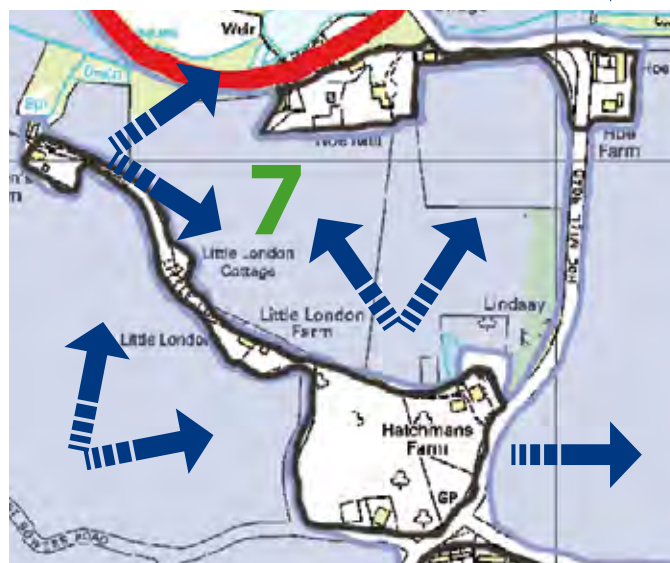
Herbage Park Road; Old London Road (6).

Overhung with large branches, Herbage Park Road climbs steadily away from the village centre and in a small area of woodland are a mock C18th brick manor house set back from the road behind large iron gates and nearby a mid-C20th rendered and tile hung house. Further along is a mid-C20th bungalow and then at its highest point (60m), the junction with Old London Road. This is level at first, past 'Albany Farm' sitting behind a tall hedge opposite arable fields, but then starts to drop, passing a substantial early C20th mock Tudor house brick with a long sweeping lawn down to Oak Farm Road. Plain tiles and casement windows predominate.

Little London (7).

As West Bowers Road rises gently away from the northern tip of the village centre it passes, behind a high hedge, the mid-C20th plain tiled, rendered and casement windowed Orchard House and enters a cutting in which sits the entrance to Little London Lane. This runs north for 100m, with arable land on one side and a tall hedgerow and poplar trees on the other, behind which sits the C19th Hatchmans Farm, slate roofed and rendered, with casement windows.

The Lane swings westwards, with wooded views towards Hoe Mill and open views southwards. It passes a C19th rendered house with a long brick outbuilding and then the early 19th C Little London House and C17th Little London Cottage. All are plastered with plain tile roofs and timber sash windows. The lane and boundaries are enclosed by indigenous hedgerow. Beyond this the Lane becomes an unmade track leading down to the tree lined river, passing the Grade 2 'Ravens', a C 15th/16th plastered hall house with plain tiles and casement windows enclosed by hedges. There are sweeping views down across the arable flood plain.



West Bowers Road



Hoe Mill House from Little London



Little London Cottage from West Bowers Road

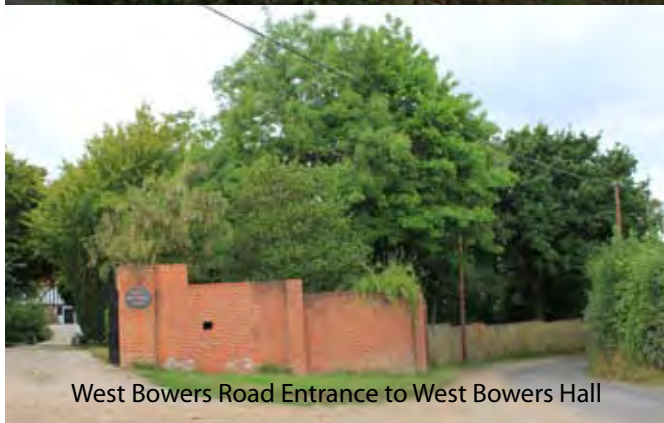
2 Evolution - Area Settlement Characteristics.



Across the Chelmer Valley from West Bowers



Glendale towards Little Baddow Road



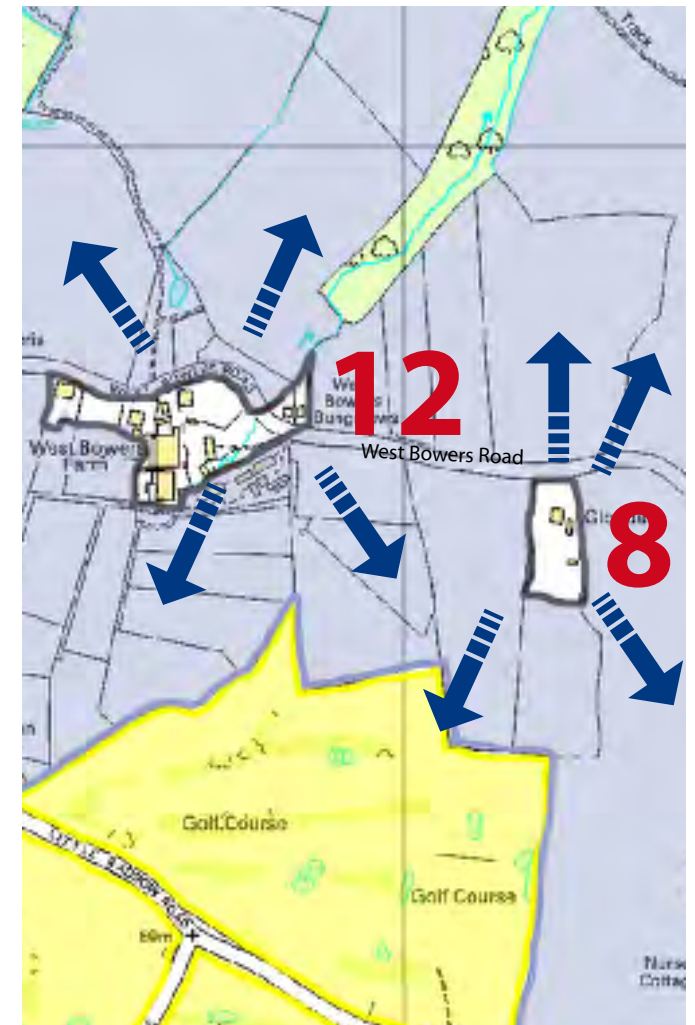
West Bowers Road Entrance to West Bowers Hall

Glendale and West Bowers Road. (8 and 12).

The single track West Bowers Road winds in a generally westerly direction from Rectory Road running parallel to, and some way above the Chelmer flood plain and rises gently to its junction with Bassets Lane.

Behind hedgerows dotted with oak and ash the landscape pattern is generally one of pasture south of the road and enclosed arable fields to the north sweeping down to the river. There are long views across the river valley.

'Glendale', an early C20th red brick slate roofed bungalow with casement windows and a prominent slate mansard roofed outbuilding, sits alone halfway along the road and further along are a cluster of five houses of varying styles. Behind a high dense hedge is a 1920's bungalow with painted brickwork walls next to a new larch clad 2-storey house with plain tile roof and casement windows. Opposite, and set back from the road, behind a long brick wall and just visible through the trees sits the imposing Grade II* West Bowers Hall with its exposed oak frame, white plasterwork, peg tile roofs and leaded casement windows. Next, 'The Barn', a rendered C19th farmhouse only thatched house in the village and then an early C21st red brick, weatherboarded and rendered house with a plain tile roof.



Indicates Views



Evolution - Area Settlement Characteristics. 2

Little Baddow Road, Spring Elms Lane (Part 10), Common Lane (9) and Robins Wood (22) (22)

Little Baddow Road and Spring Elms Lane to Little Baddow. Includes Areas 9 (Common Lane) and 22 (Robins Wood), also part of Area 10.

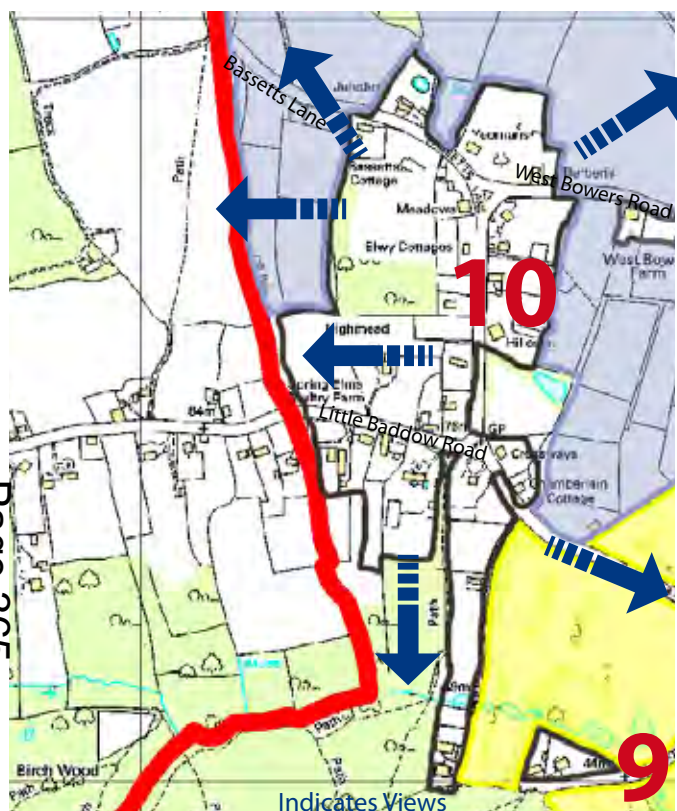
At first Little Baddow Road rises steeply westwards away from the village centre and then more gently with hedgerows and arable fields on both sides. It passes through a section lined by tall overhanging trees reaching Bunsay Downs Golf Club, a rolled concrete and plain tiled single and 2-storey mostly rendered building and beyond, a 2-storey plain tiled, weatherboard house with timber sash windows and a mid-C20th brick semi-detached house. The golf course runs along both sides of the road barely visible through tall hedgerows dotted with trees.

Common Lane dissects the golf course, drops into dense woodland, crosses a stream and then rises with two large detached houses at the end, both plain tiled, rendered, with casement windows, one early C20th, the other late C20th.

At the end of the golf course, a pair of iron gates announces a small lane leading to a large unfinished house surrounded by woodland. South of this, and accessed from a small lane in Twitty Fee, is Wood, a large plain tiled mid-C20th rendered house with casement windows on a sloping site immersed in woodland.

At the top end of Little Baddow Road behind hedgerows there are two houses. 'Chamberlain Cottage, late C19th slate roofed, red brick and weatherboard, and the plain tiled, parget plastered Arts and Crafts 'Crossways. Both have timber casement windows.

On reaching the highest point in the village (76m), with Bassetts Lane to the north, the road runs into Spring Elms Lane on which there are eight predominantly 2-storey rendered houses all C20th with casement windows plain or pantiles, often behind high hedgerows.



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Common Lane Looking South to Historic Woodland



Spring Elms Lane - the Highest Point in the Village

2 Evolution - Area Settlement Characteristics.



Bassetts Lane Triangle



Bassetts Lane Southwards up to Spring Elms



Bassetts Lane towards Bassetts

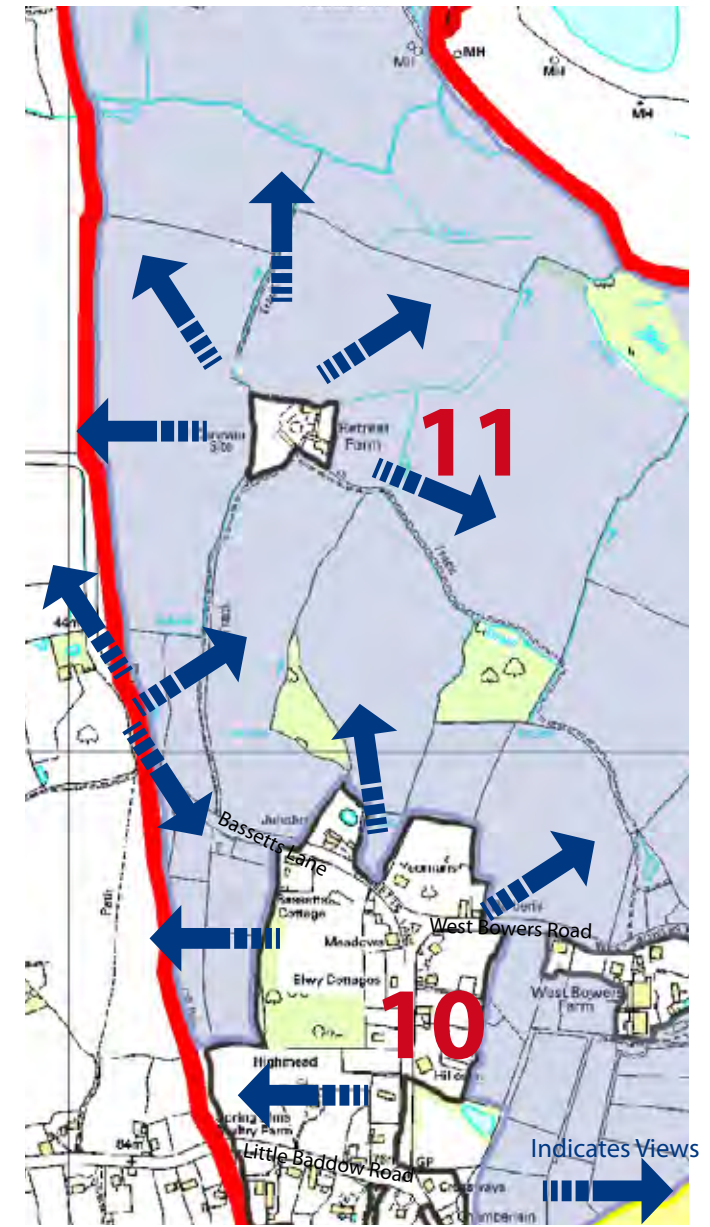
Bassetts Lane (10).

This area covers fourteen 2-storey detached houses in Bassetts Lane and a plain tiled, rendered bungalow and 2-storey bungalow conversion (under construction) at the top end of West Bowers Road which it meets, forming a triangle of land on which sit thirteen trees, mostly large oaks. West of the triangle Bassetts Lane is flat, but to the south it rises steadily to the highest point in the village where it meets Spring Elms Lane. Thick oak branches overhang much of this section.

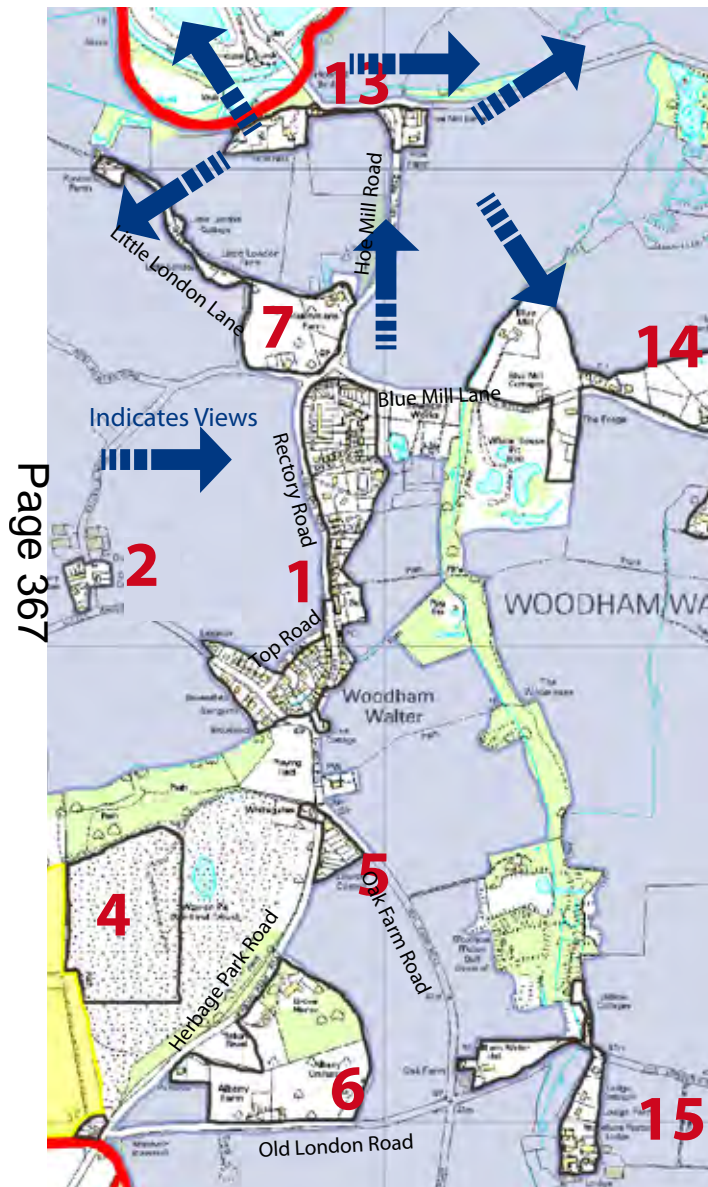
With the exception of the plain tiled, rendered C18th 'Elwy Cottages' (now a single property), and a weatherboard barn conversion, all of the houses in Bassetts Lane are early to mid-C20th, predominantly rendered with slate or plain tile roofs and casement windows. Behind mixed hedgerows, greenswards and ditches there are large mature gardens with large deciduous trees, fruit trees and specimen evergreens. Views are generally constrained by the density of planting, but there are occasional views northwards.

Bassetts and Retreat Farm (11).

The westerly end of Bassetts Lane drops gently towards the Parish boundary and Little Baddow. Hedgerows on both sides of this narrow winding road are dense, but do afford occasional long views northwards across the river valley. A narrow track leads northwards away from the lane and at the end sits the Grade II Retreat Farm, early C15th, with C16th and C19th additions. Further along the lane, 'Bassetts,' Grade II C17th, sits behind a high late C20th brick wall, with fine views south and north across the river valley. Both houses are rendered with plain tile roofs and casement windows.



Evolution - Area Settlement Characteristics. 2



Hoe Mill. Area 13.

With hedgerows and arable fields on both sides, Hoe Mill Road falls gently towards the northern boundary of the Parish. As it approaches the River Chelmer oak and ash trees dot the sparse hedgerows and there is pasture on the now visible flood plain.

At a sharp bend in the road, marked by an ancient oak is the sash windowed, slate roofed, rendered Hoe Mill Farm (1832), now a private house and next to it recently renovated plain and pantile weatherboard barns. An early C20th rendered, slate roofed bungalow and another large oak sit above the next sharp bend and as the road nears the bridge, there are very tall redwood cedars to one side. Adjacent to the bungalow is a pair of tall iron gates leading to the elegant Grade II mid-C19th Hoe Mill House, (gault brick, slate roofs and sash windows), barely visible through the hedgerow and trees.



2 Evolution - Area Settlement Characteristics.



Blue Mill Lane entrance to Blue Mill



Blue Mill Lane from Curling Tye Green

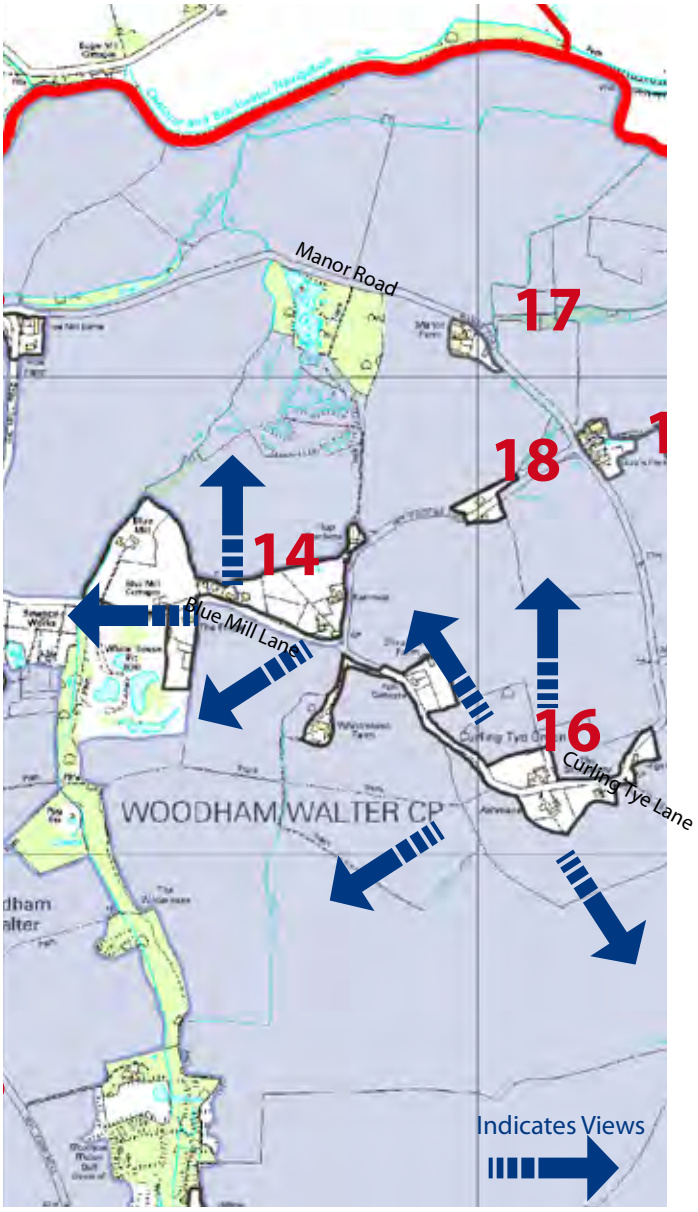
Blue Mill Lane (14) and Curling Tye Lane (16).

Winding eastwards away from the village centre, Blue Mill Lane falls gently before crossing a stream, rising and then levelling out as it runs into Curling Tye Lane. With arable fields and high hedgerows interspersed with oak and ash on both sides of these narrow lanes there are only occasional restricted views north to the river.

The two lanes accommodate twenty houses and the settlement pattern is one of small clusters with individual houses between.

Opposite an area of woodland up an unmade track next to the stream sits the brick and weatherboard Grade II Blue Mill. Close by on the lane are an early C20th brick semi, a large mid C20th brick house behind a high trimmed hedge, 'The Cats' public house, a late C18th brick and weatherboard 'double cottage' and next to it a C19th rendered cottage and another large rendered house. Plain tiles predominate and apart from Blue Mill that has part sash windows, timber casement windows are in wide use.

Further along, near the junction with Hop Gardens Lane are two pairs of semi-detached houses, both C20th one early, one mid Century; off the lane the Listed and converted Whitehouse Farm and further along Shrublands Farm and another house. Curling Tye Lane now starts to rise gently and in 200m at Curling Tye Green is a final cluster of four houses, three of which are Grade II, together with a number of Listed weatherboard barns with pantile roofs. Plain tiles, render and both sash and casement windows predominate. All sit behind high hedgerows, trimmed hedges, narrow greenswards and ditches.



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Unmade tracks now run north and south. The former to a natural slate, red brick semi-detached house backed by woodland and rising arable land in the front. The track running south has a bitty hedgerow on one side beyond which sits the low lying water meadow and on the other, two pairs of semi-detached houses and a detached house, all mid-C20th red brick, with hipped roofs, the first slate, the others plain tiles. Casement windows predominate. Beyond, the mid-C18th red brick, plain tiled with slate dormers and sash windows, Lodge Farm sit slightly elevated behind low iron railings next to a number of C18th-C20th outbuildings. There is a fine view across an ancient fishpond, the water meadow and rising arable fields. The track ends, running into a footpath, with views south across fields and woodland.



2 Evolution - Area Settlement Characteristics.



West towards Manor Farm



Across to the Village from Manor Road

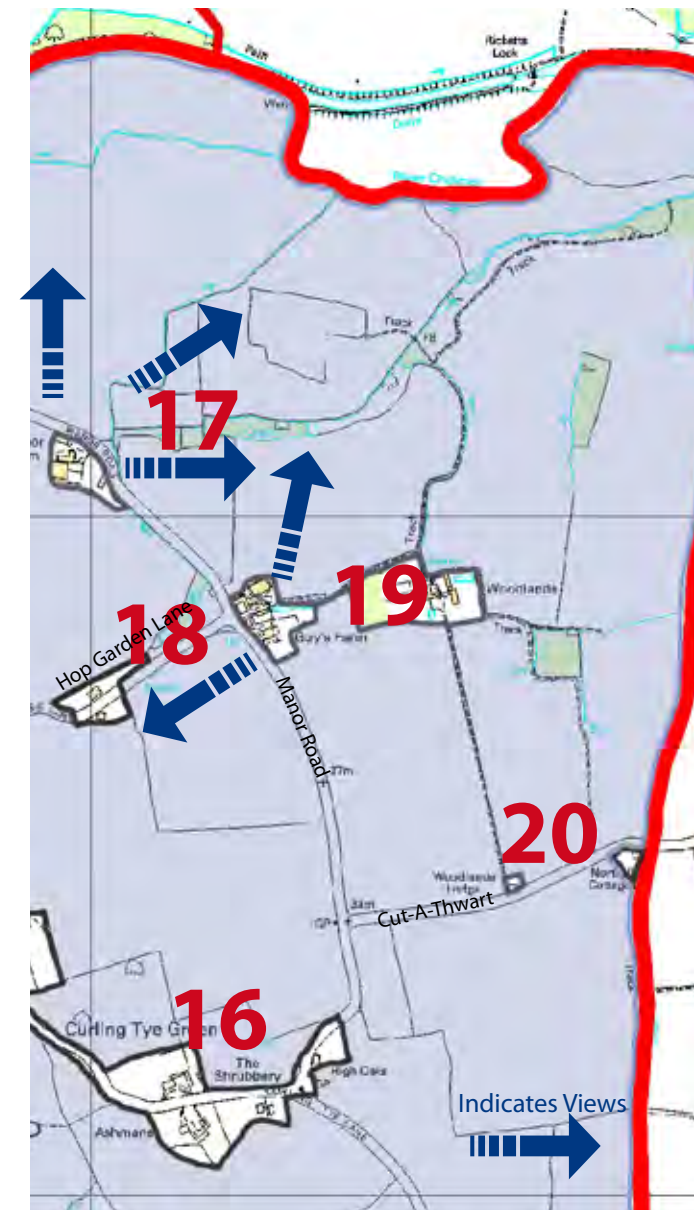


Guy's Farm

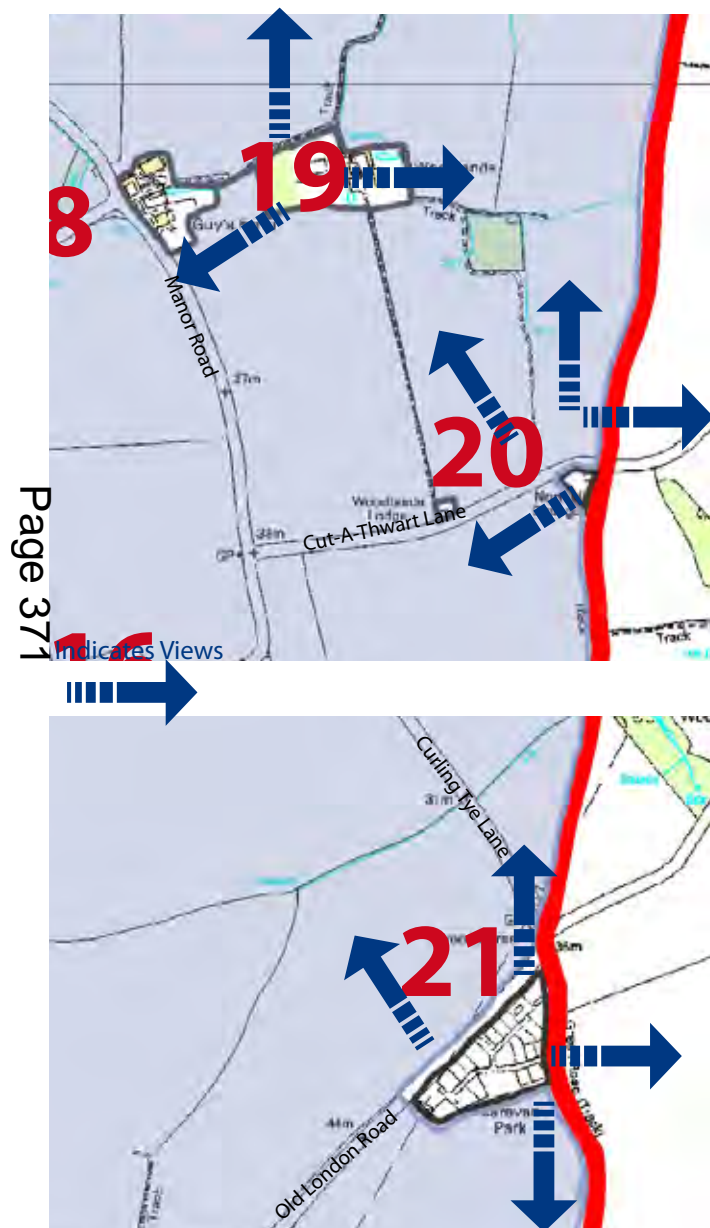
Manor Farm (17), Hop Garden Lane and Guys Farm (18).

Manor Road runs eastwards through open arable field interspersed with high hedgerows and woodland and parallel to, and just above the Chelmer flood plain. At the early C19th rendered, slate roofed Manor Farm together, with its C19th and C20th outbuildings, a number of tall trees line both sides of the road, which now moves progressively southwards. It passes a single storey weatherboard lodge house, Hop Gardens Lane and then opposite, the early C19th Grade II Guys Farm, a 2-storey with attics rendered building with plain tiled roofs and a prominent gambrel slate roof surrounded by C19th outbuildings, hedgerows and pasture and arable fields. Timber sash windows predominate.

From Guys Farm, Hop Gardens Lane winds westwards through woodland, hedgerows and arable fields, passing two mid-C20th houses, both with plain tiled roofs and casement windows, one rendered, the other red brick. Further along, the C19th semi-detached plain tiled and rendered 'Hop Gardens', with sash windows one side, casement the other. Here the lane turns southwards, with tall hedgerows covered with hops and passing another rendered property before meeting Blue Mill Lane.



Evolution - Area Settlement Characteristics. 2



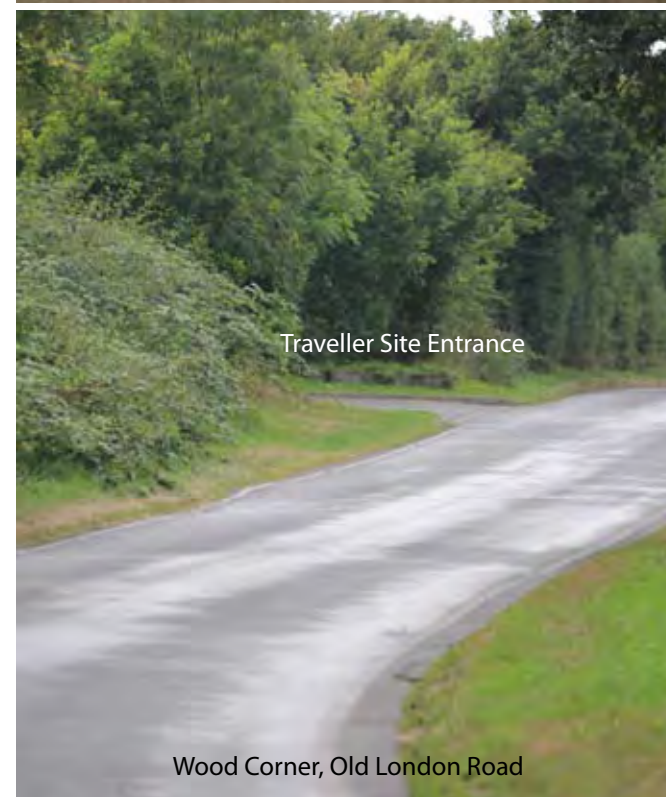
Cut-a-Thwart Lane (19 and 20).

The secluded Cut-a-Thwart Lane runs eastwards from Manor Road, with patchy hedgerows on both sides, affording occasional views down to the tree lined river and beyond. There are two houses in this very narrow lane. 'Woodlands Lodge', a 2-storey slate roofed rendered house notable for the two Doric columns on its front facade, which sits at the top of the track to the main house, and on the Parish boundary 'Northall Cottages', a C19th two storey red brick house with a plain tiled roof and dormers and casement windows.

The open track to the main house runs northwards for 400m, before reaching 'Woodlands', a substantial 2-storey rendered Grade II C18th house with plain tile roofs and large shuttered sash windows, surrounded by gardens, woodland and enclosed fields.

Wood Corner (21).

This 1990's constructed 20 pitch Traveller Site, enclosed by tall hedgerows and surrounded by arable fields is located on Old London Road at its junction with Curling Tye Lane on the eastern boundary of the Parish. The entrance to the site is very well kept with mown greenswathe.



2 Evolution - Building Styles



Plaster/ Render.

From the C16th–C21st there has been a proliferation of plastered /rendered buildings generally painted white or pastel. Early examples are in lime plaster (Retreat Farm c1580, Wingtons c1690, Elwy Cottages c1770), whilst later buildings such as The Spindle, Jasmine House, Ambleside and Ashputtles (all late C20th /early C21st) are in cement based render. Early exposed timber framed buildings (Ravens c1580, The Bell c1600, West Bowers Hall C16th) have lime plaster infill panels. Recently render has been used as a first floor band above facing brickwork (Hawkins Farm Cottages c1960, Watenlath 1985, Somerset early C21st), occasionally for over/under window panels (Dobbins c1970)) and as a gable end spandrel (Dabblers c1960's). Early C20th Arts and Crafts houses (The Warren House, Crossways) have parget plaster. A few houses have pebbledash (The Maples 1906, Church Corner c1950's).

Brickwork.

St Michaels (1564) has Tudor red facing brickwork, but for domestic buildings Lodge Farm (1757) appears to be the earliest example, although it was not in general use (chimney stacks and plinths apart) until C19th. Early examples are generally red facings (Gunhill Farmhouse 1815, Nursery Cottages 1874), but gaults have been used at Mead House (c1814) and Hoe Mill House (c1845). Contrasting brick arches, bands and quoins were used occasionally from mid C19th (Nursery Cottages, Lynton House c1905). From mid C20th brick colours in addition to red have been in common use: light orange multis and dapple lights in Mead Pastures, orange multis in Brook Close and yellow multis at Poldhu and Robins Hill (all c1970). All houses have one or more chimney stacks. Generally of brick, with round, often roll topped chimney pots, they form a strong visual element: Northall Cottages, The Warren House.

Timber Weatherboard.

Used from the late C18th, mostly horizontal and generally painted white/cream: Blue Mill c1729, Apple Cottage, Congregational Chapel 1881, Womens Club 1906, Stockwells mid C20th. High Oaks Cottage mid C20th has recently been re-clad in cream boarding. Numerous agricultural buildings (Curling Tye) and a few houses have black boarding: (Black Cottage C17th) and Beightons (c1970) where it is vertical. Horizontal natural larch is used at the Longwood (2015).

Other wall finishes.

Since mid C20th tile hanging (Parsonage Pightle c1960, Brookside c1970) and shiplap (1, Sparhawks c1960) have been used mostly in small areas such as dormers or part elevations. Berberis (2016) has an elevation in Cedral cladding.

Evolution - Building Styles 2

Windows.

Windows form a strong visual element. In older buildings, where storey heights are low, openings have vertical subdivisions and are small and squarish (Elwy Cottages, Black Cottages), or occasionally horizontal (West Bowers Hall).

Vertical sash windows of various pattern, often with a pronounced vertical emphasis were first used at Retreat Farm (late C18th) and from early C19th (Gunhill Farmhouse 1815) - early C20th (Lynton House 1905, The Maples 1906), they were the predominant window type. Fine examples are provided by Woodlands early C19th, Manor Farm c1830 and Hoe Mill House mid C19th.

Casement windows with vertical subdivisions, often with top opening fanlights have predominated since early C20th, although in the 1960's there was a shift to larger more horizontal openings (Atlantis, Hawkins Farm Cottages). Late C20th and C21st houses generally have casements. Leaded windows (Ravens C15th, The Warren House 1906, Summerfield early C21st) and bay windows (West Bowers Hall C16th, Hatchmans 1873, Wallingford mid C20th) appear on houses of all ages. The Almshouses (1908) have Gothic lancet windows. Plastic replacement windows have been used occasionally.

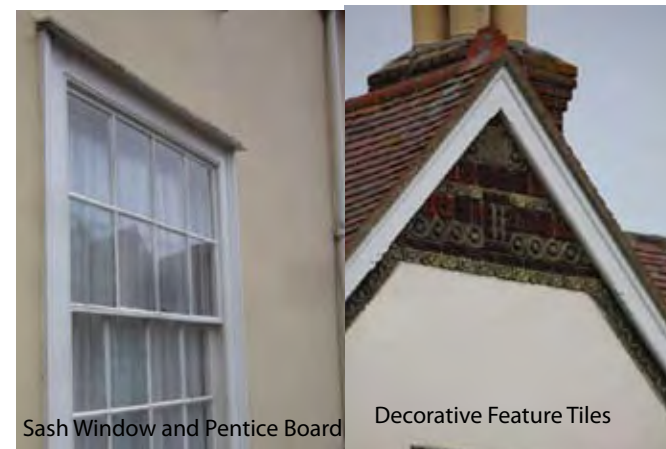
Decorative elements.

A number of houses have decorative embellishments. Tuscan columns and pilasters (Hoe Mill House), Ionic columns (Woodlands Lodge), decorative gable end terracotta tiles (Lynton House), brick dentils and gauged brick arches (Blue Mill), terracotta egg and dart lintels (Nursery Villas), blank 'windows' (Gunhill Farmhouse, Manor Farm), external shutters (Woodlands, The Warren House). There is occasional use of relief decoration to rendering (Tudor Rose c1970). Decorative porticos are a feature of numerous houses of all ages (Blue Mill, 3-18 Mead Pastures).

Roofs.

Most buildings have conventional pitched roofs. Plain clay peg tiles on roofs with a pitch of 40-55deg. were in general use up until C19th (Bassetts c1665, Elwy Cottages c1770). Welsh slate on pitches down to 25 deg. appeared in the C19th at Mead House c1814, Manor Farm c1830 and Hoe Mill Farm c1840, Nursery Villas 1899, Glendale c1925 and a number of other buildings.

Numerous buildings have hipped roofs (Blue Mill c1729, Manor Farm) and Guys Farm (C17th/ C18th) and Ravensfield (mid C20th) have gambrels. There is a catslide roof at Gunhill Farmhouse (1815). Most early outbuildings have clay pantile roofs (various at Curling Tye, North Hall Cottages, Nursery Cottages). The Barn (C18th) has the only thatched roof in the Village.



Sash Window and Pentice Board

Decorative Feature Tiles



Lancet Windows

Blank Windows



Ionic Columns

Black Boarding and Pantile Roof

2 Evolution - Building Styles

Roofs continued.

In the late C19th machine made red/brown plain clay tiles were available and used at The Warren House (c1906), the Almshouses (1908), the Village Hall (1928), 1-6, Church Corner (c1950's), Yeomans (c 1990), Somerset and Jasmine (both early C21st), Berberis (2016).

Machine made interlocking tiles, either cambered, (The Maples 1906, Church Corner c1950's, Gunhill Cottages c1960) or flat (Cartref, Valley, c1970's), have been used on numerous houses. Roman profile/pantiles are common: 1-20 Rectory Road (c1950), Brook Close (c1970), Summerfield (early C21st). Flat roofs have appeared infrequently, generally on dormers or domestic garages.

Dormers.

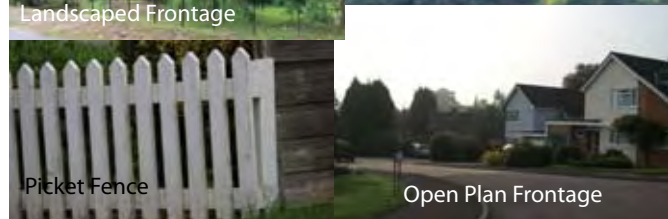
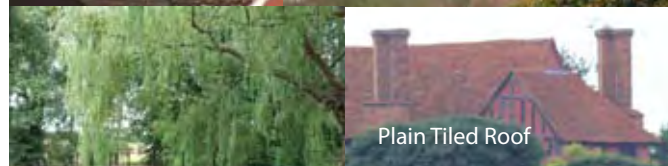
Dormers have been used since the C16th and are pitched or hipped in tiles or slate to match main roofs or flat in lead. At Retreat Farm they are pitched in plain tiles, at Blackberry Cottage plain tiled catslide, at Ashmans, plain tiled hipped, at Hoe Mill House slate, at The Warren House flat in lead and in a number of houses late C20th flat in roofing felt.

Porches.

A feature on some houses either fully enclosed, (Milestone Barn late C20th) or open sided, (Ambleside) and generally with a pitched or lean-to plain tiled roof. The Almshouses (closed) and Hoe Mill House (open with flat roof) provide fine examples.

Boundary Treatments.

With the exception of farmsteads and one or two large houses, buildings are orientated towards the road or lane with hedges, trees, greenswards and sometimes ditches, running along front boundaries. White painted picket fences (Lynton House), low iron railings (Nursery Cottages), and tall brick walls and high metal gates (Grove Manor) are in occasional use. Most houses have vehicular access on the front boundary and there are a few examples of open plan front gardens (Mead Pastures, Brook Close).



Evolution - Heritage Assets 2

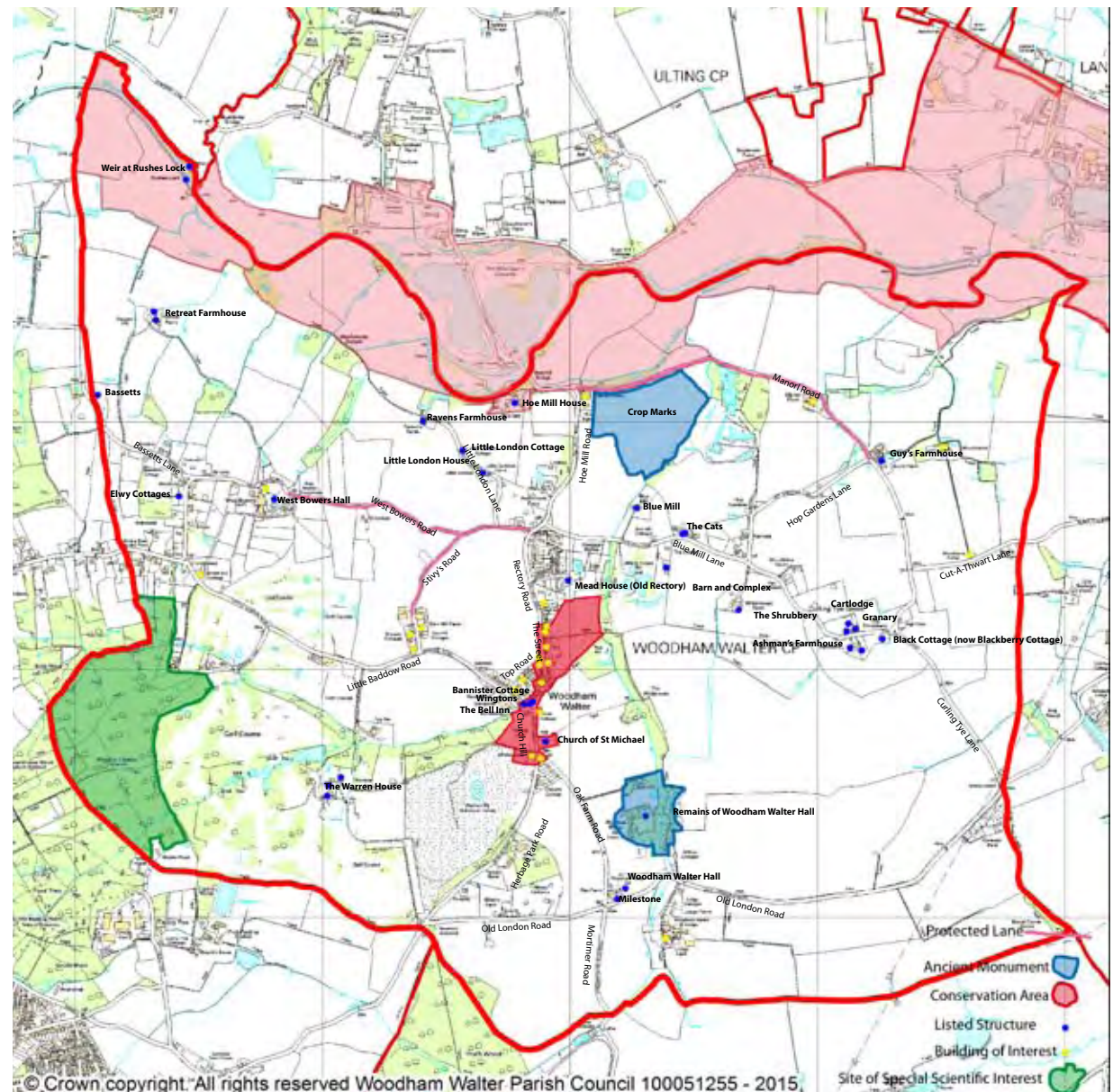
Listed Buildings D3

Illustrated on adjacent map

Ashman's Farmhouse and Barn Grade II; Curling Tye Lane
Bannister Cottage Grade II; 2 The Street.
Barn and complex Grade II; Whitehouse Farm Curling Tye Lane.
Bassetts Grade II; Tofts Chase.
Black Cottage (now Blackberry Cottage) Grade II; Curling Tye Lane.
Blue Mill and Mill House Grade II; Blue Mill Lane
Cartlodge Grade II; Curling Tye Lane.
Church of St Michael Grade II*; Church Hill
Elwy Cottages Grade II; Bassetts Lane.
Granary Grade II; Curling Tye Lane.
Guy's Farmhouse Grade II; Manor Road.
Hoe Mill House Grade II; The Causeway.
Little London Cottage Grade II; Little London Lane.
Little London House Grade II; Little London Lane.
Mead House (Old Rectory) Grade II; Woodham Walter.
Milestone Grade II; Old London Road
Pump Grade II; Blue Mill Lane.
Ravens Farmhouse Grade II; Little London Lane.
Retreat Farmhouse Grade II; Woodham Walter.
Ruins of Old Woodham Walter Hall Grade II; Old London Road.
Rushes Lock and Lock Gates Grade II; Bumfords Lane.
The Bell Inn Grade II; Little Baddow Road.
The Cats Grade II Blue Mill Lane
The Shrubbery Grade II; Curling Tye Lane.
The Warren House Grade II; The Warren
Weir at Rushes Lock Grade II; Bumfords Lane, Ulting.
West Bowers Hall Grade II*; W Bowers Road.
Wingtons Grade II; 2 The Street.
Woodham Walter Hall Grade II; Old London Road.

Ancient Monuments D3

Remains of Woodham Walter Hall
Crop Marks East of Hoe Mill



2 Evolution - Heritage Assets

Guidance

Heritage Assets

2.01 *Wherever possible, heritage assets should be saved rather than demolished and replaced, retaining as much historic fabric as possible.*

2.02 *Encompassing a building's significance, preserve or enhance the special character, setting and townscape in a manner appropriate to a heritage asset.*

2.03 *New buildings and building extensions should respect the scale, form, and design of heritage asset buildings in the vicinity.*

2.04 *Development within the Conservation Area should contribute by having a character and appearance serving to enhance and preserve the area.*

Conservation and Heritage Assets **D1 D3**

The Parish has the great fortune to have some twenty-nine Listed Buildings and two Ancient Monuments. Of the Listed Buildings, two are listed as Grade II *, West Bowers Hall and the Church of St Michael the Archangel. The two Ancient Monuments are the remains of Woodham Walter Hall and the crop marks east of Hoe Mill. The full list of buildings together with their locations can be found on page 20. In addition to these there are a number of other buildings that are of architectural or historic interest to the Village that are to be put forward for inclusion on the District Council's Local List.

Building Listing helps in acknowledging and understanding shared history. Listing marks and celebrates a building's special architectural and historic interest, and its setting. There are several categories of Listing that are broadly: Grade I buildings are of exceptional interest, sometimes considered to be internationally important; only 2.5% of listed buildings are Grade I. Grade II* buildings are particularly important buildings of more than special interest; just 5.5% of listed buildings are Grade II*. Grade II buildings are nationally important and of special interest; 92% of all listed buildings are in this class. In England there are over 374,000 listed building entries on the register.

There is also a proposed Conservation Area for the Village core under consideration the details of which can be found on page 22. A Conservation Area is one that is of special architectural interest that has a character or appearance which has merit in being preserved and enhanced.

It is not just the buildings that are significant but also their settings and the 'added value' that they bring to the character of the area. Therefore the landscape has as much importance as the townscape.



St Michael the Archangel,
Listed Grade II*



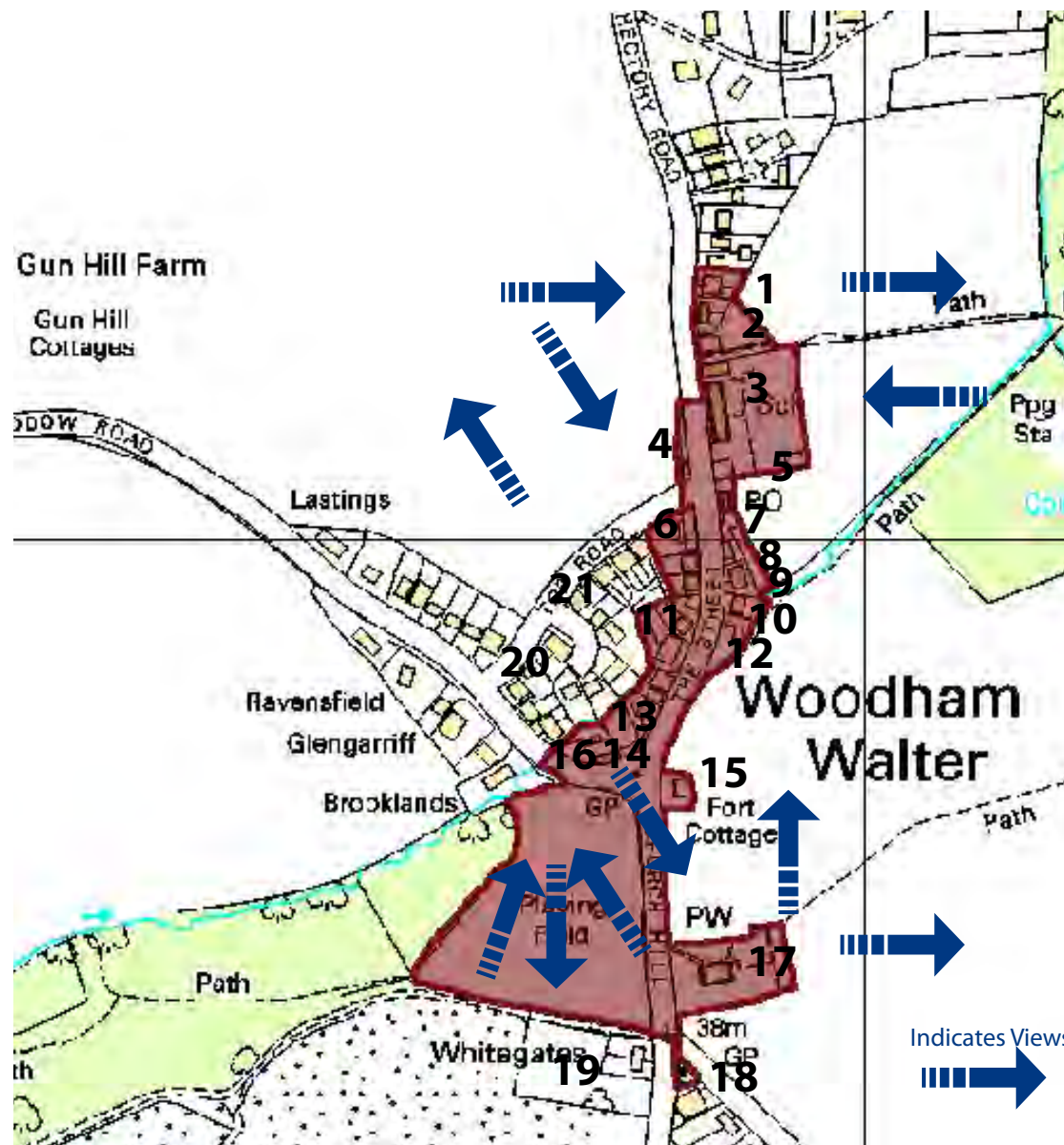
West Bowers Hall,
Listed Grade II*

Evolution - Conservation Area 2

Properties of Interest

Clockwise from the north

- 1 The Maples 1 and 2, (Old Village Dairy).
- 2 Robert Falkener Almshouses 1 and 2 C20.
- 3 Original section of the School C19.
- 4 Ash Cottage (The Village Undertaker's House).
- 5 Chapel Stores C19, (Formerly a Non-Conformist Chapel).
- 6 The Queen Victoria C19, (Formerly a Cottage and Beer House).
- 7 Severn House C17, (Farm Labourers' Cottages now one house).
- 8 Red Brick Cottages C19.
- 9 Wendover Cottage.
- 10 Lynton House (Old Village Bakery).
- 11 Ferndale.
- 12 Bannister Cottage C18, Grade II, (Formerly the Village Poorhouse).
- 13 Wingtons C17, Grade II. (Old Village Shop).
- 14 Fort Cottage C17, (Farm Labourer's Cottage).
- 15 The Bell C16.
- 16 St Michael's Church C16, Grade II*.
- 17 Church Corner.
- 18 Whitegates C20, (Arts and Crafts Style House).
- 19 White Lodge C19, (Farm Labourer's Cottage).
- 20 The Womens' Club C20, (Former Garden 'Room').



2 Evolution - Threats to Character

The perceived threats to the described Village character include:

- Replacement dwellings or extensively extended dwellings that do not relate to the Village Character in scale or architectural style.
- Threats to the historic linear settlement pattern and the visual impact of new development from the unbuilt village fringe.
- The existing variety of building forms, storey heights, plot widths and changes in building lines can be threatened by redevelopment which involves the amalgamation of plots and can apply a uniform massing and style throughout the development exacerbated by the removal of greenswards, hedges and ditches.
- Failure to observe the local vernacular of building scale and form of elevations introducing discordant or overwhelming buildings into the Village and street scene.
- The loss of features such as chimneys, eaves details, traditional roof spans and additive forms can lead to a characterless silhouette and blandness diminishing local character.
- The use of materials that lack traditional textures, weathering qualities and hues prevents new development from being subsumed into its surroundings. The use of local materials in the wrong context can have a similar effect.
- Removal of hedgerows, hedgerow trees and other landscape features in rural development.
- The use of architectural details that are incorrectly applied.
- Potential increases in through and 'rat-run' traffic arising from out-of-Parish development may add to the environmental and safety impact and could be visually detracting thereby jeopardising an appreciation of the distinctive character of Woodham Walter.
- Provision of parking and vehicular access erodes the limited amenity space and close knit settings of traditional groupings.
- The increase in size of extended properties and replacement dwellings reduces the number of smaller, more affordable dwellings and therefore does not provide the accommodation required by today's younger age group to attract them to the Village.
- Modern infilling with no locally distinctive character prevents the assimilation of new development into its surroundings. The use of imported materials that lack the texture, weathering qualities and hue of traditional local materials also prevents the assimilation of such development.



The Built Environment - Background 3

Presumption **S1 S7 D3**

The Village Design Statement (VDS) is a tool to ensure that any new development in Woodham Walter respects and reflects the distinctive visual character of the village and its environs and fulfills the criteria and policies of the Local Authority included in the Local Development Plan. The VDS does not detail what type of development should take place in the Parish nor the state of local services, but strives to promote good design in accord with the character assessment of each area of defined settlements.

The VDS is primarily concerned with the visual character of a village and how it might be protected or enhanced. It is an important evidence based document that has been wholly driven by the response from the community to the VDS Questionnaire. Results of the Questionnaire can be found on the Parish Council website.

'Sustainable Development' is a phrase that is often used in conjunction with the built environment and one that is frequently heard, but what is sustainable development? Within the planning environment the definition of 'sustainable' is considered by the National Planning Policy Framework (NPPF) as having three dimensions:

- **an economic role** – contributing to building a strong, responsive and competitive economy, by ensuring that sufficient land of the right type is available in the right places and at the right time to support growth and innovation; and by identifying and coordinating development requirements, including the provision of infrastructure;
- **a social role** – supporting strong, vibrant and healthy communities, by providing the supply of housing required to meet the needs of present and future generations; and by creating a high quality built environment, with accessible local services that reflect the community's needs and support its health, social and cultural well-being;
- **an environmental role** – contributing to protecting and enhancing our natural, built and historic environment; and, as part of this, helping to improve biodiversity, use natural resources prudently, minimise waste and pollution, and mitigate and adapt to climate change including moving to a low carbon economy.

Objectives

Presumption

- 3.01 *Guide owners and occupiers on caring for their village.*
- 3.02 *Guide developers on design aspects that are regarded as acceptable to the community, including the setting of buildings and the use of materials.*
- 3.03 *Guide people undertaking building work.*
- 3.04 *Assist in the protection of local heritage buildings and artefacts particularly in the conservation area.*
S1 D3
- 5.05 *Protect and visually improve open spaces and the street scene.*
S1
- 3.06 *Ensure conservation and protection of those qualities and characteristics of the village and its environs which are most valued by its inhabitants.*
S1 S7
- 3.07 *Assist Council Officers in the determination of Village planning applications.*

3 The Built Environment - Background

Background

Essential to the concept of sustainable development is an approach to building that endeavours to balance different, and often competing, needs against an awareness of the environmental, social and economic limitations. It is within the fabric and spirit of this that this VDS has been compiled.

Planning is governed by the Town and Country Planning Act and the Planning (Listed Buildings and Conservation Areas Act). For Woodham Walter these acts are administered by the Maldon District Council with Historic England and the Conservation Officer acting as consultees. Under the Ecclesiastical Exemption Order, the Diocesan Advisory Committee deal with internal Church alterations.

The Village of Woodham Walter is based on an historical road layout that contains a local distributor road linking the A414 at Runsell Green to Ulting and Hatfield Peverel together with lanes linking other parts of the Parish and to Little Baddow. These lanes are in the main narrow without pedestrian ways and are at times overburdened with traffic and have influenced the development of the Village. Evidence based responses to the Questionnaire expressed strong opinions that any further development would intensify the current road inadequacies.

The areas outside of the existing defined settlement area have agricultural or recreational uses and in the past have included gravel extraction. Agricultural use is a major factor affecting future development and a significant majority of questionnaire respondents did not wish to see development extended beyond the defined settlement area or to the detriment of agricultural land.

For economic sustainability services infrastructure is an important consideration. The Village is served with electricity that is mainly distributed by overhead power cables and these impact on the open views. In new development they should be laid underground; there is no natural gas distribution network so consideration should be given to LPG (liquid petroleum gas) storage in new development. Water supply within the Village core is via underground pipework but in other areas is drawn from wells and springs thereby influencing siting. *Superfast* Broadband is programmed to be rolled out to the central Village area and will influence the design and use of home offices. Mobile telephone reception is improving in some areas of the Village according to topography. The absence of good public transport requires use of the motorcar which then becomes a major influence on design.



The Built Environment - Design Relationships 3

Settlement Pattern **S8 D1**

Of those who expressed an opinion in the questionnaire, a firm majority did not want the boundaries of the Defined Village Settlement to be expanded to accommodate any new development. However, a small majority considered that infill housing, i.e. using the land between existing houses, should be encouraged provided it did not detract from the surrounding buildings and is sympathetic in design, scale, height and materials. As an 'Arcadian' style of village, the maintenance of open views between dwellings where possible is important. An overwhelming majority did not wish to see 'back garden' developments in order to preserve the open aspects of the 'Arcadian' Village and maintain the housing density level.

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Possible Development Site as Existing



Housing Infilled Between Existing Buildings



Backland Development Increasing Density.



Defined Village Envelope Contained within Red Boundary

Guidance

Settlement Pattern

- 3.08 S8** Sustainable Development within the Village core Defined Settlement Boundary will be supported (see map).
- 3.09 D1** Wherever possible, character, landscape and local context should be maintained and enhanced.
- 3.10 H4** The use of infill sites should be encouraged, but with any development recognising the integrity of the adjacent buildings in size, scale, height and materials.

Preferred Options

- 3.11** Where possible open rural views should not be obstructed by backland development whether inside the Defined Settlement or not.
- 3.12** Outside of the Defined Settlement Area only existing sites with unsustainable buildings should be considered for development and sustainability should be considered as to its appropriateness.

3 The Built Environment - Design Relationships

Guidance

Design Relationships

- 3.13 **D1** Retain a mix of designs with a rural character commensurate with that of the Village.
- 3.14 **D1** Ensure that buildings reflect the relationship with the site and adjoining structures.
- 3.15 **H1 H2** Housing types should be mixed and appropriate in size and design to augment the particular site and immediate locaton.

Preferred Options

- 3.16 Siting of any new development should be such so as not to obscure any of the open vistas across the rural landscape.
- 3.17 Avoid 'mock period' design. Utilise contemporary design within the rural context. Buildings should be in proportion and in scale with surrounding structures.

Design Relationships **D1 H1 H2**

Woodham Walter has an eclectic mixture of buildings encompassing listed buildings to C20th dwellings. They include religious, recreational, educational, community and residential and range between one and two storeys, with some having rooms in roofs. Their relative relationships with adjoining buildings, choice of materials and scale add to the Village character.

Importantly, the siting of buildings allows many views through to open country emphasising the rural nature of the community. Overwhelmingly, questionnaire respondents considered it critical to maintain this building mixture to retain the established Village character and, because of the open views, the building density. Some concern was voiced at the possibility that the Village would become a ribbon development that would destroy its heart and ambience.

The majority of respondents were opposed to 'mock period' rather than contemporary design providing that it fitted the Village character. Considerable concern was expressed in the narrative responses that the current mix of housing was becoming biased towards the larger family unit rather than what the Village needed to retain younger residents, mixed housing affordable to a broad range of people.



The Built Environment - Design Relationships 3

Design Relationships - continued

S8 D2 E4 H4

Questionnaire respondents were concerned at the proliferation of replacement dwellings designed in an unrelated style and size and bearing little relationship to the site, adjacent buildings or the impact on the Village. A number of respondents expressed concern at the 'creeping urbanism' that was manifest in high walled, gated housing completely out of character with the Village, the street scene and the rural setting and are generally inappropriate according to area characteristics.

A large majority of respondents considered that any development of new build or alteration outside of the Defined Settlement Area should be designed to satisfy the guidelines that apply to the core of the Village.

A majority of respondents considered that alternative uses should be found for proven redundant agricultural buildings in sustainable locations to avoid decay and demolition as they 'punctuate' the rural landscape. Uses for such buildings would include proposals supporting employment generation or on rare occasions, residential. The demolition of such buildings detracts from the rural nature of the Village. At the same time they did not wish to see any loss of agricultural land to development unless it was associated with agricultural or other land-based uses.

An overwhelming majority of respondents considered that recycling facilities and green waste bins should be incorporated into any new design together with bicycle storage. Such structures need to be large enough for the containers, conveniently located for collection and built in compatible materials.

When considering the design and planning of new developments, a majority of respondents judged that non-arable grazing land, green swards and grassed verges were an essential part of the visual amenity of the Village and should be incorporated and maintained as such.



Recycle Bin Store



Keep verges and swards



Change of Use for Redundant Agricultural Buildings in sustainable locations



Guidance

Design Relationships

3.18 S8 Avoid urbanisation of rural area by ensuring new designs are compatible with the surrounding area.

3.19 H4 Alterations, extensions, rebuilds and new builds outside of the Defined Settlement Area should comply with the guidance set out in this document.

3.20 E4 Redundant agricultural buildings should be retained wherever possible. Where retention is not possible, alternative uses should be found for providing there is no detrimental loss of agricultural land.

3.21 D2 Design provision should be made in all new projects for the covered storing of re-cycling bins and bicycles.

Preferred Options

3.22 Where possible any new developments should make provision for the inclusion of grass verges and swards which should be permanently maintained.

3 The Built Environment - Design Relationships

Guidance

Design Relationships

- 3.23 **D1** Consider 'Secured by Design' for any new building project (Subject to the Government's Housing Standards Review).
- 3.24 **D4** Solar panels where possible should be discreetly located and not detract from the street scene or long views. They should be fixed in accordance with the regulations.
- 3.25 **D4** The Village does not support the installation of commercial wind turbines or wind farms unless it can be demonstrated not to have an adverse visual, noise or wildlife impact either individually or cumulatively.

Preferred Options

- 3.26 Satellite dishes should be of minimum size and where possible discreetly located on the side or rear of dwellings.
- 3.27 Mobile 'phone or wifi masts should be incorporated and concealed within an existing structure or camouflaged.

Design Relationships - continued **D1 D4**

A majority of respondents considered that the location of satellite dishes should be discreet. Dishes should therefore be of the minimum appropriate size and fixed to the side or rear of the property. They, like some (but not all) solar panel installations, are categorised as Permitted Development but it is hoped that Residents will take heed of the guidance offered. A small majority (3.26%) of respondents considered that the installation of solar panels should be encouraged providing they are discreetly located and do not adversely affect the street scene or can be viewed from other Village vistas.

A substantial majority of residents considered that large commercial wind turbines have an adverse impact on landscape character, wildlife, heritage assets, footpath safety and visual impact. Installations should not be encouraged but considered on a case by case basis. Similarly, a substantial majority considered that the erection of mobile 'phone or wifi masts should be discouraged. However, the need for proper mobile 'phone and wifi services is supported by the Villagers and as such, masts should be discreetly located and where possible camouflaged or concealed within an existing structure.

An overwhelming majority considered that overhead cables should be placed underground. The concealing of overhead cables and supporting system would be in line with maintaining the rural visual impact of the Village. This would include new sub-stations if they are required.

Crime is not a major issue within the Village but it nevertheless exists together with its victim impact. A significant majority of respondents agreed that any new development including alterations and extensions should be designed with security in mind. The Association of Chief Police Officers (ACPO) have created 'Secured by Design' which should be adopted for all new developments. It covers many aspects included within this document such as parking, lighting, window and door security, boundary treatments etc and may be found at www.securedbydesign.com.



Satellite Dishes Should be Discreet



Commercial Wind Turbine
See above

The Built Environment - Materials 3

Materials

D1 D2 D3 H4

Materials for extensions and alterations should, wherever possible, match or complement the existing building and its setting on the site relative to the settlement characterisation. Similarly, materials for new build should complement the site and respect adjacent buildings whilst at the same time reflecting the rural nature of the Village. Use of materials from sustainable sources should be maximised. Facing brickwork in red and brown hues, pargetting, render and feather edged boarding are all acceptable materials painted in Woodham Walter colours. Cementitious self-coloured cladding is no substitute for traditional timber feather edged boarding. Painted shiplap boarding is more suited to American colonial style buildings and not a rural Essex environment.

Roofing should be clay peg tiles, plain tiles, pantiles or slates depending upon the existing property and should add to the rural character of the location.

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Shiplap Boarding



Feather Edge Timber Boarding



Facing Brickwork



The Colours of Woodham Walter



Clay Peg Tiles



Concrete Plain Tiles



Slate



Concrete Pantiles

Guidance

Alterations and Extensions

3.28 Replacement windows should be consistent with the age and style of building or complement or match the existing.

**D1
D3
H4**

3.29 Materials for extensions should be compatible with the existing building and be of a local Woodham Walter vernacular. The use of materials from sustainable sources should be maximised.

**D1
D2
H4**

3.30 Suitable materials include brick, render, pargetting, slate, tile, feather-edged boarding, glass.

Preferred Options

3.31 Suitable materials include brick, render, pargetting, slate, tile, feather-edged boarding, glass.

3.32 Render or pargetting should be decorated in traditional Woodham Walter Colours.

3 The Built Environment - Alterations and Extensions

Guidance

Alterations and Extensions

3.33 Extensions to buildings should be proportional to the main dwelling size and site plot. They should be subservient to the main building by setting back from the building line by 500mm and not overlook adjoining plots.

H4
D1

3.34 Extensions in all cases should make a positive contribution to the character of the main building, its site and its neighbours.

H4

3.35 Extension roofs should be designed so as not to impact on the main dwelling and preferably should be pitched and not flat.

Preferred Options

3.36 Extension roofs should be designed so as not to impact on the main dwelling and preferably should be pitched and not flat.

3.37 Extensions into roof spaces should not project beyond the roof plane.

3.38 Single storey extensions should have a roof of a form and design which matches the host building.

Alterations and Extensions **D1 H4**

Despite a majority response indicating that unrestricted alterations to buildings should not be permitted, there are Permitted Development rights that take an alteration or extension out of the planning process. It is hoped that Residents contemplating such a development will approach the project by considering the design guidance offered here. Extensions to existing properties should be design led, sympathetic in scale, height and materials to their sites, existing buildings and surrounding environment and make a positive contribution to the character of the main building and street scene by not being visually intrusive. Every property should be treated on its own merits.

Large extensions, conservatories and orangeries should be appropriate to the size of the building and the building plot. A majority considered that the size of extensions should be proportional with the main dwelling size and should not overlook adjoining property. Extensions should generally be subservient to the main building and should be designed to break up the visual mass of the extension and the design should complement the varied rural and 'Archadian' character of the village.

It was the opinion of a very significant number of respondents that extensions into an existing roof space should not protrude beyond the roof plane.



Single Storey Side Extension
Reduced Ridge Height



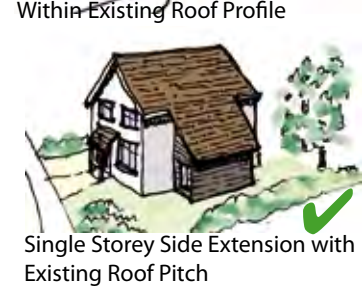
Upward Extensions to be Contained
Within Existing Roof Profile



Overlooking Flat Roof Side Extension



Two Storey Side Extension set
Back From the Building Line



Single Storey Side Extension with
Existing Roof Pitch



Oversize, Disproportionate and Out of
Character Extensions to be Avoided

The Built Environment - Alterations and Extensions 3

Alterations and Extensions - continued

There was a majority support for the inclusion of special character design features such as porches, chimneys and similar features within any new building, extension or alteration. These should be in keeping with the existing building, the surrounding structures and the general rural character of the Village. They should also be within the parameters set out elsewhere in this Village Design Statement.

Porches in particular should relate to the main building style and reflect the rural character of the village rather than being of an urban 'portico' format. Chimneys add to the character of dwellings and should be of rural vernacular style rather than period style.

Replacement windows should be similar to those that exist within the structure unless to do so would adversely alter the character of the building.

Whilst Permitted Development relative to porches is acknowledged, it is hoped that Residents will recognise the importance of a design led initiative and endeavour to comply with this guidance.



Rural Open Porch



Porch designed with Extension



'Lean To' Porch and Chimney



Urban Style Portico



Resident's Guidance

Alterations and Extensions

- 3.39 The addition of porches, lobbies and chimneys are encouraged.
- 3.40 Porches should be in keeping with and of a style that relates to the host building.
- 3.41 Where a porch is required to a late C20th building, where possible this should be designed as part of an extension or original structure.
- 3.42 Roofs should be pitched either as a dual pitch or mono-pitch and either built off a framework or bracketed off the host building wall.
- 3.43 Materials for porches should reflect the host building.
- 3.44 Chimneys of traditional construction are encouraged. Stainless steel flues should not be used unless no practical alternative is viable.

3 The Built Environment - Roofs

Guidance

Roofs

3.45 **D1** All roofs should be of an appropriate scale and design to make a positive contribution to the character of the building and its surrounding area.

Preferred Options

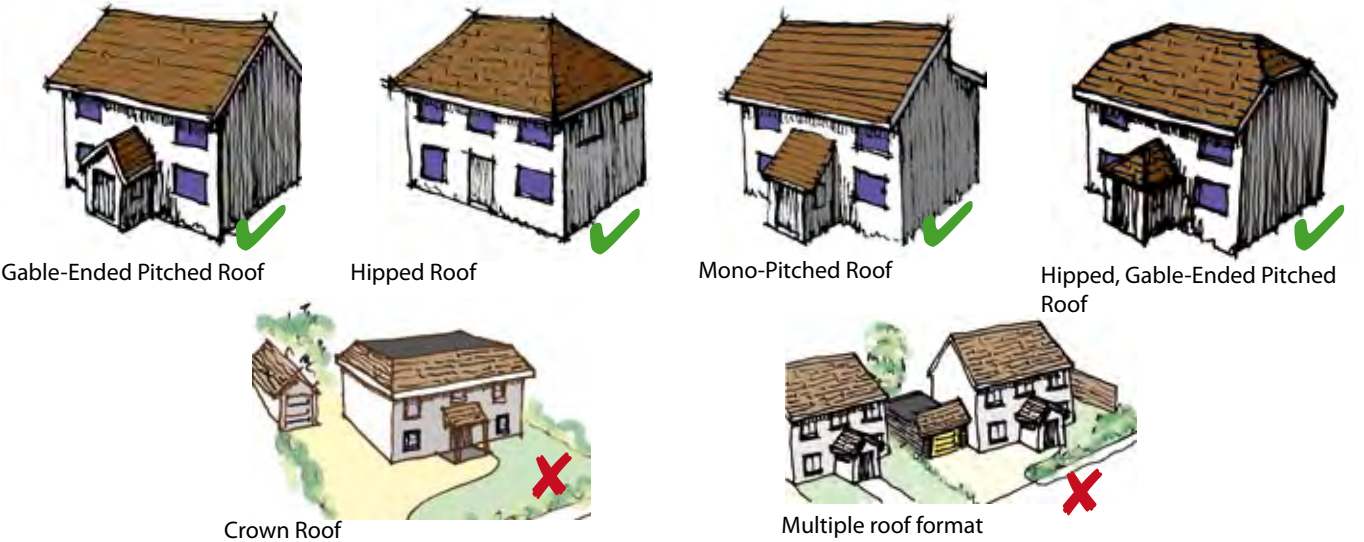
- 3.46 Where possible all roofs should be pitched either dual or mono-pitched.
- 3.47 Roof ends should have a gable, hip end or hipped gable.
- 3.48 Flat roofs should be avoided for main and dormer roofs.
- 3.49 Clay tiles or natural slates are preferred to concrete tiles and felt roofs should be avoided.
- 3.50 Crown roofs should not be used.
- 3.51 Composite roof forms should not be used as they can be seen from angles other than a straight front elevation.

Building Roofs **D1 H4**

The style of roof should be design-led and appropriate to the individual building. There was overwhelming support for the view that roofs of all new buildings and extensions should be pitched rather than flat to reflect the rural nature and established character of the Village and that the materials selected should also be in keeping with its character. Throughout the Village the majority of roofs are pitched with tile or slate coverings and there is one thatched roof at West Bowers Farm. Flat roofs are restricted to dormer windows and garages and although these are existing forms, it is not the type of design that the document promotes.

Pitched roofs would include mono-pitches, those pitched roofs with gable ends, hip ends or hipped gable ends. These design styles together with a 'cat-slide' roof should also be applied to dormers. Composite roof styles where exposed to the rear or side are not considered to be a satisfactory design solution.

The selection of materials was therefore deemed to be very important. It was considered that roofing materials, as with other building materials, should reflect the rural character of the Village and relate to surrounding buildings by using clay tiles, natural slate or concrete tiles.



The Built Environment - Roofs 3

Building Roofs - continued

There was support that upward extensions to buildings into roof spaces should be retained within the roof line of the existing premises. This precludes the addition of dormers giving the appearance of a two and a half or three-storey dwelling although roof windows or, in the case of older properties, 'heritage roof lights' would obviate the need for dormers and form a viable substitute. These would maintain the line of the roof plane.

Where the roof line of a two storey building requires to be reduced in height to relate to adjacent properties, dormer windows could be a suitable and practical alternative.

Whilst Permitted Development relative to roofs is acknowledged, it is hoped that Residents will recognise the importance of a design led initiative and endeavour to comply with this guidance. Although some flat roof dormers exist, they are not preferred in new build or alterations.

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Roof Windows Within Roof Plane



One and a half Storey with Pitched Roof Dormers to enhance Appearance



Flat Roof, Full Width Dormer



Not preferred Two and a half Storey Appearance



Gable-Ended Dormer Roof



Cat-Slide Dormer Roof



Hipped Dormer Roof



Flat Dormer Roof

Preferred Options

Roofs - continued

- 3.52 Upward, full width extensions into roof spaces should be maintained within the roof profile wherever possible.
- 3.53 Flat roofs, including crown roofs, should be avoided for main and dormer roofs.
- 3.54 Dormer roofs to be either pitched with gable or hip end or cat-slide.
- 3.55 Roof windows or heritage style rooflights to be used in preference to dormer windows.

3 The Built Environment - Boundaries

Guidance

Boundary Treatment

3.56 *Front gardens should take a soft landscaped rather than a townscape format to enhance the local character and context to protect and enhance the street scene and rural village character.*

3.57 *Boundary hedges should include indigenous species and existing hedges should not be removed but constrained in height to assist with bio-diversity.*

3.58 *Fencing should be of a rural character and height to blend in with its surroundings rather than high (2m) brick walls. Materials and design should be sympathetic to the surrounding area and the property concerned.*

Preferred Options

3.59 *Boundary walls and gates should be in context with the streetscape and Woodham Walter vernacular.*

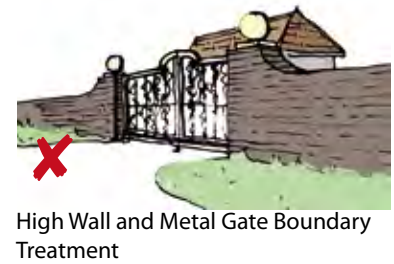
3.60 *Large areas of uniform hard standing in front of houses should be avoided. Materials and design should be influenced by the surrounding area.*

Boundary Treatment D1 N1 N2

There was an overwhelming response indicating that dwelling front gardens should follow a soft landscape rather than townscape format. Paved front gardens are visually unattractive and planning permission is required if the treated area exceeds 5m² and is impermeable. Similarly there was overwhelming response that street front boundaries should be maintained in keeping with the existing environmental characteristic of Woodham Walter. Boundaries adjacent to or looking on to heritage assets should be carefully designed to enhance the prospect.

There was agreement that the height and style of boundary materials was important within the village environment and there was very strong agreement that the planting of native hedgerows should be encouraged to integrate with other types of land uses and to support bio-diversity.

A majority of Questionnaire respondents considered that 2m high brick walls and large 2m metal gates were inappropriate boundary treatments for most dwellings within the rural community of Woodham Walter but may be considered where appropriate to the area character or the property concerned.



The Built Environment - Garages 3

Garages **D1 H4 S1 T1 T2**

A high level of questionnaire response considered speeding traffic and indiscriminate vehicle parking in the Village to be dangerous and a major concern. Particular reference was made to The Street and the narrow village lanes. To reduce the danger from parked vehicles, all new development including alterations and extensions where possible should provide off-street parking to the very minimum of the local authority standard.

Woodham Walter is a rural area with a noted lack of public transport, cars are therefore essential but they should not deter from the countryside environment. The provision of off-street garaging is necessary but buildings should not dominate the site and diminish the street scene particularly by exposing composite roofs. Large expanses of hard driveway in front of dwellings are not visually acceptable (see Boundary Treatment on the previous page) and exacerbate surface water run-off in rainy weather. Garage provision and off street parking should be located to the side of the main building, form part of the main structure or be located at the rear of the premises but not in front of it detracting from the dwelling. Garage size should be fit for purpose and follow local authority and national guidelines as a minimum.

Although not a planning matter, parking on grass verges visually destroys the Village ambience and character and results in deep, muddy ruts that are both unattractive and potentially dangerous. Comment was also made in response returns about vehicles parking on the whole or part of the pavement restricting passage for prams, wheelchairs and elderly with walking sticks or frames.



Guidance

Parking

3.61 *Garage and off-street parking should be to the local authority standard.*

D1
H4
T1
T2

3.62 *Garages should not dominate the site but be set to one side rather than in front of the dwelling.*

S1
D1

3.63 *Garages sited in front of the main dwelling should be avoided.*

D1

3.64 *Sufficient off-street parking outside of garages should be provided on drive-ways.*

D1
T2

Preferred Options

3.65 *Integral garages could be considered on restricted sites.*

3.66 *Driveways and vehicle hard standing treatments should reflect the Woodham Walter character.*

3.67 *Parking on grass verges and on pavements should be discouraged.*

3.68 *Garages should have pitched roofs, not flat or combinations of pitch and flat.*

3 The Built Environment - Roads

Preferred Options

Roads

- 3.69 Avoid the introduction of new roads, widening road carriageways and junctions.
- 3.70 Access points should be designed to reduce vehicle speeds.
- 3.71 Private drives should be designed for any new multiple unit developments.
- 3.72 Straight roads with formal hammerheads should be avoided in favour of serpentine routes with formally shaped turning heads.
- 3.73 Where pavements are required, incorporate grassed verges.
- 3.74 Use 'soft' varying materials using a local palette to designate pedestrian areas from vehicular transit zones.
- 3.75 Vary surface treatments in accordance with Highway requirements.
- 3.76 Avoid the proliferation of road traffic signs. Highway safety is paramount.

Roads

Roads are intrinsically a matter for the Highway Authority and it is essential to work with them to secure a safe and design conscious solution.

A majority of respondents commented that speeding vehicles in The Street and Rectory Road was the second most important issue (after the lack of a village shop) causing the greatest disgruntlement. A significant majority considered that adding new roads, widening existing carriageways and road junctions would add to the existing traffic volume and speed issues. A very large percentage of respondents were advocating that the Village should have HGV and load restriction limitations applied although a significant majority considered that physical methods of speed control by way of speed bumps and similar measures were inappropriate for a rural community as were a proliferation of traffic signs. These issues fall outside of the Village Design Statement but were important to the residents so that it is necessary to work closely with Highways to ensure that the roads are up to standard and safe.

Housing layouts tend to be dominated by the car. In any new development where roads are necessary, a large percentage of respondents considered that such roads should be of a rural character but not to the extent of marginalising pedestrians. Small developments should have a 'private drive' approach for which a precedent is set off Rectory Road. Where pavements are required, these should be softened by the use of additional grass verges. Straight roads with formal hammerheads are discouraged in favour of serpentine routes within formally shaped turning areas. Variety in surface materials, not solely tarmacadam, and footpath denotation will also create more attractive areas, define spaces and reduce traffic speed. Sizes and types of turning area are determined by the highway authority as are materials.



Private Drive Style Development



Straight Access Road with Turning



Prolific Road Signs

The Built Environment - Lighting 3

Lighting **D1 D2**

A majority of respondents (3.8%) considered that street lighting should be increased beyond its current level although a significant majority (19%) expressed an opinion that a 'dark sky policy' of no street lighting would contribute to the tranquillity of the Village. Street lights add to road safety and household security although currently they are turned off by the Council between midnight and 05:00 each night.

Community (street) lighting is a matter for the Highways Authority but Residents considered that lamp posts should be positioned so as not to cause interference to householders such as shining directly into bedroom windows. In 'private drive' style developments location of community lighting should be discreet but effect the necessary coverage for safety and security.

Domestic building, garden and security lighting was considered by a majority of questionnaire respondents to be intrusive. Security lighting should be carefully located and use of such lighting should take advantage of movement detectors for intermittent use rather than full on during the hours of darkness. Positioning of floodlights at high level together with eaves lighting should be discouraged so as not to be an inconvenience to neighbours or be intrusive in the street scene.

Glaring lights can be both a safety and a security risk so expert design of lighting schemes and careful selection of luminaires is necessary. Criminals need light too! Lights being on does not assure that crime will go away especially if there is no one around to stand guard watching the area concerned. External lighting also has an environmental impact that in turn increases energy demand.



Low Key, Local Illumination



Directional Street Light



Floodlight Illumination

Guidance

Lighting

- 3.77 **D1** Work with Highways to review the level and type of community lighting.
- 3.78 **D2** Use smaller, low key, local and directional lights fitted to movement detectors where possible.
- 3.79 **D2** Floodlighting the whole house is undesirable and may be dangerous to occupants and passers by causing glare.

3 The Built Environment - Street Furniture

Objectives

Roads

- S3** 3.80 Continue to work with Highways to instigate traffic calming measures and speed restrictions that are sympathetic to the character of the area.
- S3** 3.81 Continue to work with Highways to improve the road conditions and existing pavements.

Guidance

Street Furniture

- 3.82 Avoid the proliferation of road traffic signs by combining symbols.
- 3.83 Position street signs where they can be clearly seen, easily read, limit exposure to graffiti and ensure that they cannot become overgrown.

Preferred Options

- 3.84 Locate salt bins discreetly and where possible group with other street furniture such as seating and litter bins.
- 3.85 Retain iconic and vintage street furniture such as telephone kiosk and more modern post boxes.

Street Furniture **S3**

A substantial majority of respondents agreed that in a rural community street furniture needs to be at a minimal level but must satisfy road safety provisions. Street furniture which is not in keeping with the scale and character of the village detracts from the overall visual amenity. This point has been made elsewhere in this document. Woodham Walter is very fortunate in retaining a telephone kiosk after the iconic design of Sir Giles Gilbert Scott, together with three pedestal mounted letter boxes that add to the street scene.

Street names of a standard size should be replicated in any new development. Where road traffic signs can be combined, they should be to decrease the number of standing poles and to avoid confusion. Signs should be positioned at a suitable height to avoid vandalism, subjection to graffiti and can be easily read. They should be free from obstructions such as street greenery, simple and straightforward, with clear lettering and colour contrast with the background.

Whilst not a specific questionnaire question, the prospect of a traditional Village Sign has been raised to be positioned in the core village.

A salt bin has been provided in recent years and there are still demands for more away from the core Village. Where these are provided, in conjunction with Highways, they should be as discreet as possible and fit in with the adjacent area.



Iconic Telephone Kiosk
Adds Character



Combined Street Sign Reduces Clutter



Discreetly Locate Salt Bin with
other Street Furniture

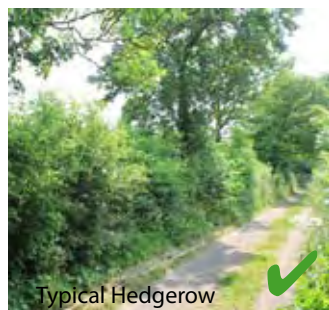
The Built Environment - Landscape Design 3

Landscape Design **N1**

Area landscape character is described in detail by the individual area character appraisals but the impact of landscaping on the built environment cannot be overemphasised. The questionnaire returns support this over several questions.

There is a very large palette of local materials and indigenous species that can be used in the rural environment of Woodham Walter. The use of hard, nonporous materials for drives etc. over 5m² currently require planning permission and as previously discussed are to be avoided. Non-permeable finishes are considered as permitted development. Ballast, pebbles and permeable setts/pavers/gravel mix make a surface that is crunchy when walked upon and are a good security warning to the householder; soft earth allows clear reproduction of footprints and are deterrents to criminals. If a gravel retention system is used then the surface is compacted enough for wheelchair and baby buggy users and prevents overspill of material onto the carriageway which becomes a Highways issue. A combination of these elements are a productive way of landscaping the front of properties whilst providing a pleasant visual appearance to the passerby.

Planting to front gardens should be low in height to maintain the open rural feel and character of the Village and to avoid providing cover for unauthorised trespass and entry. Spiky plants and shrubs such as berberis and pyracantha (firethorn), when planted near to the building and windows, provide a further deterrent to the criminal whilst having colour shape and texture. The planting of indigenous trees and hedgerow species such as blackthorn, hawthorn, hazel, dogwood, holly, field maple and beech interspersed with oak, ash, hornbeam, rowan and birch are to be encouraged as are roses, hypericum, hebe, senecio, berberis and many others. These will support the area's character and the overall appearance of the Village by enhancing the natural and historic environment.



Typical Hedgerow



Pyracantha



Berberis



Holly

Guidance

Soft Landscaping

3.86 **N1** Wherever possible apply soft landscaping to front gardens rather than hard paving.

N1 Use combinations of gravels, setts and pavings to provide attractive walking and parking areas.

N1 Use native tree and shrub species wherever possible.

Preferred Options

3.89 Residents are encouraged to maintain front garden landscaping to retain colour, shape and form as part of the street scene.

3 The Built Environment - Leisure

Guidance

Open Space, Sport, Leisure and Tourism

3.90 *Development that would result in loss of or have a negative impact on any sport, leisure, tourist facility, heritage asset or public footpath or bridleway will not be supported. Alternative provisions must be provided.*

3.91 *Encroachments and constrictions to existing public open space facilities and footpaths will not be supported.*

3.92 *To maintain the extensive footpath network and address the safety of the cycle network.*

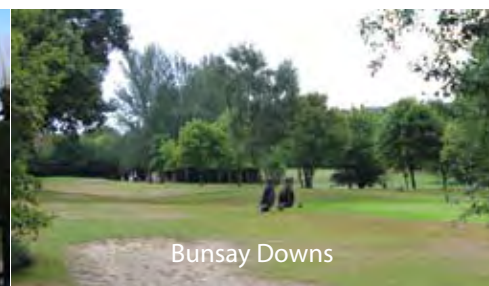
Open Space, Sport, Leisure and Tourism E4 E5 N1 N2 N3 T1

People choose to live in Woodham Walter so that they can experience the unique benefits of living in a small rural community – fresh air, peace and tranquillity, space and fine views in addition to the many social and recreational facilities:

- St Michaels Church
- Village hall
- Woodham Walter Women's Club
- Mobile library
- The Bell PH
- The Queen Victoria PH
- The Cats PH
- The Warren Golf and Country Club
- The Warren Active Health Club
- Bunsay Downs Golf Club
- The Retreat Holistic Centre
- The Chelmer and Blackwater Navigation – boating and angling
- Allotments
- The Bell Meadow
- Woodham Walter Common (SSSI)
- Elwy Lodge NGS Garden
- Public Footpaths
- Other activities (weekly or fortnightly) – Village Supper Club, Village Lunch Club (monthly), whist drives, yoga and zumba classes, mobile library



The Womens Club



Bunsay Downs



Bell Meadow



The Bell P.H



The Queen Victoria P.H

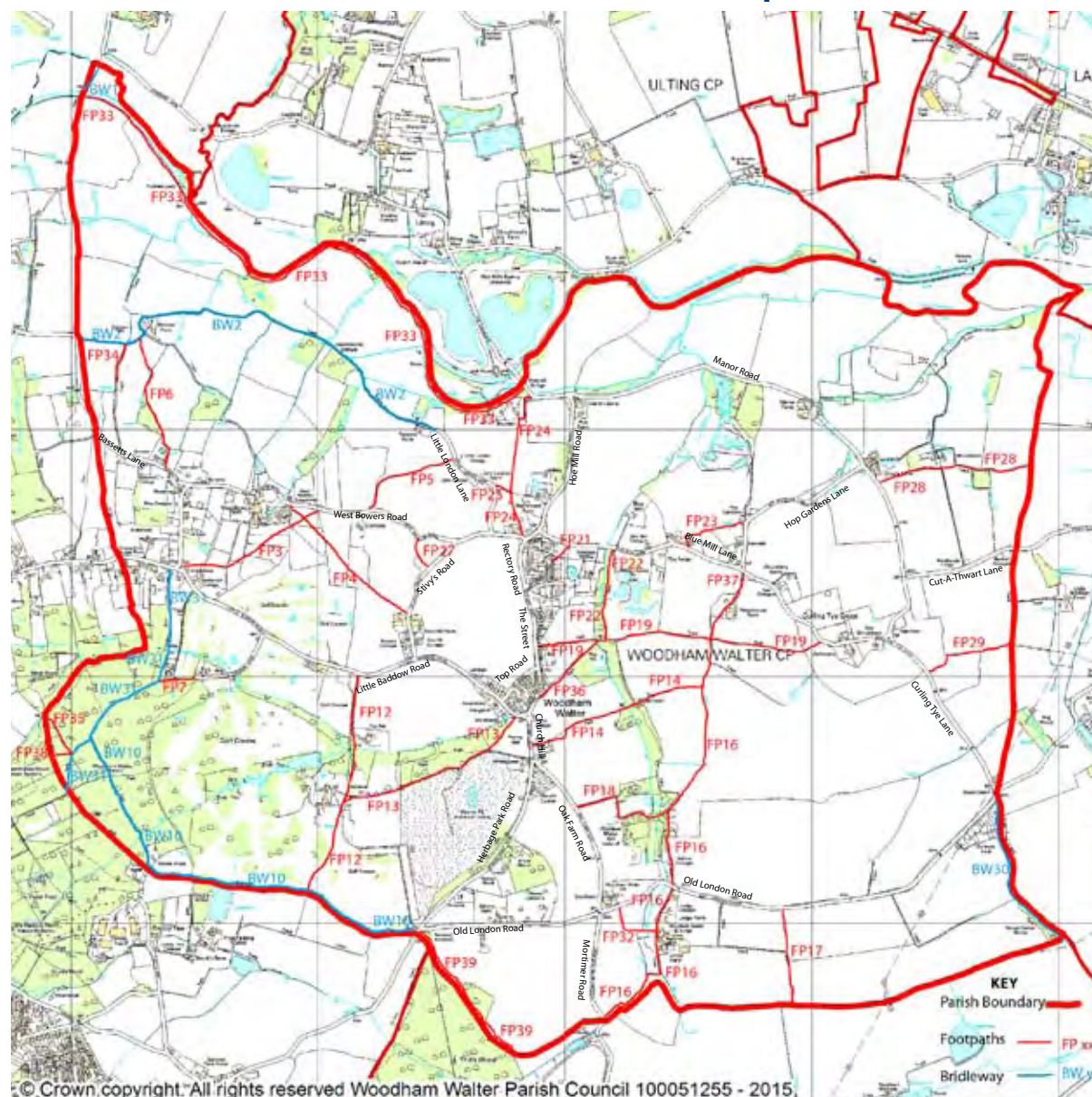


The Cats P.H

The Built Environment - Footpaths 3

Footpaths **E5 N1**

- BW1 Bridleway. From Hatfield Peverel towards the River Chelmer.
 BW2 Bridleway. From the parish boundary eastwards through Retreat Farm and then south-easterly to Raven's Farm.
 FP3 Crossways eastwards crossing FP4 east of Westbowers.
 FP4 Westbowers in a south-eastwards crossing FP3 north of Gunhill Farm.
 FP5 From the road north of Glendale in a north and eastwards towards Little London.
 FP6 From BW2 in a southward to the road leading to "Elwy".
 FP7 From Common Lane westwards to its junction with BW31.
 BW10 From BW31 along the District/Parish boundary line to Redgates.
 FP12 From Hawkins Farm southwards past Warren Farm to BW10 at the boundary with Woodham Mortimer, continuing as FP22.
 FP13 From FP12 in a northeastwards direction to the Bell Inn.
 FP14 From St Michael's Church eastwards through The Wilderness to junction with FP16.
 FP16 From FP19, passing east of The Wilderness, to Lodge Farm and then to the parish boundary.
 FP17 From Maldon road east of Lodge Farm southwards to the parish boundary with Woodham Mortimer where it continues as FP14.
 FP18 From FP16 southwestwards, to Oak Farm. 4 ft. min. width.
 FP19 From the School eastwards towards Curling Tye Green.
 FP21 From the estate path connecting to the northern end of Mead Pastures in a north-easterly direction to Blue Mill Lane.
 FP22 From its junction with FP19 in a north-easterly direction to Blue Mill Cottages.
 FP23 From the Cats Public House in a northerly and easterly direction to the road north-west of Whitehouse Cottages.
 FP24 From the Lodge south of Hoe Mill Bridge southwards towards Hatchmans.
 FP25 From Little London in an eastwards direction to FP24.
 FP27 From West Bowers Road southwards towards Gunhill Farm.
 FP28 From south of Guys Farm eastwards to the parish boundary.
 FP29 From Curling Tye Green Road in an easterly direction to the parish boundary with Maldon where it continues as FP19.
 BW30 Bridleway. From Wood Corner in a southerly direction along the parish boundary to Maldon where it continues as BW5.
 BW31 Bridleway. Spring Elms southwards through Woodham Walter Common parish boundary with Little Baddow.
 FP32 From the road south of Oak Farm southeastwards to Lodge Farm.
 FP33 Continuation of Little Baddow FP3 along towpath to Hoemill Bridge.
 FP34 From Retreat Farm southwards for 90 yards to the public highway.
 FP35 Continuation of Little Baddow FP55 to join BW31.
 FP36 From FP19 south west to the road north of the Smithy.
 FP37 From Whitehouse Cottages, south to FP19.
 FP38 Continuation of FP87 Little Baddow eastwards to join BW31.
 FP39 From Redgates southwards along boundaries of Thrift Wood to continue as FP7 in Woodham Mortimer. Path width of is 4 feet in part.



4 Bibliography

Key Evidence Base Documents

Green Infrastructure Study (MDC EB041a)
 National Planning Policy Framework (NPPF)
 Emerging Local Development Plan (MDC)
 Maldon District Characterisation Assessment (EB053)
 Essex Design Guide 2006
 PPG3 Housing (ODPP)
 PPS1 Delivering Sustainable Development (ODPP)
 Scheduled Ancient Monuments (MDC)
 Rural-Urban Classification for Output Areas in England (DEFRA)
 Braintree, Brentwood, Chelmsford, Maldon and Uttlesford Landscape Character Assessments (Chris Blandford Associates) [EB46]
 National Character Area profile: 111 Northern Thames Basin

Other Documents

"A Village History, Woodham Walter" by Patricia M Ryan
 "London, An Illustrated History" by Cathy Ross and John Clark

Web Based Research

Planning Portal (UK Government)
 Historic England (Building Listings)
 Superfast Essex Broadband - <http://www.superfastessex.org>

Village Design Statement Committee

Keith Amato
 David Beattie (Chairman)
 James Bunn
 James Nolan
 Ken Rennie
 John Tompkins

* * *

Cover Image (Bell Meadow Day Photography Competition Winner)
 Photo courtesy of Stuart Koenig-Roach © 2013
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* * *

The Committee would like to express their thanks to

Ann and Sean at The Bell
 for accommodating Committee Meetings.

The Maldon District Council for their help and advice.

* * *

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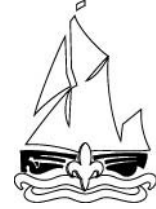
* * *

Cover Image, Church Corner.
 Back Cover, The Bell PH and a View towards the Village from the river



The School from Church Hill





REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

A12 CHELMSFORD TO A120 WIDENING: HIGHWAYS ENGLAND CONSULTATION

1. PURPOSE OF THE REPORT

- 1.1 The purpose of this report is to consider the Council's response to the A12 Chelmsford to the consultation into the A120 Widening. A draft response is set out in **APPENDIX 1** to this report.
- 1.2 The consultation period runs until 3 March 2017.

2. RECOMMENDATION

That the response to the A12 Chelmsford to A120 widening consultation, as set out in **APPENDIX 1** be approved.

3. SUMMARY OF KEY ISSUES

- 3.1 Highways England is consulting on proposals to improve the A12 between Chelmsford and the A120 at Marks Tey. The improvements are programmed to start in 2020 and are in response to general traffic congestion, environmental and safety factors.
- 3.2 At peak travel times much of the A12 between J19 and J25 is nearing capacity. Highways England state that *'forecasts show that by 2038, the road will operate above the capacity it was ever designed to handle if no improvements are made'*. As the road reaches capacity, congestion increase, which will not benefits communities along the A12 corridor, and access to Maldon District, which relies on the A12 as a major strategic route for residents, visitors and businesses. A congested A12 will not benefit the aim to attract new businesses to and create 2,000 new jobs in Maldon District. The aim of the A12 widening scheme is to increase the road's capacity to improve its long term safety and reliability.
- 3.3 Highways England have identified six key benefits of the proposals:
 - A more free flowing network;
 - A safe and serviceable network;
 - Supporting economic growth;

- An improved environment;
- A more accessible and integrated network;
- Customer satisfaction.

3.4 Four options are being consulted upon:

3.4.1 Option 1:

3.4.1.1 Widening the existing A12 to three lanes in both directions from Junctions 19 to 25 (Boreham to Marks Tey). This will potentially will be the most disruptive option for residents and businesses located close to the A12. It will bring the road closer to existing premises, private accesses to the road will be closed and diverted, Highways England acknowledge that this option *'requires the acquisition of most properties'*. It is not clear whether this option would include the same opportunity as the off line options for junction improvements that improve access to and from Maldon District

3.4.2 Option 2:

3.4.2.1 Widening of the A12 to three lanes in both directions from Junctions 19 to 25, with 2 new bypasses between junctions 22 and 23 (Witham and Kelvedon) and between Junctions 24 to 25 (Feering to Marks Tey). The two lengths of the A12 that are constrained by at-grade private accesses, with existing safety and capacity issues, are proposed to be bypassed in this option. This will reduce impacts on residents and businesses along the A12 corridor along the two stretches most affected by direct private access.

3.4.2.2 Due to the constraint of the railway line running parallel to the north of the A12, the bypasses are shown as running to the south of the existing road. The bypasses will generally take traffic away from residential properties.

3.4.2.3 A bypass between J22-23 (also proposed in Option 3) will circumvent Rivenhall End and should enable safer access to and from Braxted Road, as the new bypass will enable a new junction to be designed.

3.4.2.4 The proposed bypass between Feering and Marks Tey (J24-25) (also proposed in Option 4) will enable a north and southbound junction to be created at Feering, which currently only has northbound access. This should improve access to the A12 from the north of the District. A north and south bound junction at this point is particularly important, as the draft Braintree Local Plan is proposing a 1,000 home development at Feering. Without the two way junction, all south bound traffic from the development will pass through Kelvedon High Street which is already highly congested. This would negatively impact Maldon District residents' and businesses' access to the A12 via Great Braxted, Tiptree and Inworth.

3.4.3 Option 3:

3.4.3.1 Widening of the existing A12 corridor to three lanes from Junctions 19 to 25 with one bypass between Junctions 22-23 (Witham to Kelvedon). One of the two lengths of the A12 that are constrained by at-grade private accesses, with existing safety and capacity issues, are proposed to be bypassed in this option. This will reduce impacts

on residents and businesses along the A12 corridor at Rivenhall and provide the opportunity to improve the access point to the A12 from Braxted Road.

3.4.4 Option 4:

- 3.4.4.1 Widening of the existing A12 corridor to three lanes from Junctions 19 to 25, with one bypass between Junctions 24 to 25 (Feering to Marks Tey). One of the two lengths of the A12 that are constrained by at-grade private accesses, with existing safety and capacity issues, are proposed to be bypassed in this option. It will provide the opportunity to provide a new north-southbound junction at Feering.
- 3.5 The lines of each option are shown indicatively on the consultation maps (reproduced in **APPENDIX 2** to this report).
- 3.6 The proposed improvements are a one-off opportunity for the Highways England to ensure that Maldon District is not bypassed and to the fix inherent accessibility limitations caused by the inadequacy of existing junctions. If the opportunity is not taken now, and the route plus junctions are confirmed in the preferred option in the summer 2017, it would have a lasting impact on the ability of the District to attract investment, visitors and new residents. Therefore, the Council must be clear in its intent to secure the best access for Maldon.
- 3.7 Option 2 provides the best opportunities to improve access to the A12 from this District at both Rivenhall End and Feering. Building the bypasses off-line along two stretches, will reduce the impact of the roadworks on users of the A12, as less of the existing road will need to be changed. Crucially, it will also create the best opportunities for new grade separated junctions, built to modern standards and safety, that provide easier access to the north and southbound carriageways. For these reasons, Option 2 is this Council's preferred option.
- 3.8 The consultation asks for responses on whether junction improvements are required. Throughout the stakeholder engagement process, both Members and Officers have emphasised the importance of improving access to the A12 at Hatfield Peverel.
- 3.9 The consultation offers three options for the Hatfield Peverel junctions 20a and 20b:
- A) Retain and improve existing junctions 20a and 20b;*
 - B) Remove junction 20a and 20b and create a new junction 20;*
 - C) Neither.*
- 3.9.1 Either option A or B should result in improved access to the A12 from Maldon District. The consultation information shows that although the volume of traffic using J20a and J20b is similar to that using Junction 22 (north Witham), there have been twice as many collisions at J20a / 20b than at J22 over the last five years. Junction 20a currently offers only southbound access to the A12. The northbound exit at Junction 20a enters Hatfield Peverel at the T-junction of Bury Lane and The Street (B1137). Accessing The Street from Bury Lane is difficult at peak times, due to the volume of traffic passing through the village.
- 3.9.2 At Junction 20b, there is a very good slip road coming off the A12 south bound. The entrance slip to the A12 northbound is below standard, reducing the ability to join the

A12 safely at this point. Junction 20b offers northbound access only. Access to J20b is further constrained by access onto The Street from Maldon Road.

- 3.9.3 Option A for Junction 20a and 20b will result in improved access to and from the A12, however it may not deal with the current access issues at J20a of the Bury Lane / The Street T-junction or the Maldon Road/The Street mini-roundabout near J20b.

Option B for Junction 20a and 20b will result in the creation of a new single Junction 20. This will create safer, access both to and from the A12, north and southbound. However, it may not result in reduced volumes of traffic passing through the village itself, as there will only be one junction in the future, not two. As this option provides the opportunity to overcome the access and safety issues with the current two junctions, it is this Council's preferred option. There is already a significant bottleneck at Hatfield Peverel where traffic backs up along Maldon Road when seeking to access the A12. Solutions will need to consider appropriate B Road access to the new junction to address this.

3.10 **A12 widening scheme timetable**

- 3.10.1 This consultation closes on 3 March 2017.

- 3.10.2 The key milestones for this project are given below. Highways England has, wherever possible, compressed the different stages for this project, enabling the proposed construction start date to be brought forward to March 2020. The Development Consent Order (effectively the planning application) process is a statutory 18 month process, over which Highways England has no control.

January - March 2017.....	Public consultation on the Options.
Summer 2017	Preferred option announced; further engagement with communities, landowners and stakeholders.
Autumn 2017	Statutory public consultation on the preferred route.
Summer 2018	Submit an application for a Development Consent Order.
Winter 2019	Planning Inspectorate makes a recommendation to the relevant Secretary of State, who issues the final decision.
March 2020	Construction starts (estimate).

4. **CONCLUSION**

- 4.1 Although the proposals are only indicative at this stage, the improvements proposed for the A12 in this consultation aim to improve its long term capacity, traffic flow and safety. The proposals offer the opportunity for improved access to the A12 from Maldon District and are therefore welcomed. Option 2 would provide the greatest benefit to Maldon District.

5. IMPACT ON CORPORATE GOALS

- 5.1 The improvements proposed for the A12 will have positive impacts on the corporate goals of protecting and shaping the District, and creating opportunities for economic growth and prosperity.

6. IMPLICATIONS

- (i) **Impact on Customers** – The aim of the A12 widening scheme is to increase the road's capacity to improve its long term safety and reliability. This will improve journey times and journey safety for both district residents and businesses using the A12.
- (ii) **Impact on Equalities** – There should be no negative impacts on equalities from this scheme.
- (iii) **Impact on Risk** – The completed scheme should reduce business risks through reducing congestion on the A12. The improvements should improve the safety of the road and junctions for all users.
- (iv) **Impact on Resources (financial)** – None.
- (v) **Impact on Resources (human)** – None.
- (vi) **Impact on the Environment** – As with any road widening project there will be environmental impacts from this scheme. The online widening will bring the road closer to residential areas, and will result in private accesses to the A12 being closed and diverted. The proposed bypasses, will improve the environmental quality at existing residences by moving the A12 away from them. The bypasses will result in the loss of farming and other land along their route. The environmental impacts, both positive and negative, of the scheme are detailed on pages 28 - 31 of the consultation brochure.

Background Papers:

‘A12 Chelmsford to A120 Widening consultation’ brochure; ‘A12 Chelmsford to A120 widening Have Your say’ booklet – both provided to all Members w/c 23-1-17. Copies are available at Maldon District Council reception.

Enquiries to: Leonie Alpin, Planning Policy Officer, (Tel: 01621 876278).

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Proposed response from Maldon District Council

Section 1: A12 Widening Options

- 1) Maldon District Council's Preferred route option is Option 2 (online widening and two bypasses)

Maldon District Council supports option 2. The J22-23 bypass will bypass Rivenhall End and should enable safer access to and from Braxted Road, as the bypass will enable a new junction to be designed that connects the Braxted Road to the A12.

The proposed bypass between Feering and Marks Tey (J24-25) will enable a north and southbound junction to be created at Feering, which currently only has northbound access. This should improve access to the A12 from the north of the District. A north and south bound junction at this point is particularly important, as the draft Braintree Local Plan is proposing a 1,000 home development at Feering. Without the two way junction, all south bound traffic from the development will pass through Kelvedon High Street which is already highly congested. This would negatively impact Maldon District residents' and businesses' access to the A12 via Great Braxted, Tiptree and Inworth.

Section 2 Junction Improvements

- 2) *Do you think that improvements are needed to junction 19? (Boreham)*

The Council welcomes junction improvements along the A12 that improve the efficiency of the junctions and minimise disruption to the A12 and approach roads.

- 3) *Preferred option at junction 20a & 20b (Hatfield Peverel):*

- A) *Retain and improve existing junctions 20a and 20b*
- B) *Remove junction 20a and 20b and create a new junction 20*
- C) *Neither*

Maldon District Council's preferred option for junction 20a and 20b is Option B (a new junction).

Junction 20a currently offers south bound access onto the A12 and northbound egress only. 20b offers northbound access and southbound egress.

The northbound exit at Junction 20a enters Hatfield Peverel at the T-junction of Bury Lane and The Street (B1137), the main road through the village. Accessing The Street from Bury Lane is difficult at peak times, due to the volume of traffic on the B1137.

At Junction 20b, there is a very good slip road coming off the A12 south bound. However, the entrance slip to the A12 northbound is below standard, reducing the ability to join the A12 safely at this point. Junction 20b offers northbound access only. Access to J20b is further constrained by the access from Maldon Road, onto The Street.

There are safety issues with the existing junctions. Junctions 20a and 20b, combined, have a similar volume of traffic as that at Junction 22, yet there have been twice as many collisions at the Hatfield Peverel junctions (see page 36 - 37 of the consultation brochure).

A new junction would provide the opportunity to design and build one junction, to an adequate standard, with both north and south bound access that could overcome the existing constraints and safety issues at the current junctions.

There is already a significant bottleneck at Hatfield Peverel where traffic backs up along Maldon Road when seeking to access the A12. Solutions will need to consider appropriate B Road access to the new junction to address this.

4) *Do you think that improvements are needed to junction 21? (Witham S)*

The Council welcomes junction improvements along the A12 that improve the efficiency of the junctions and minimise disruption to the A12 and approach roads.

5) *Do you think that improvements are needed to junction 22? (Witham N)*

The Council welcomes junction improvements along the A12 that improve the efficiency of the junctions and minimise disruption to the A12 and approach roads.

6) *Do you think that improvements are needed to junction 23? (Kelvedon)*

The Council welcomes junction improvements along the A12 that improve the efficiency of the junctions and minimise disruption to the A12 and approach roads.

7) *Do you think that improvements are needed to junction 24? (Feering)*

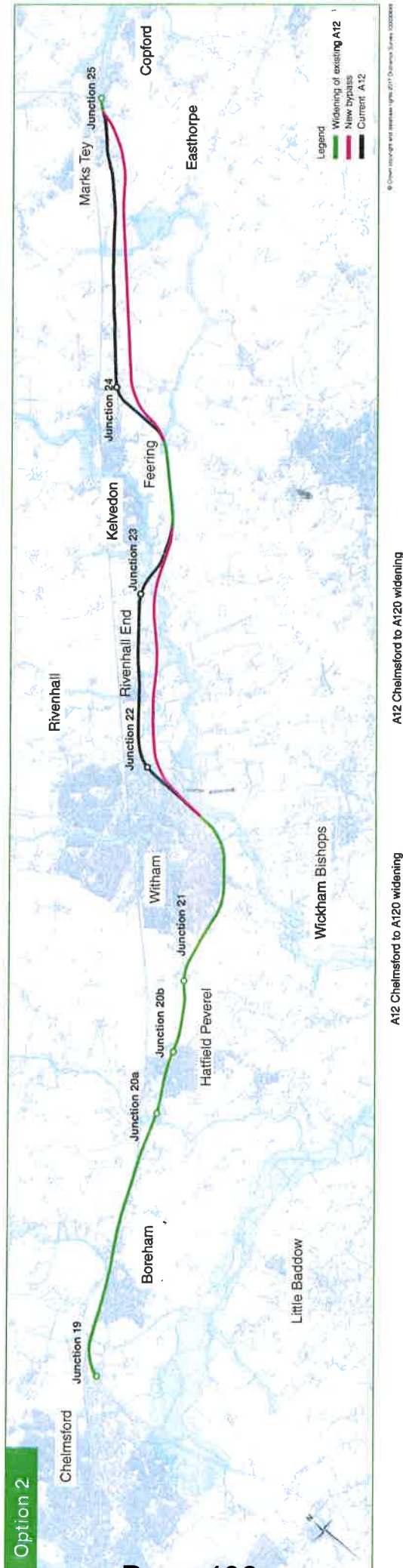
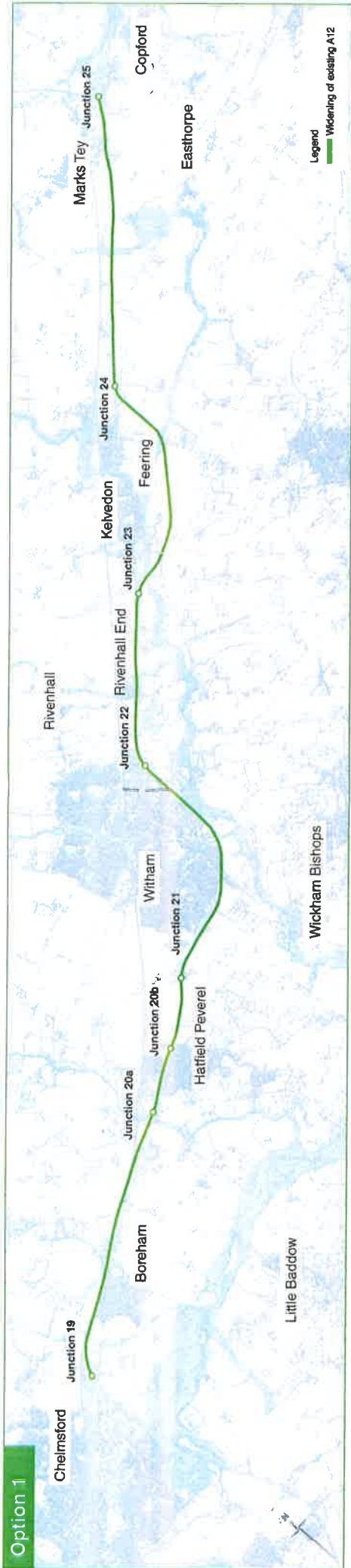
Yes. The proposed bypass between Feering and Marks Tey (J24 - 25) will enable a north and southbound junction to be created at Feering, which currently only has northbound access. A north and south bound junction at this point is particularly important, as the draft Braintree Local Plan is proposing a 1,000 home development at Feering. Without the two way junction, all south bound traffic from the development will pass through Kelvedon High Street which is already highly congested. This would negatively impact Maldon District residents' and businesses' access to the A12 via Great Braxted, Tiptree and Inworth.

8) *Do you think that improvements are needed to junction 25? (Marks Tey)*

The Council welcomes junction improvements along the A12 that improve the efficiency of the junctions and minimise disruption to the A12 and approach roads.

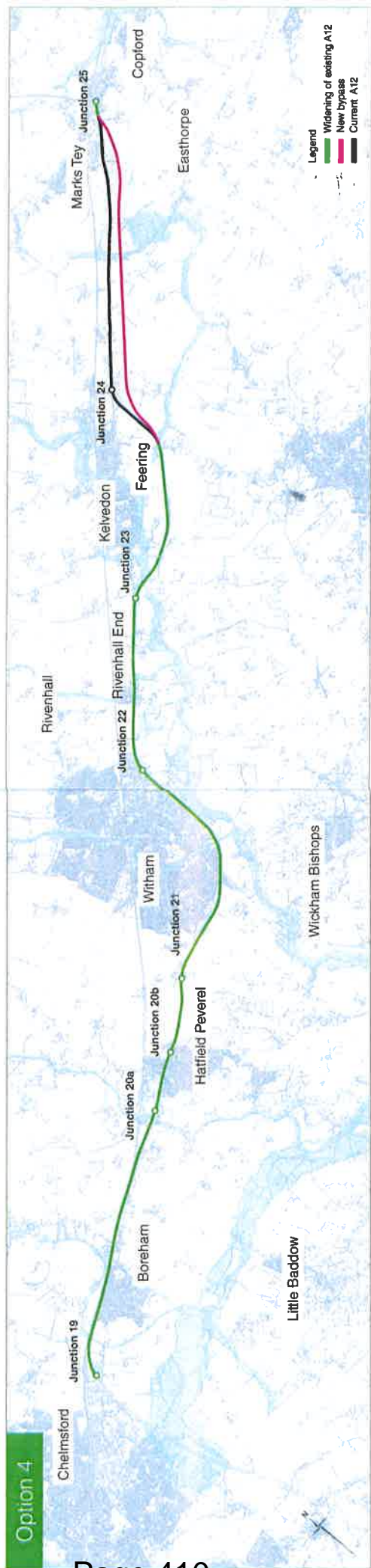
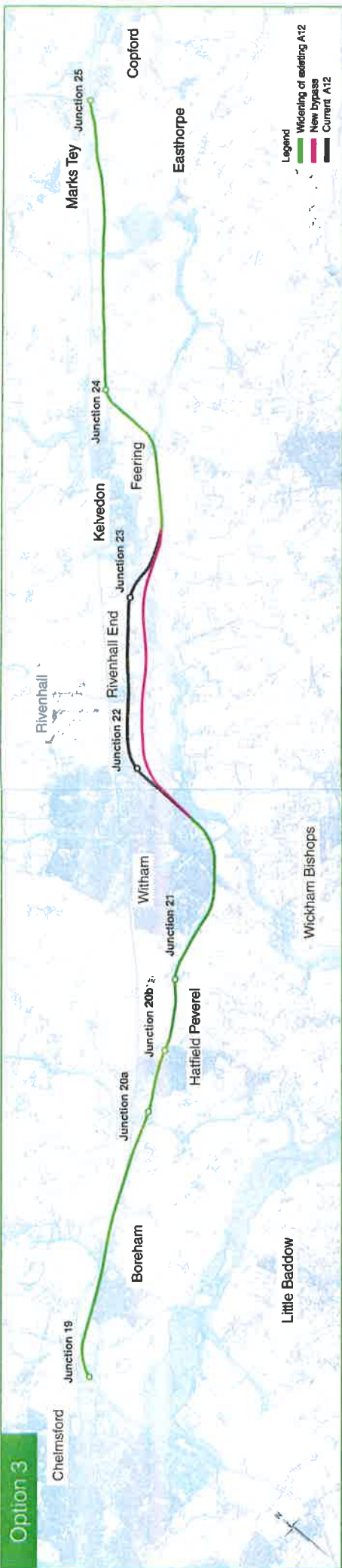
9) *Any further comments regarding existing and/or new junctions along the route*

The Council welcomes junction improvements along the A12 that improve the efficiency of the junctions and minimise disruption to the A12 and approach roads.



A12 Chelmsford to A120 widening

A12 Chelmsford to A120 widening



A12 Chelmsford to A120 widening

A12 Chelmsford to A120 widening



REPORT of INTERIM HEAD OF PLANNING SERVICES

**to
PLANNING AND LICENSING COMMITTEE
2 MARCH 2017**

PLANNING POLICY UPDATE

1. PURPOSE OF THE REPORT

1.1 This report provides an update on the Local Development Plan, progress of supplementary planning documents and the Housing White Paper. Separate reports have been prepared for this Committee on:

- Community Infrastructure Levy
- A12 Widening Consultation
- Maldon Design Guide

1.2 This report is for Members' information only.

2. RECOMMENDATION

That the Committee notes the Planning Policy update outlined in this report and that at the appropriate time matters for decision will be brought to this Committee for consideration.

3. SUMMARY OF KEY ISSUES

3.1 Local Development Plan

3.1.1 The Council considered the draft Post Examination Modifications to the Local Development Plan at its meeting on 16 February 2017. At the time of this report, Officers had not received confirmation from the Inspector that the draft Modifications had been approved. Once this is received the Modifications can be published for Public Consultation. Officers have advised the Inspector that the earliest we can start the consultation, given the lead in time for the publication of the Statutory Notice is Friday 10 March 2017.

3.1.2 Officers have also compiled a set of documents requested by the Inspector during the hearings. This includes updates to previous evidence and Hearing Statements to clarify the Council's position.

3.1.3 The Inspector also requested that the Sustainability Appraisal of the Plan is updated. This has been completed and will be published alongside the Modifications.

3.2 Local Development Scheme Documents

3.2.1 The Council approved a new Local Development Scheme (LDS) at its meeting on 16 February 2017. In addition to the Maldon Design Guide, the Central Area Master Plan and the Green Infrastructure Study, which are already advanced, the LDS includes new Supplementary Planning Documents for:

- Affordable Housing;
- Renewable Energy;
- Vehicle Parking Standards.

3.2.2 The LDS also included an updated Statement of Community Involvement.

3.2.3 Preliminary work has started on each of these, and updates will be provided in future Planning Policy Update reports, with each document being presented to the Planning and Licensing Committee at the appropriate stages.

3.3 Planning Legislation and Policy Update

3.3.1 The Secretary of State for Communities and Local Government published a Housing White Paper called 'Fixing our Broken Housing Market' on 7 February 2017. The White Paper is open for Public Consultation which began on 7 February 2017 and ends on 2 May 2017. There is an opportunity for the Council to formally respond to the White Paper. The Consultation is based on 38 main questions which relate to each sub-section of the White Paper.

3.3.2 The Government has produced the White Paper in response to what they see as a threefold problem in the housing sector:

'Not enough local authorities planning for homes they need

House building that is simply too slow

A construction industry that is reliant on a small number of big players.'

3.3.3 The White Paper is broadly split into 4 sections:

'Planning for the right homes in the right places

Building homes faster

Diversifying the market

Helping people now'

3.3.4 The Housing White Paper includes proposals to modify the National Planning Policy Framework. This will impact on future planning policy and decision making.

3.3.5 It is proposed to report the draft response to the Housing White Paper to this Committee before the consultation closing date.

4. CONCLUSION

- 4.1 This report provides an update to Members on the A12 / 120 consultation, the Maldon Design Guide and Planning Policy legislation.

5. IMPACT ON CORPORATE GOALS

- 5.1 The preparation and adoption of the Local Development Plan (LDP), including supporting evidence and other supplementary planning documents, support corporate goals which underpin the Council's vision for the District and in particular protecting and shaping the District and balancing the future needs of the community and meeting the housing needs of the District.

6. IMPLICATIONS

- (i) **Impact on Customers** – This report provides customers with the most up-to date progress on a planning policy matters. The Local Development Plan will replace the Maldon District Replacement Local Plan 'saved policies' as the Development Plan for the District. This will provide a clear and up-to-date spatial vision and policy framework for the District. New supplementary planning guidance will provide greater detail on the implementation of planning policies for developers and residents strengthening the planning process.
- (ii) **Impact on Equalities** – The progression of the Local Development Plan and supplementary planning documents helps to provide sustainable and well planned communities which promote social cohesion and help to provide for the future needs of the local community. Planning policy documents are subject to equality analysis.
- (iii) **Impact on Risk** – An up to date Local Development Plan, supported by robust supplementary planning documents, is required in accordance with the National Planning Policy Framework in order to enable the Council to strategically plan for future needs, growth and sustainable development, and provide an adequate five year supply of deliverable land for housing.
- (iv) **Impact on Resources (financial and human)** – Without an up-to-date Local Plan, the Council is at increased risk of planning appeals, which could potentially have significant resource implications. Any significant change in policy direction would be likely to require additional work or alterations to the evidence base which may have significant financial and human resource implications for the Council.
- (v) **Impact on the Environment** – An adopted Local Development Plan will enable the Council to promote sustainable development and safeguard the local environment in accordance with the local priorities for the District. The Local Development Plan has been subject to Sustainability Appraisal and Appropriate Assessment in accordance with the European Union (EU) directive on Strategic Environmental Assessment.

Background Papers:
Local Development Scheme 2017
Housing White Paper

Enquiries to: Ian Butt, Interim Strategic Planning Policy Manager, (Tel: 01621 876203).